

Environmental and Social Management Plan (ESMP)

Nepal Urban Governance and Infrastructure Project (NUGIP)

**Upgradation of Sagarmatha Agriculture bazar west to south
Madan Bhandari Hall to Janpremi tole Bisanpur ward 12 Road
(4.717KM)**

**Triyuga Municipality
Udayapur District, Koshi Province
Nepal**

March, 2024

EXECUTIVE SUMMARY

Introduction

The upgrading of 4.717Km length of Sagarmatha Agriculture bazar west to south Madan Bhandari Hall to Janpremi tole Bisanpur ward 12 Road alignment starts at PG chowk of Madan Bhandari highway (Sagarmatha agri bazar) and passes through Buddha chowk, Ekta chowk, Purano laxmipur, Sonapur, Chiura mil, Baruwa, Bisanpur park and end at three locations namely Baruwa corridor, Bisanpur park and Saonapur.

The road alignment passes through settlement of ward no 12 and 13 of Triyuga municipality. Its starting point coordinates are Easting 470878.9835m, Northing 2963083.1229m and elevation 195.592m end point coordinates are Easting 471719.2625m, Northing 2961019.3334 and elevation 189.744m. The proposed subproject is located in plain area of inner Terai and passes mainly through mixed land use having rural settlement and cultivated land. The project alignment does not run through ecologically sensitive area such as protected area, conservation area, wetland, national parks etc.

Baseline Information

The subproject is located in plain area of inner Terai at the average altitude of 192.668 masl with an average yearly temperature is 17.42°C (63.36°F) as well as average annual precipitation of sub project area is 131.8mm. The altitude of the municipality is ranges from 70m to 2509m from sea level. The proposed road alignment lies in the Sub Himalayan range. The alluvial river deposit is the most predominant along the road alignment. Road alignment does not cross any major rivers. Most of the area of present land use of Triyuga Municipality includes forest 76%, cultivated land 20%, barren land 2% and river 2%. In the proposed road alignment and Right of Way (RoW), there is no landslide/flood area and other disaster-prone area. The air and noise quality of surrounding areas is affected by the local city vehicles as well as Highway vehicles. The Noise and air quality of project area were tested and found within government standards. During the construction of the road, vehicular circulation along the road is likely to be disturbed. Trees like Pipal (*Ficus religiosa*), Banana-kvare (*Musa acuminata Colla*), Supari (Areca Catechu), Sissoo (*Dalbergia Sissoo*), Mango (*Magnifera indica*), Bakaino (*Melia azadirach*), Ashoka (*Saraca asoca*), Amba (*Psidium guajava*) and Banana (*Musa balbisiana*) are found in the project area.

The total population of Triyuga Municipality is 102,725 and total household is 25623. The average household size of the Municipality is 4.01 which is lower than that of district household size (4.20). The male and female population of the municipality is 48463 and 54262 respectively and sex ratio is 89.31. The total population and household of project affected wards - ward 12 and 13, is 14293 and 3520 respectively. The average household size of the project wards is 4.05. Total male population of the project wards is 6752 and total female population of the project wards is 7541. The sex ratio of the project wards 89.36. The project area is inhabited by brahmin, chhetri, chaudhary/tharu, rai, magar, biswokarma and mushar. In terms of religion, hindus, christians and buddist reside in these communities. There is presence of mixed groups of castes and ethnicities in the project area. The indigenous peoples in project area are chaudhary/tharu, Rai and Magar. The total households along the road alignment are 365nos. The majority of the people of this area are involved in agricultural work, business, labor work, and foreign employment. Water supply pipelines of Triyuga small town water supply committee provides drinking water supply in this area but there is no any integrated wastewater disposal system in the municipality. Within the construction width of the road, there is 42 nos. of electric poles identified for relocation.

There are few issues related to the GBV, SEA/SH were identified from field survey and records of nearby Police Office. Necessary mitigation programs have been planned and will implement during construction of sub project. The most prioritized issues on violence for planning of mitigation program are Domestic violence (physical and mental abuse), child marriage and sexual violence/ rape.

Legal and Regulatory Requirements

The sectoral and cross-sectoral guidelines and standards promulgated by the Government of Nepal (GoN) in various periods are adequate to mainstream the environmental and social safeguard dimensions in the project preparation and implementation phases. The report has included the applicable GoN plan, policies, act, regulations, guidelines, and standards. Similarly, the report has also included the environmental and social Safeguard requirements of the World Bank.

Screening, Scoping, Impact identification, Prediction and Management

The Direct Impact area of the project is considered as the 8m RoW of the subproject for the entire length of the road. Similarly, the indirect impact falls within 50 meters from the edge of the RoW. Environmental and social checklists were used for screening and summarizing the overall impacts. The site-specific impacts in construction and operation phases are included in the ESMP report. Some of the impacts include:

Beneficial Impacts from the Project Implementation

This sub project will create job opportunities for local residents by prioritizing employment for the poor, ethnic minorities, and disadvantaged locals. Other beneficial impacts includes enhancement of local business, improved accessibility and skill development of people. Active participation by local residents in sub project provides valuable skills and experience, contributing to personal growth and employability. Additionally, improved road infrastructure enhances accessibility to markets and social services, ultimately leading to a better quality of life for residents in the area. The availability of efficient transportation systems can positively impact education, healthcare, tourism, and other socio-economic sectors. Furthermore, an expanded road network may contribute to increased land values, benefiting the local community.

Adverse Impacts from the Project Implementation

The sudden increase in construction worker can strain public infrastructure and utilities such as roads, water supply, sanitation of project area. Ambient air pollution, Noise nuisance and Water pollution may likely to contribute to environmental pollution. The solid waste & spoil generation and stockpiling of construction materials might result in erosion during construction and operation phase. Vehicular movement, flowing water on the side drain of the road might cause erosion of soil on adjacent agricultural land. The operation of this road may result in expansion of settlement area and market which may increase encroachment of RoW. Similarly, the increase in dust, smoke and smoke of vehicles can increase air and noise pollution to some extent.

Mitigation Measures

The report proposes various measures to enhance benefits and mitigate adverse impacts, aiming to make this project environmentally friendly. Local people will be given priority for employment opportunities. Necessary measures will be taken to reduce the adverse effects on environment. The workers will be insured and provided

with first aid facilities and safety equipments like PPE. Provision of safe, clean and hygienic workplace and adequate WASH facilities at campsite. Code of Conduct to be implied for the workforce. For plantation on edge of the road, local species of plant will be given priority. Appropriate bio/civil engineering structures will be adopted to protect the environment and to stabilize the slopes along the road as were applicable. Proper drainage system will be provided to prevent accumulation of water on the nearby agricultural lands during operation. Adequate road safety measures like information and warning signs will be established to minimize road accident. Water supply pipelines, and electrical poles to be reinstated without delay.

Sexual exploitation/Assault/Harassment Prevention and response Action Plan

Based on the SEA/SH Risk Assessment checklist and assessment carried out for NUGIP by the World Bank, the Project's SEA/SH risks are assessed to be "Low". An SEA/SH Risk Mitigation Action Plan has been developed for NUGIP based on this assessment and includes specific measures that aim to prevent and mitigate GBV, in particular SEA/SH risks that the project activities might trigger. The Plan has also addressed "Table – 1: Recommended actions to address SEA/SH Risks in IPF Projects" as per the "Good Practice Note" published by the World Bank in September 2018.

Environment and Social Management Plan

The Environmental and Social Management Plan (ESMP) has been proposed with this report including issues identified, possible effects and impacts, measures for their mitigation, monitoring methods. The mitigation cost for environmental and social impacts in construction phases are included in report. In addition, agencies responsible for executing environmental and social mitigation measures and monitoring have been identified in the ESMP. Different monitoring indicators on the physical, biological, socio-economic and cultural environment have also been identified. The project along with the stakeholders will monitor during construction and operation phase.

Project-related grievances will be addressed through grievance redress mechanism (GRM) established for the project for the uptake and timely response on stakeholders' query and concerns about sub project. The committee can be provided with grievances through any of the mediums like written, verbal, telephone, letter, etc. and the committee will process it following the procedures of the project, and if not solvable, it will be forwarded to the higher level of GRM. Also, for mitigation of GBV, SEA/SH present with in the project area, the necessary orientation, workshops and trainings has been proposed in this ESMP report.

Institutional arrangements

The Ministry of Urban Development (MoUD) has set up a Project Coordination Office (PCO) under the Department of Urban Development and Building Construction (DuDBC) to implement NUGIP. The PCO is responsible for overall project compliance including compliance with environmental and social measures. The PCO will be supported by a Project Management Support Team (PMST). A Project Implementation Unit (PIU) will be established in Triyuga Municipality for implementation of the subproject at the local level and will be responsible for implementation of the ESMP and other environmental and social instruments. Technical Assistance will be provided through a Design and Supervision Consultancy (DSC) which includes safeguards specialists.

कार्यकारी सारांश

परिचय:

त्रियुगा नगरपालिकाको सगरमाथा कृषि बजार पश्चिमदेखि दक्षिण मदन भण्डारी हल हुँदै जनप्रेमी टोल बिसनपुर वडा नं १२ को ४.७१७ किलोमिटर लम्बाइको सडक स्तरोन्नति कार्य मदन भण्डारी राजमार्गको पिजी चोकबाट शुरु भई बुद्धचोक, एकताचोक, पुरानोलक्ष्मीपुर, सोनापुर, चिउरामिल, बरुवा, बिसनपुर पार्क हुँदै बरुवा करिडोरमा गएर अन्त्य हुन्छ।

त्रियुगा नगरपालिकाको वडा नम्बर १२ र १३ को बस्ती भएर यस सडक निर्माण हुनेछ । यसको प्रारम्भिक बिन्दु पिजी चोक चेनेज ०+००० मि. पूर्वान्तर ४७०८७८.९८३५ मीटर, उत्तरान्तर २९६३०८३.१२२९ मीटर र उचाई १९५.५९२ मीटर तथा समाप्त हुने बिन्दु बरुवा करिडोरको चेनेज ४+७१७ मि. पूर्वान्तर ४७२७२९.२६२५ मीटर, उत्तरान्तर २९६१०९९.३३३४ र उचाई १८९.७४४ मीटर रहेको छ। प्रस्तावित उपआयोजना भित्री तराईको समथर भूभागमा अवस्थित छ र मुख्यतया ग्रामीण बस्ती र खेतीयोग्य जमिन भएको मिश्रित भू-उपयोग हुँदै जान्छ। यस आयोजनाको बाटोको वरिपरिका वस्तीका स्थानीयहरूको जिवनस्तर र जीविकोपार्जनमा सुधार ल्याउनेछ। संरक्षित क्षेत्र, संरक्षण क्षेत्र, सिमसार क्षेत्र, राष्ट्रिय निकुञ्ज जस्ता पारिस्थितिकीय दृष्टिले संवेदनशील क्षेत्रमा आयोजना पर्दैन ।

विद्यमान अवस्था :

यो उपआयोजना तराईको समथर भू-भागमा समुद्री सतहबाट १९२.६६८ मि. उचाईमा अवस्थित छ। आयोजना क्षेत्रको नगरपालिकाको औसत वार्षिक तापक्रम १७.४२ डिग्री सेल्सियस रहेको छ साथै औसत वार्षिक वर्षा १३१.८ मिलिमिटर रहेको छ । यस नगरपालिकाको उचाई समुन्द्री सतहदेखि ७० मिटर देखि २५०९ मिटर सम्म रहेको छ। प्रस्तावित सडक एलाइनमेन्ट सब हिमालयन रेन्जमा पर्छ । सडक एलाइनमेन्टले कुनै पनि प्रमुख नदीहरू पार गर्दैन। यस नगरपालिकाको हालको भूउपयोगको अधिकांश क्षेत्रफल वन ७६ प्रतिशत, खेतीयोग्य जमिन २० प्रतिशत, बाँझो जमिन २ प्रतिशत, र पानीको क्षेत्र २ प्रतिशत रहेको छ । प्रस्तावित सडक एलाइनमेन्ट र सडकको क्षेत्राधिकारमा पहिरो/बाढी तथा अन्य विपद् जोखिम युक्त क्षेत्र छैन । आयोजना क्षेत्रको हावामा स्थानिय सवारी साधनवाट असर पर्ने र उक्त सवारीसाधन वाट ध्वनिमा असर पर्ने भएपनि ध्वनि गुणस्तर सीमा भित्रनै रहेको पाइएको छ । परियोजना क्षेत्रको आवाज र हावाको परिक्षण गर्दा सरकारी मापदण्ड भित्र नै रहेको पाइएको छ । सडक निर्माणको क्रममा सो सडकमा सवारी साधनको आवागमनमा बाधा पुग्ने सम्भावना रहन्छ । पिपल, केरा, सुपारी, सिसौ, बकाइनो, आसोका र अम्बा उप आयोजना क्षेत्रमा पाइने प्रमुख रूख प्रजातिहरू हुन्।

त्रियुगा नगरपालिकाको कुल जनसङ्ख्या १०२७२५ रहेको छ भने कुल घरधुरी २५६२३ रहेको छ। नगरपालिकाको औसत घरधुरी को आकार ४.०१ छ जुन जिल्लाको परिवारको आकार (४.२०) भन्दा कम छ। यस नगरपालिकाको पुरुष र महिलाको जनसङ्ख्या क्रमशः ४८४६३ र ५४२६२ रहेको छ भने लिंग अनुपात ८९.३१ रहेको छ । आयोजना प्रभावित वडा नम्बर १२ र १३ को कुल जनसंख्या र घरधुरी क्रमशः १४२९३ र ३५२० रहेको छ । उप आयोजना पर्ने वार्डहरूको औसत घरको आकार ४.०५ छ। आयोजना संचालन हुने वडाहरूको कुल पुरुष जनसंख्या ६७५२ र महिला कुल जनसंख्या ७५४१ रहेको छ ।

परियोजना सञ्चालन हुने बडाहरूको लिंग अनुपात ८९.३६ रहेको छ। आयोजना क्षेत्रमा ब्राह्मण, क्षेत्री, चौधरी/थारू, राई, मगर, विश्वकर्मा र मुसहरको बसोबास छ। धर्मको हिसाबले हिन्दू, इसाई र बुद्धिस्ट समुदायहरू बसोबास गर्छन्। उप-आयोजना क्षेत्रमा जातजातिको मिश्रित समूहको उपस्थिति रहेको छ। आयोजना क्षेत्रमा चौधरी/थारू, राई र मगर आदिवासी जनजाति बसोबास गर्दछन्। यस सडकखण्डमा ३६५ घरधुरी रहेका छन्। यस क्षेत्रका अधिकांश मानिसहरू मुख्यतया जीविकोपार्जनका लागि कृषिमा आधारित काम, व्यापार, श्रमिक र वैदेशिक रोजगारीमा संलग्न छन्। त्रियुगा साना नगर खानेपानी समितिको खानेपानी पाइपलाइनबाट यस क्षेत्रमा खानेपानी आपूर्ति भए पनि एकीकृत फोहरमैला व्यवस्थापन प्रणाली छैन। सडक निर्माण हुने क्षेत्र भित्र रहेको ४२ वटा विद्युतको पोल सार्नु पर्ने देखिन्छ। यौन शोषण तथा दुर्व्यवहार, लैङ्गिक हिंसा सम्बन्धित केही विषयहरू फिल्ड सर्भे र नजिकैको प्रहरी कार्यालयको रेकर्डबाट पहिचान गरिएको थियो। घरेलु हिंसा (शारीरिक तथा मानसिक दुर्व्यवहार), बालविवाह र यौन हिंसा/बलात्कार आयोजना प्रभावित क्षेत्रमा बिध्यमान रहेको छ।

ऐन तथा नीति, नियमको आवश्यकता

नेपाल सरकारले विभिन्न समयमा जारी गरेका विषयगत तथा बहुविषयगत निर्देशिका तथा मापदण्डहरू आयोजना तयार गर्न तथा कार्यान्वयन चरणहरूमा वातावरणीय एवम् सामाजिक सुरक्षण आयामहरू मूल प्रवाहीकरण गर्न यथेष्ट छन्। यस प्रतिवेदनले सम्बन्धित नेपाल सरकारका योजना, नीति, ऐन, नियम, निर्देशिका एवम् मापदण्डहरू समेटेको छ। त्यसैगरी यस प्रतिवेदनले विश्व बैङ्कको वातावरणीय तथा सामाजिक मापदण्डहरू पनि समेटेको छ।

वर्गीकरण, क्षेत्र निर्धारण, प्रभाव पहिचान, अनुगमन र व्यवस्थापन

सडकको सम्पूर्ण लम्बाइका लागि प्रस्तावित उप-आयोजनाको लागि प्रत्यक्ष प्रभाव क्षेत्र ८ मिटर सडकको क्षेत्राधिकारलाई मानिएको छ। त्यस्तै, अप्रत्यक्ष प्रभाव सडकको क्षेत्राधिकारको किनाराबाट ५० मिटरभित्र पर्छ। प्रभावहरूको वर्गीकरण तथा संक्षेपीकरण गर्न वातावरणीय तथा सामाजिक चेकलिष्ट प्रयोग गरिएको छ। निर्माण र सञ्चालन चरणहरूमा साइट-विशिष्ट प्रभावहरू यस प्रतिवेदनमा समावेश गरिएको छ। केही प्रभावहरू निम्नानुसार छन्।

उप आयोजना कार्यान्वयनबाट पर्ने सकारात्मक प्रभाव

यस उप आयोजनाले गरिब, जातीय अल्पसङ्ख्यक र विपन्न स्थानीयका लागि रोजगारीलाई प्राथमिकता दिई स्थानीय बासिन्दाका लागि रोजगारीको अवसर सिर्जना तथा आर्थिक सशक्तीकरणमा योगदान गर्नेछ। अन्य लाभकारी प्रभावहरूमा स्थानीय व्यवसायको वृद्धि, सुधारिएको पहुँच र मानिसहरूको सीप विकास समावेश छ। निर्माणमा सक्रिय रूपमा भाग लिने स्थानीय व्यक्तिहरूले बहुमूल्य सीप र अनुभव प्राप्त गर्छन्। यी सीपहरूले उनीहरूको व्यक्तिगत विकास र रोजगारमा योगदान पुर्‍याउँछ। सडक पूर्वाधारले बजार र सामाजिक सेवाहरूमा पहुँच बढाउन महत्वपूर्ण भूमिका खेल्छ तथा यस क्षेत्रका बासिन्दाहरूको लागि जीवनको राम्रो गुणस्तरको नेतृत्व गर्दछ। कुशल यातायात प्रणालीको उपलब्धताले शिक्षा, स्वास्थ्य सेवा, पर्यटन र अन्य सामाजिक-आर्थिक क्षेत्रमा सकारात्मक प्रभाव पार्न सक्छ। यसबाहेक, विस्तारित सडक सञ्जालले स्थानीय समुदायलाई फाइदा पुर्‍याउँदै जमिनको मूल्य वृद्धि गर्न योगदान पुर्‍याउँछ। उपआयोजनाको गतिबिधिहरूको परिणाम स्वरूप कुनै भौतिक वा आर्थिक विस्थापन हुने अपेक्षा गरिएको छैन।

उप आयोजना कार्यान्वयनबाट पर्नसक्ने नकरात्मक प्रभावहरु

निर्माण कर्मीहरूको अचानक वृद्धिले सार्वजनिक पूर्वाधार जस्तै सडक, खानेपानी आपूर्ति, परियोजना क्षेत्रको सरसफाइमा असर पार्न सक्छ। धुलो, ध्वनि र निर्माण मेसिनरीबाट निस्कने उत्सर्जनले वातावरणीय प्रदूषणमा योगदान पुऱ्याउन सक्छ । सवारी आवागमन, सडकको छेउछाउको नालीमा बगेको पानीले छेउछाउको खेतीयोग्य जमिनमा माटो कटान हुन सक्छ । यो सडक सञ्चालनमा आएपछि बस्ती क्षेत्र र बजार को विस्तार हुन सक्छ जसले सडकको क्षेत्राधिकारको अतिक्रमण बढाउन सक्छ। त्यस्तै सवारी साधनको धुलो, धुवाँ र धुवाँको वृद्धिले वायु र ध्वनि प्रदूषणलाई केही हदसम्म बढाउन सक्छ।

प्रभाव न्यूनीकरणका उपायहरु

यस प्रतिवेदनले आयोजनालाई वातावरणमैत्री बनाउने उद्देश्यले लाभ बढाउन र प्रतिकूल प्रभावहरु कम गर्न विभिन्न उपायहरु प्रस्ताव गरेको छ। रोजगारीका अवसरका लागि स्थानीय जनतालाई प्राथमिकता दिइनेछ । कामदारको बीमा गरी प्राथमिक उपचार सुविधा र पीपीईजस्ता सुरक्षा सामग्री उपलब्ध गराइनेछ । कामदारहरूलाई आचार संहिता लागू गरिनेछ । वातावरणीय संरक्षण र सामाजिक सुरक्षण सम्बन्धी जनचेतनामूलक कार्यक्रम सञ्चालन गरिनेछ। आयोजनामा १६ वर्षभन्दा कम उमेरका बालवालिकालाई काम लगाउन निषेध गरिनेछ। सडकको किनारामा बृक्षारोपणको लागि स्थानीय जातका बिरुवालाई प्राथमिकता दिइनेछ । वातावरण संरक्षण गर्न र सडकको छेउछाउका ढलानहरूलाई यथास्थितिमा स्थिर गर्न उपयुक्त बायो/सिभिल इन्जिनियरिङ संरचनाहरू अपनाइनेछ। सञ्चालनका क्रममा नजिकैको खेतीयोग्य जमिनमा पानी जम्न नदिन ढल निकासको उचित व्यवस्था मिलाइनेछ । सडक दुर्घटना न्यूनीकरणका लागि पर्याप्त सडक सुरक्षाका उपायहरु जस्तै सूचना र चेतावनी संकेतहरू स्थापना गरिनेछ। खानेपानीको पाइप लाइन र बिजुलीका खम्बाहरु अविलम्ब मर्मत सम्भार गरिने भएको छ ।

यौन शोषण तथा दुर्वसन एवम् दुर्व्यवहार रोकथाम तथा सम्बोधन कार्य योजना ;

विश्व बैङ्कले नेपाल शहरी शासकीय तथा पूर्वाधार उपआयोजनाको लागि गरिएको यौनिक शोषण, दुर्वसन एवम् यौन दुर्व्यवहार जोखिम मूल्याङ्कनमा आधारमा यस आयोजनाको SEA/SH जोखिमको "न्यून" मूल्याङ्कन गरेको छ । यस मूल्याङ्कनमा आधारित भई आयोजनाको लागि SEA/SH निरोध तथा सम्बोधन कार्ययोजना आयोजनाको लागि SEA/SH रोकथाम तथा सम्बोधन कार्ययोजना बनाइएको छ । यसमा उपआयोजनाको कार्यक्रमले सिर्जना गर्न सक्ने SEA/SH जोखिमहरु निषेध एवम् रोकथाम तथा न्यूनीकरण गर्ने उद्देश्यका निश्चित व्यवस्थाहरु समावेश गरिएका छन् । यस योजनाले तालिका-१; विश्व बैङ्कले सेप्टेम्बर २०१८ मा प्रकाशित "असल अभ्यास नोट" अनुसार IPF परियोजनाहरूमा SEA/SH जोखिमहरूलाई सम्बोधन गर्न सुझाएको कार्यहरु लाई पनि समावेश गरेको छ।

वातावरण तथा सामाजिक व्यवस्थापन योजना ;

पहिचान गरिएका मुद्दाहरू, सम्भावित प्रभावहरु, तिनीहरूको न्यूनीकरणका उपायहरु, अनुगमन विधिहरु सहित यो वातावरणीय र सामाजिक व्यवस्थापन योजना रिपोर्ट प्रस्तावित गरिएको छ। निर्माण र सञ्चालन चरणहरूमा वातावरणीय र सामाजिक प्रभावहरूको लागि न्यूनीकरण लागत प्रतिवेदनमा समावेश गरिएको छ। यसबाहेक पर्यावरणीय र सामाजिक न्यूनीकरण उपायहरु कार्यान्वयन गर्न र अनुगमनको लागि जिम्मेवार एजेन्सीहरु यस वातावरणीय र सामाजिक व्यवस्थापन

योजनामा पहिचान गरिएको छ। भौतिक, जैविक, सामाजिक, आर्थिक र सांस्कृतिक वातावरण सम्बन्धी विभिन्न अनुगमन सूचकहरू पनि पहिचान गरिएको छ । आयोजनाले पुनर्निर्माण र सञ्चालनको चरणमा सरोकारवालासँग मिलेर अनुगमन गर्नेछ । सरोकारवालाको जिज्ञासा र चासोको उठान र समयमै जवाफ दिनका लागि गुनासो निवारण संयन्त्र (GRC) मार्फत आयोजनासँग सम्बन्धित गुनासाहरूको सम्बोधन गरिनेछ। गुनासो समितिलाई लिखित, मौखिक, टेलिफोन, पत्र आदि कुनै पनि माध्यमबाट गुनासो उपलब्ध गराउन सकिन्छ र समितिले परियोजनाको प्रक्रिया अनुसार आवश्यक प्रक्रिया गर्नेछ र समाधान हुन नसकेमा माथिल्लो तहमा पठाउनेछ । उपआयोजना निर्माणका क्रममा GBV, SEA/SH जोखिमहरूलाई आवश्यक न्यूनीकरण गर्न योजना बनाइ आवश्यक सचेतना तथा तालिमहरूको ब्यवस्था गरिएको छ।

संस्थागत व्यवस्था ;

आयोजना कार्यान्वयन गर्न शहरी विकास मन्त्रालयले शहरी विकास तथा भवन निर्माण विभाग अन्तर्गत एउटा आयोजना समन्वय कार्यालय स्थापना गरेको छ । वातावरणीय तथा सामाजिक विधिको साथै सम्पूर्ण विधिहरू पालना सम्बन्धी जिम्मेवारीको जवाफदेहिता आयोजना समन्वय कार्यालयमा रहने छ । आयोजना समन्वय कार्यालयलाई एउटा आयोजना व्यवस्थापन सहयोग टोलीले सहयोग गर्नेछ । उपआयोजनाहरूको वातावरणीय तथा सामाजिक व्यवस्थापन योजना कार्यान्वयन स्थानीय तहमा गर्न र तथा अन्य वातावरणीय एवम् सामाजिक संयन्त्रहरूको कार्यान्वयनका जिम्मेवार हुने गरी नगरपालिकामा एक आयोजना कार्यान्वयन इकाइ स्थापना गरिनेछ । सुरक्षण विशेषज्ञ सहितको डिजाइन तथा सुपरिवेक्षक परामर्शदाता मार्फत प्राविधिक सहायता पुऱ्याइनेछ।

Acronym

BoQ	: Bill of Quantity
CBS	: Central Bureau of Statistics
CESMP	: Construction Environment and Social Management Plan
CoC	: Code of Conduct
DPR	: Detailed Project Report
DSC	: Design and Supervision Consultant
DUDBC	: Department of Urban Development & Building Construction
EA	: Environmental Assessment
EHS	: Environment, Health and Safety
EPR	: Environmental Protection Rule
ESMP	: Environmental and Social Management Plan
FGD	: Focus Group Discussion
ILO	: International Labor Organization
NUGIP	: Nepal Urban Governance and Infrastructure Project
OHS	: Occupational Health & Safety
OP/BP	: Operational Policy/Bank Policy
PCO	: Project Coordination Office
PIM	: Project Implementation Manual
PIU	: Project Implementation Unit
PPE	: Personal Protective Equipment
RoW	: Right of Way
SEA/SH	: Sexual Exploitation and Abuse/Sexual Harassment
STD	: Sexually Transmitted Disease
WB	: World Bank
WASH	: Water, Sanitation and Hygiene

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1. PROJECT INTRODUCTION

The Department of Urban Development and Building Construction (DUDBC) under Ministry of Urban Development (MoUD) of Government of Nepal has been executing the Nepal Urban Governance and Infrastructure Project (NUGIP) within the strategic framework for urban development envisaged in National Urban Development Strategy since the fiscal year 2077/78 B.S. The Project Development Objective (PDO) of NUGIP is to strengthen institutional capacity in participating municipalities for strategic municipal infrastructure and service delivery. In particular, NUGIP will aim at: a) improving access to core municipal services (includes expansion of coverage, and construction and rehabilitation of basic infrastructure systems, e.g., urban roads & storm water drainage etc) in participating municipalities; b) strengthening planning, budgeting and implementation systems for municipal service delivery; and c) strengthening municipal finances and financial management systems.

The DPR of *Upgradation Works of Sagarmatha Agriculture bazar west to south Madan Bhandari Hall to Janpremi tole Bisanpur ward 12 Road* (4.717Km) in Triyuga Municipality has been prepared as per the Contract between the Office of the Municipal Executive of Triyuga Municipality; Nepal Urban Governance and Infrastructure Project (NUGIP) (Client) and JV of Bosphorous Technical Consulting Corp (BOTTEK) and GOEC Nepal P. Ltd which have entered into the agreement, for performing work REF No: NP-DUDBC-216329-CS-QCBS to provide services on Detailed Engineering Design and Construction Supervision (DSC) covering the upgradation and rehabilitation works. The project is expected to contribute towards the municipal capacity for urban development planning, infrastructure development and institutional development of the municipality together with the improvement of livelihood of the local people along the settlement.

The study for Environmental and Social Management Plan (ESMP) of Upgradation Works of Sagarmatha Agriculture bazar west to south Madan Bhandari Hall to Janpremi tole Bisanpur ward 12 Road in Triyuga Municipality was conducted in September/October 2023. The study and preparation of the this ESMP is guided by the Environmental and Social Management Framework (ESMF) for NUGIP.

Sub Project Title	Upgradation of <i>Sagarmatha Agriculture bazar west to south Madan Bhandari Hall to Janpremi tole Bisanpur ward 12 road</i> (4.717Km)
Estimated Cost:	NPR. 294,147,294.73 (including VAT and contingency) ESMP cost of Sagarmatha Agriculture bazar road: 1,150, 000.00
Start/Completion Date:	March 2024 - July 2025

2. DESCRIPTION OF THE PROJECT SITE

Triyuga is one of the eight municipalities that make up the Udayapur district of Koshi Province. The municipality's headquarters is Gaighat. The name of the municipality is taken from the Triyuga River, which passes through the town. In terms of size, Triyuga is the third-biggest municipality in Nepal and the largest in Koshi Province (area). The municipality has an area of 547.43 square kilometers. It is located between 26° 41' 17''N' to 26° 56' 42'' north latitudes and 86° 32' 11.5'' to 86° 50' 29'' east longitude. The municipality is surrounded by Mahabharat hills in north and Churey hills in south. Udayapurgadhi and Rautamai are in north of the municipality, Khotang District is in north-east and Chaudandigadhi Municipality is in east. Saptari District is in south of Triyuga Municipality.

This city is in the famous Udayapur Valley. Gaighat (other name of Triyuga) is the shadow of a historic kingdom of Sen dynasty in Udayapurgadhi (Chaudandi), this Inner Terai Valley of south-eastern Nepal has a glorious history with a bright future. Gaighat is in the plane of the Valley. Udayapur Valley is the largest valley in eastern Nepal having fabulous natural attractions such as the Triyuga river, Baruwa river flowing through the town. Koshi River is east of the city. According to the census of 2078, there are 25623 HHs having total population of 102,725, with 48,463 males and 54,262 females. This municipality is divided into 16 wards.

2.1 Location of the project

Sagarmatha Agriculture bazar west to south Madan Bhandari Hall to Janpremi tole Bisanpur ward 12 Road is a rural road of Triyuga Municipality which starts at PG chowk of Madan Bhandari highway (Sagarmatha agri bazar) and passes through Buddha chowk, Ekta chowk, Purano laxmipur, Sonapur, Chiura mil, Baruwa, Bisanpur park and end at three locations namely Baruwa corridor, Bisanpur park and Saonapur. The overall length of the proposed road is 4.717 km. This road passes through ward no 12 and 13 of Triyuga Municipality. Its starting point coordinates are Easting 470878.9835m, Northing 2963083.1229m and elevation 195.592m end point coordinates are Easting 471719.2625m, Northing 2961019.3334 and elevation 189.744m. The proposed subproject is located in terai area and passes mainly through mixed land use having rural settlement and cultivated land.

2.2 Land use

The project area consists of different settlements having scattered settlements and cultivated land. The proposed roads are opened track and vehicles are moving throughout the road sections. The ROW was declared in 2055 BS by Municipality with the help of "Eastern area Directorate" under Ministry of Building and Physical planning. Government of Nepal. According to declared criteria-2055 by "Eastern area Directorate", the ROW of this road is 8m. The land within the proposed construction width which is the existing width is already in use by the public. The upgrading work for the road sub-project will be limited within the existing road width. Thus, additional land is not required for road upgradation. The project does not run through any ecologically sensitive area such as protected area, conservation area, wetland, national parks etc. No historical or archaeological sites are located within the project area. ROW declaration related documents attached in **Annex II** of this report.

Table 2-1: RoW details

SN	Name of the Road/ Sections	Length (Km)	RoW (m)	Existing width (m)	Proposed construction width (m)	Remarks (Cross section type)
Sagarmatha Road						
1.1	Section I	1.29	8	8	8	Type_3
1.2	Section II	2.17	8	8	8	Type_3
1.3	Section III	0.712	8			
1.3.1	0+000 to 0+040		8	6.3	6.3	Narrow section
1.3.2	0+040 to 0+680		8	8	8	Type_3
1.3.3	0+680 to 0+712		8	6.3	6.3	Narrow section
1.4	Section IV	0.545	8	8	8	Type_3

2.3 Topography and Hydrology

Triyuga Municipality located in the central part of Udaypur District is a commercial contact center between the Terai and the mountains and is an entry way of the Sagarmatha highway. It is an important connecting centre of the hill districts of Khotang, Okhaldhunga and Solukhumbu with Terai, Triyuga municipality. The geography of chure region in the south and Mahabharata range in the north to the Sunkoshi River covers a large area. In this municipality which has a heterogeneous structure, there are scattered and thinned settlements between the Mahabharat range and flat plains between Mahabharat range and chure area on the southern border. This municipality is considered as inner Madesh as it is a large long shaped land between Mahabharat range and chure range. The major rivers flowing through this municipality as Koshi, Triyuga and Baruwa. The altitude of the municipality is ranges from 70m to 2509m from sea level. The proposed road alignment lies in the Sub Himalayan range. The alluvial river deposit is the most predominant along the road alignment.

Road alignment does not cross any major river. Number of small seasonal and perennial small Hume pipe crosses the alignment at different Chainage which details are presented in **Table 2-3**. Department of Hydrology and Meteorology (DHM) data of 24-hour maximum rainfall for Gaighat (Index no. 1227) station is used for analysis of side drain and cross drain. Average annual precipitation of Gaighat is 131.88 mm. The latitude, longitude and elevations of the nearest station are shown in **Table 2-2**.

Table 2-2 :Details of Hydrology Station near Triyuga Municipality

Station Name	Index no.	Latitude	Longitude	Elevation (m)
Triyuga	1227	26°47'N	86°43'E	152

Table 2-3: Natural Drainage Existed Along the Road Alignment

SN	Name of the Road	Chainage	Natural Drainage	Adopted Structures	Remarks
Sagarmatha Road					
1.1	Section I	0+585	Crossing	0.6m dia Hume pipe	Drain Crossing towards Section II
1.2	Section II	1+487	Crossing	0.6m dia Hume pipe	Irrigation crossing
		1+612	Crossing	0.9m dia Hume pipe	Outlet toward river

2.4 Landslide and unstable slopes

As per field study, there is no any landslide, unstable slopes and flood prone area and other disaster-prone area present within the proposed alignment.

2.5 Climate and Vegetation

Udayapur has a Temperate highland tropical climate with dry winters climate (Classification: Cwb). The district's yearly temperature is 17.42°C (63.36°F) and it is -4.58% lower than Nepal's averages. Udayapur Garhi typically receives about 253.05 millimeters (9.96 inches) of precipitation and has 199.06 rainy days (54.54% of the time) annually. Monsoon rain occurs from the month of Ashad to month of Ashwin. Road alignment does not cross any major river and streams however rainwater is discharged in natural gully.

Since this road sections lies within the municipality city area (Purano Gaighat area) having scattered settlement and cultural cultivated land. There is no any forest and rivers within the alignments. Therefor there is no direct effect for the biological aspect of the environments. It is not a habitat for terrestrial fauna and avifauna. Major trees, wildlife and Aquatic species found in this municipality area are presented in **Table 2-4, 2-5 and 2-6.**

Table 2-4 Major tree species found in municipality area

SN	Scientific Name	Common Name	Use
1.	Ficus religiosa	Pipal	Hardwood
2.	Musa acuminata Colla	Banana (kyare)	Fruit
3.	Areca Catechu	Supari	Fruit
4.	Dalbergia Sissoo	Sissoo	Timber
5.	Magnifera indica	Aanp	Fruit/Fuelwood
6.	Melia azadirach	Bakaino	Fuelwood/Fodder
7.	Saraca asoca	Ashoka	Ornamental
8.	Psidium guajava	Amba	Fruit/Fuelwood
9.	Musa balbisiana	Banana	Fruits

Table 2-5 Major wildlife found in the municipality area

SN	Local Name (Nepali Name)	English Name	Scientific Name	Remarks
Wildlife: Animal				
1	स्यल	Wolf	Canis lupus	

SN	Local Name (Nepali Name)	English Name	Scientific Name	Remarks
2	बादर	Monkey	Macaca mulatta	
Major Birds				
1	काग	Crow	Corvus brachyrhynchos	
2	सुगा	Parrot	Phaethontidae Psittaciformes	
3	भगेरा	Sparrow	Ploceidae Passer	

Table 2-6 Major Aquatic species found in the municipality area

SN	Local Name (Nepali Name)	English Name	Scientific Name	Remarks
1	जुगे माछा	Gangetic mystus	Mystus cavasius	
2	बाम माछा	Cuchia	Monopterus cuchia	
3	भ्यागुता	Frog	Rana tigrina	
4	गगटो	Crab	Carcinus maenas	

2.6 Air Quality

Air quality of the project area was tested by instrument. The real time based 24 hours average TSP, PM10, PM2.5 and CO were 164.5µg/m³, 77.3µg/m³, 32µg/m³ and 230µg/m³ respectively at the site. The observed values are within the prescribed limit of NAAQS 2012 for the respective air quality parameters. The air quality test report is presented in **Annex V**.

The main construction activities that cause air pollution are earthworks excavation, asphalt plants operations and vehicle movement. These activities generate dust, which directly affect the air quality. In addition, vehicles and machinery emit smoke and fine particles. These substances will increase the local air pollution significantly during the construction stage. Burning of fossil fuels will result air pollution due to emission of sulfur oxides (SO_x), nitrogen oxide (NO_x), carbon dioxide (CO₂) and particulates.

For maintaining the quality of air in construction site, during construction, water -will be sprayed on the road surface and personal protective equipment (PPE) for the construction workers will be provided. The construction vehicles will be well maintained and will strictly comply with the GoN pollution regulation with compulsion in obtaining green sticker. Similarly, all construction plants should adhere to emission regulation. The vehicles carrying construction materials should ensure that it is well sealed and covered so as to avoid littering. The anticipated cost and specific conditions related to air pollution containment are included in the construction contract

2.7 Water Quality

The construction debris, paints, oil and grease are likely to create water pollution both surface and subsurface. The dust and silt from the construction sites will also create water pollution of the receiving streams. If workers living in tents/camps do not have access to toilet facilities, open defecation may be practiced, which may contaminate water sources, causing health problems. The anticipated impacts on water pollution such as

eutrophication, spreading of water born diseases, effect on downstream aquatic life will be direct in nature, low in magnitude, local in extent and of short-term in duration.

Spoil should be disposed off at designated spoil sites mentioned in CESMP and approved by PIU and DSC and efforts should be made to minimize such waste as far as possible through reuse, reduction, and recycling concepts. Similarly, the contamination of water by the use of cement and bitumen should be avoided and strongly monitored by contractors, PIU and DSC. The Contractor needs to arrange for sufficient water supplies and proper sanitation facilities for its labor force. Separate arrangements are necessary for work camp and labor camps. The anticipated cost and specific conditions related to water pollution containment are included in the construction contract

The water samples were collected from the project affected areas and tested in laboratory. The water quality report was found complied with National Drinking Water Quality Standards /GoN. The detail of water quality test report is enclosed in **Annex V**.

2.8 Noise level

The daytime and night time observed sound pressure level at the monitoring site were 54.2B(A) and 47.8dB(A) respectively. The observed daytime and nighttime average sound pressure levels complied the prescribed limits of GoN- Noise level standard 2069 for category urban residential area. The details report of air, water and noise are summarized in **Annex V**.

Noise impacts will be significant in the RoW and vicinity of the proposed subproject upgradation road during construction periods due to increase of vehicular movements and operation of machinery equipment. The heavy construction equipment movement in the site should be maintained in the day time only. Vibrations effects are closely observed and may need to do vibration activities by considering local requirement maintaining prescribed quality. For the safety of construction workers, PPE along with earplugs must be provided while on duty. The anticipated cost and specific conditions related to noise and vibration containment are included in the construction contract

2.9 Existing infrastructures/ utilities

The details of existing infrastructures situated within the construction width are presented in the tables below.

Water Supply pipelines

As per field study, consultation and discussion with the Municipality representatives and Triyuga Small Town Water Supply Committee, there is presence of existing water supply pipeline along the alignment. Two parallel pipelines are placed in section I of this road. It has been proposed that, water pipes are carefully handled during construction and necessary estimation for pipe relocation and new construction has been proposed in DPR and relocate/new pipelines will be laid in edge of road i.e under the footpath during the construction of road project. Necessary consultation with Triyuga Small Town Water Supply Committee was conducted during field study. The list of existing pipelines along the alignment are listed in **Table 2-7**.

For continuous flow of water, there are existing two sets of pipes running parallel to the alignment. Thus, arrangement should be made for continuous flow of one pipeline at a time during construction. So that, there will be minimal disturbance due to relocation/new construction of water supply pipelines during construction/relocation.

Table 2-7 : Existing water supply pipeline Details

S N	Name of the Road / Sections	Chainage		Side	Length (m)	Dia. of pipe	Type of pipe	Quantity (m)	Remarks
		From	To						
	Sagarmatha Road _ Section I	0+000	1+290	Both	1290	4''	CI	2580	
Total								2580m	

Religious Places, Resting Area and Community Owned Properties

The details of the public owned properties lie alongside of the road alignment are presented below. The below listed structures won't be affected by the project upgradation works however necessary precaution will be taken during construction works.

Table 2-8: List of temples, resting areas and other community owned properties

S.No.	Sagarmatha Road Sections	Chainage	Name	Type	Remarks
1.	I	0+000	Madan Bhandari Highway	Road	None of these structures will be affected.
2.	I	0+600	Shiva Mandir,	Temple	
3.	I	1+060	Rice mil way	Temple	
4.	I	1+290	Baruwa Corridor	Road	
5.	II	2+140	Bisanpur Harid Park, Bisanpur	Park	
6.	IV	0+545	Triyuga Corridor	Road	

Schools

Here is the list of schools which are lies along the alignment and near to the road alignment. The physical structures of these schools won't be affected by upgradation works. Necessary consultation with school management were conducted during field study, Footpath, necessary road safety signs such as zebra cross, sign board etc has been proposed. In these sections, the road is wider than other section to ease the student flow during school hours.

Table 2-9 Schools in and near the road alignment

S.N.	Chainage	School Name	Location	Remarks
1	0+620	Janapremi Basic School	Sagarmatha Road Section I	None of the structures of school will be affected during construction.

Existing Storm water drainage structures

Table 2-10: Existing storm water drain details

SN	Chainage		Direction of Sagarmatha Road	Remarks
	FROM	TO		
1	0+000	1+290	Section I (Left and Right)	Proposed for reconstruction
2	0+000	0+712	Section III (Left and Right)	

Existing Electric lines

In the whole alignment, the road section has 42 electric poles with in construction width of proposed sub projects which needs to be relocated during construction. Details of electric poles which needs to be relocated during construction are presented in below **Table 2-11**.

Table 2-11 Existing electric pole

SN	Chainage	Direction	SN	Chainage	Direction	SN	Chainage	Direction
Sagarmatha Road Section I			3	0+175	Right	19	2+005	Right
1	0+005	Left	4	0+210	Right	20	2+030	Left
2	0+050	Left	5	0+355	Right	Sagarmatha Road Section III		
3	0+100	Left	6	0+375	Right	1	0+005	Left
4	0+295	Left	7	0+385	Right	2	0+290	Left
5	0+350	Left	8	0+470	Right	3	0+350	Right
6	0+370	Left	9	0+505	Right	4	0+380	Left
7	0+480	Left	10	0+805	Left	5	0+395	Right
8	0+530	Left	11	0+830	Left	6	0+410	Left
9	0+910	Right	12	0+985	Right	7	0+460	Right
10	1+170	Right	13	1+290	Right	8	0+500	Right
11	1+220	Left	14	1+430	Left	9	0+660	Left
12	1+260	Left	15	1+675	Right	10	0+710	Left
Sagarmatha Road Section II			16	1+695	Right	Total	42	Nos.
1	0+005	Left	17	1+895	Right			
2	0+055	Right	18	1+950	Right			

2.10 Demography of the project area

The total population of Udayapur District, according to the Census of Nepal, 2021 (2078B.S.) conducted by National Statistics Office (NSO) is 340,721 and the number of households is 81,089. The average household size of the district is 4.20, which is lower than that of the national average (4.37). The total population of Triyuga Municipality is 102,725 and total household is 25623. The average household size of the Municipality is 4.01 which is lower than that of district household size (4.20). The male and female population of the municipality is 48463 and 54262 respectively and sex ratio is 89.31.

Table 2-12: Population and Household details of affected wards

Ward No.	House hold	Population			Average Household size	Sex ratio
		Total	Female	Male		
12	1882	7907	4137	3770	4.2	91.13
13	1638	6386	3404	2982	3.90	87.60

Source: NSO, 2021

2.11 Settlements

There is scattered settlement within the project area. The major settlement along the alignment is Sonapur, Ekta tole, Puma tole, Buddha tole, Rice mil tole and Bisanpur.

2.12 Land availability

The project area consists of scattered settlements and cultivated land. The proposed roads are opened track and vehicles are moving throughout the road sections. The ROW was declared in 2055 BS by Municipality with the help of “Eastern area Directorate” under Ministry of Building and Physical planning. Government of Nepal. According to declared criteria-2055 by “Eastern area Directorate”, the ROW of this road is 8m. The land within the existing width which is proposed construction width is already in use by the public. The existing width of the road is varying from 6.3m to 8m (including side drain and shoulder). The proposed width of the road also varying from 6.3m to 8m. However due to the availability of existing road width in a few locations, the construction width of the road is reduced as narrow section. The details are presented in **Table 2-1** (Section 2.2: Land use). The proposed road will be upgraded within existing road width only. Thus, there are no any built-up structure/permanent houses lies within the construction width of this road. There are no any impacts on structures, private or public/ temporary or permanent structures due to upgradation works. so, the additional land won't be necessary for this sub-project for the upgradation works.

2.13 Physical Structures

There are no any public as well as private structures present along the construction width of this sub-project. Therefor the alignment is clear for the construction of work.

2.14 Caste/Ethnicity

The caste/ethnicity wise details of households are presented in **Table 2-13**. There is presence of mixed groups of castes in the project area. The necessary consultations with these groups were conducted during field study. The consultations with different groups including IPs and Dalits were also organized during filed study. The consultation summary with different groups is presented in chapter 5 of this report.

Table 2-13: Caste/Ethnicity of People along proposed road alignment

SN	Ethnicity	HH no.	%	Remarks
1	Brahamin	40	10.96%	Khas/Arya
2	Chhetri	45	12.33%	Khas/Arya
3	Tharu	94	25.75%	IPs
4	Rai	74	20.27%	IPs
5	Magar	60	16.44%	IPs
6	Madheshi	22	6.03%	Khas/Arya
7	Biswokarma (Kami)	10	2.74%	Dalit
8	Mushar	20	5.48%	Dalit
Total		365	100	

Source: field study 2023

2.15 GBV, SEA / SH

During the field study, group discussions, interviews and meetings revealed that, there is gender-based violence in the project area. In the focus groups discussion people said that some domestic (mental) and economic violence is presence in the area. The municipal social section record also shows that GBV present within the Municipality. There are more than 20 number of consultations (KII, FGD and meetings) were conducted during preparation of ESMP. During the consultations, 134 numbers of people participated. The male and female participants are 83 (61.94%) and 51 (38.06%). Most of the local women were not aware

about the provisions of complaint registering through toll free number 1145 facilitated by National Women Commission. Details of violence present within the municipality are presented in the **Table 2-14** and related documents attached in **Annex-II**. According to municipal data, issues on GBV/SEA/SH are taken for planning of mitigation measures which are presented below. Necessary mitigation programs are planned and implemented during construction of sub project.

As a part of the project compliance, an Anti-Harassment Cell and GRC has been formed in Triyuga Municipality with dedicated focal person. **Mr Raj Kumar Khadka** is appointed for focal person who also serves as GBV focal person of the municipality. The letter regarding deputation of the focal person has been attached in **Annex II**. The main purpose of establishment of Anti-Harassment cell is to deal with cases of sexual harassment in a timely manner by providing empathetic support to the victim and ensuring prevention of harassment through proper redressal of the case. The cells process individual complaints and take appropriate action in accordance with the guidelines provided by the respective authorities.

Table 2-14 GBV cases details of Municipality

SN	Identified Cases	Number			Total	Remarks
		F/Y 2077-078	F/Y 2078-079	F/Y 2079-080		
i.	Physical Abuse	13	29	25	67	
ii.	Polygamy	3	6	5	14	
iii.	Railing (गालीगलौज)	4	8	20	32	
iv.	Mental Abuse	2	8	13	23	
v.	Character Abuse	6	2	3	11	
vi.	Resource violence	2	4	2	8	
vii.	Child Marriage	15	7	5	27	
viii.	Rape/Sexual violence	21	7	13	41	
ix.	Rape attempt	2	4	2	8	
x.	Sexual abuse	4	0	0	4	
xi.	Women trafficking	0	0	1	1	

Source: Municipality Social section, Triyuga Municipality

According to the above presented data, there is presence of different types of GBV within the municipality and project area. Therefore, issues on GBV, SEA/SH issues are taken for planning of mitigation measures. Necessary mitigation programs will be planned and implemented during construction of sub project. The most prioritized issues on violence are

- Physical & mental abuse
- Sexual violence/rape cases
- Child marriage

For mitigation of SEA/SH, and GBV during the construction phase, the following activities are proposed and will be implemented which needs to continue in the operation stage also. For carrying out these activities, the mitigation action plan is prepared, budget is estimated and included in DPR. The overall responsibility of carrying out these activities will be on PIU/DSC and contractor based on the activities.

- Awareness programs related to community and workers, for reduction of **sexual violence/rape**

cases.

- Awareness Program for **physical/mental abuse** in the project area.
- Awareness raising in Schools on **Child marriage**.
- Establishment of Anti-Harassment Cell in Municipality and assign a Focal Person in the Municipality.
- Conduct an orientation program on code of conduct for contractors and subcontractors to manage GBV/SEA/SH risks during construction phase and Implementation of Code of Conduct on SEA/SH behavior and the provision of punishment for breaching of the code of the conduct.

3. DESCRIPTION OF THE PROJECT AND ITS ACTIVITIES

The upgrading of Sagarmatha Agriculture bazar west to south Madan Bhandari Hall to Janpremi tole Bisanpur ward 12 Road starts at PG chowk of Madan Bhandari highway (Sagarmatha agri bazar) and passes through Buddha chowk, Ekta chowk, Purano laxmipur, Sonapur, Chiura mil, Baruwa, Bisanpur park and end at three locations namely Baruwa corridor, Bisanpur park and Saonapur. The overall length of the proposed road is 4.717 km. This road only passes through ward no 12 of Triyuga Municipality. The settlement of this municipality lies in the plain Area of inner Terai. The road section requires pavement reconstruction to maintain acceptable levels of service. There are no alternative routes to the project road that serve the same function as that of the stated road.

The ROW was declared in 2055 BS by Municipality with the help of “Eastern area Directorate” under Ministry of Building and Physical planning. Government of Nepal. According to declared criteria-2055 by “Eastern area Directorate”, the ROW of this road is 8m. The existing width of the road is 6.3m to 8m (including side drain and shoulder). The proposed width of the road also 6.3m to 8m having carriageway width 5.5m. The components of the proposed roads are improvement of pavement, footpath, kerbs, covered drains, road crossings, retaining wall/breast wall, zebra crossing, shifting of electric poles and improvement of the road furniture.

3.1 Salient Features of the Project

Salient features of the proposed Sagarmatha Agriculture bazar west to south Madan Bhandari Hall to Janpremi tole Bisanpur ward 12 Road sections (Section I to Section IV) are shown below **Table 3-1**.

Table 3-1: Salient features of Sagarmatha Agriculture bazar road

SN	Description	Description	
1	Name of the Road	Sagarmatha Agriculture bazar west to south Madan Bhandari Hall to Janpremi tole Bisanpur ward 12 Road	
2	Road Type	Urban/ Local Road (NURS 2076)	
3	Proposed road length	4.717 Km	
4	Number of Lane	Two Lane	
5	Right of Way	8m	
6	Road Attributes	Existing width	Proposed width
	• Road Width	6.3m for Narrow Section (i.e. CH 0+000~CH 0+040 and CH 0+068~CH 0+712); 8m for remaining Section (including drain and shoulder)	6.3m for narrow section and 8m for remaining section
	• Carriageway Width *	4m for Narrow Section and 5.5m for remaining Section	3.8m for Narrow Section and 5.5m for remaining section
	• Camber of Carriage way	2.5%	
	• Pavement Surfacing	40 mm Asphalt Concrete 0.15 m Base	

SN	Description	Description
		0.15 m Sub-base
7	Terrain Type	Plain
8	Climate	Tropical
9	Geology	Sub Himalayan
10	Wards & Major settlements	Triyuga Municipality –12 and 13 Sonapur, Ekta tole, Puma tole, Buddha tole, Rice mil tole and Bisanpur
11	Design Parameters	
12	Design speed of Road	30 km/hr
13	Design Life of Road	20 Years
14	Maximum Gradient	4%
15	Minimum Gradient	0.35%
16	Total cost of EMP	NPR. 1150,000.00
17	Total Project cost	NPR. 294,147,294.73 (including VAT and contingency)
18	Cost per km	NPR. 62,358,977.05 (including VAT and contingency)

* **Note:** *Right of Way- RoW defined by the municipality;*
Carriageway Width- Black topped road width.

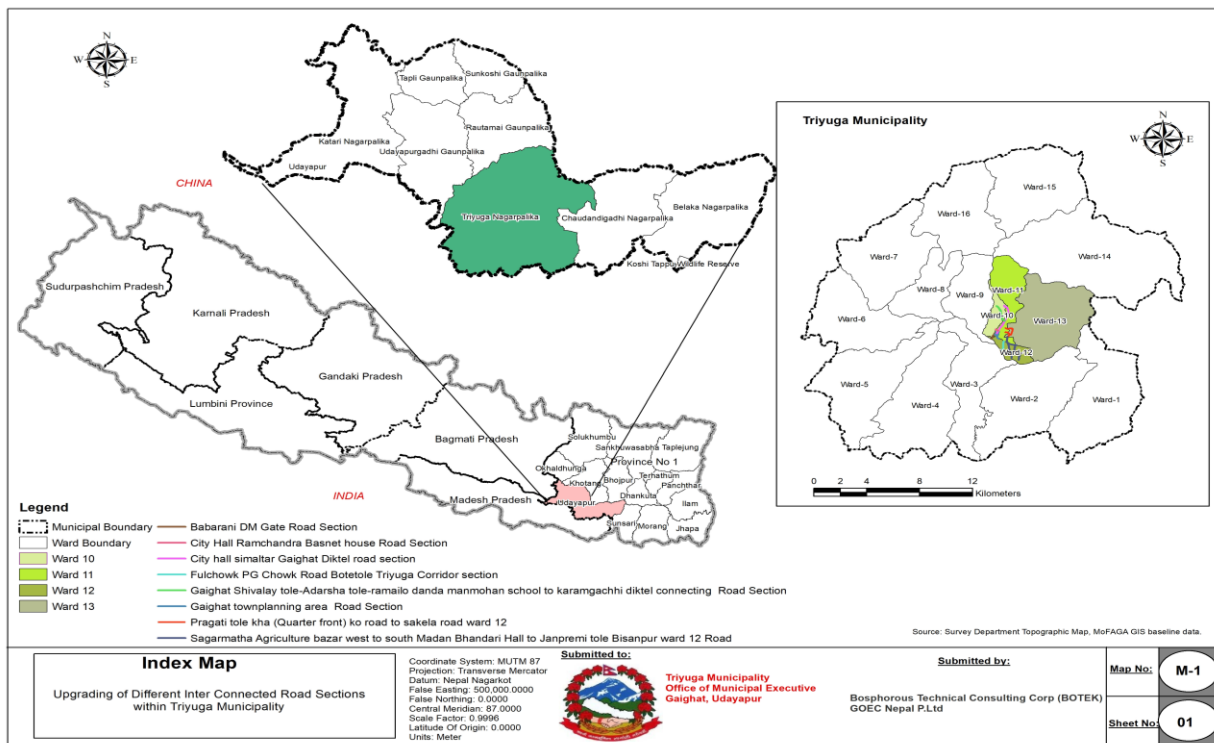


Figure 3-1 : Index Map

3.2 Proposed Campsite and Stockpile Area

During the field study, the campsite and stockpile area has been identified. The proposed campsite and stockpiling area are a government land having an area of about 31436 Square meters which is situated near the north side of proposed Baruwa Bus Park (Sakela Area). It is 600m far from the proposed site. It is an open

ground area situated near Sakela Tole and can be used for contractor’s campsite and stockpiling of the construction materials. The existing land is barren land having some vegetation. Required facilities like separate male and female camps and toilets will be constructed during construction phase. Necessary consultation with municipality along with locals were conducted with presence of Mayor. The minutes related to consultation is attached in **Annex II**.

Table 3-2: Campsite and Stockpiling location details

SN	Description	Easting	Northing	Available Area (Sqm)	Remarks
1	Camp site	471259.33m E	2963533.47m N	2884	
2	Stockpile area	471252.25 m E	2963346.15 m N	7289	
3	Solid Waste management area	471255.50m E	2963501.14m N	50	Within camp area



Figure 3-2: Proposed camp and stockpiling site

3.3 Solid Waste Management

Solid waste generated from the labor camps will be disposed within the proposed camp site. Soak pits or septic tanks will be established for the sanitation units/latrines at appropriate location within campsite. As the labor camp/stockpiling area is big, contractor will use one point (471255.50m E, 2963501.14m N), tentative area 50sqm) for solid waste management. Also, the waste generated during decommissioning of the temporary campsite, the reusable like cardboards, plastics, bins, etc. will be sold, the metal scrap will be sent or sold to scrap dealers, and any residue will be disposed off in coordination with the local ward/municipal authority through existing solid waste management (collection & disposal) system of the municipality. The land will be cleared and restored to the satisfaction of the Municipality.



Picture: 1 : Solid waste management area

3.4 Spoil Management

As per the DPR of the road, there is earth cutting (5956.797m³) is more than filling work (5927.343m³). After the backfilling, 29.454m³ construction spoil will need to be managed in appropriate site. An open public space located around 500m from proposed road section II end point (near Bisanpur Harid Park ward-12) with area of around 10,000 sqm has been proposed as spoil disposal site which is the government land and far from the nearby settlement. Municipality letter related to spoil management site is attached in **Annex II**. Construction debris will be disposed at designated spoil site only and efforts will be made to minimize such waste through reuse, reduction, and recycling concepts. While hauling and storing spoil temporarily, spoil will be covered with plastic/tarpaulin cover. Construction trash should only be disposed of at approved spoil sites. The land will be cleared and restored to the satisfaction of the Municipality by following environmental obligations.

Table 3-3: Details of spoil generated by sub project

SN	Particulars	Cutting (m3)	Filling (m3)
1	Earthwork	5956.797	5927.343

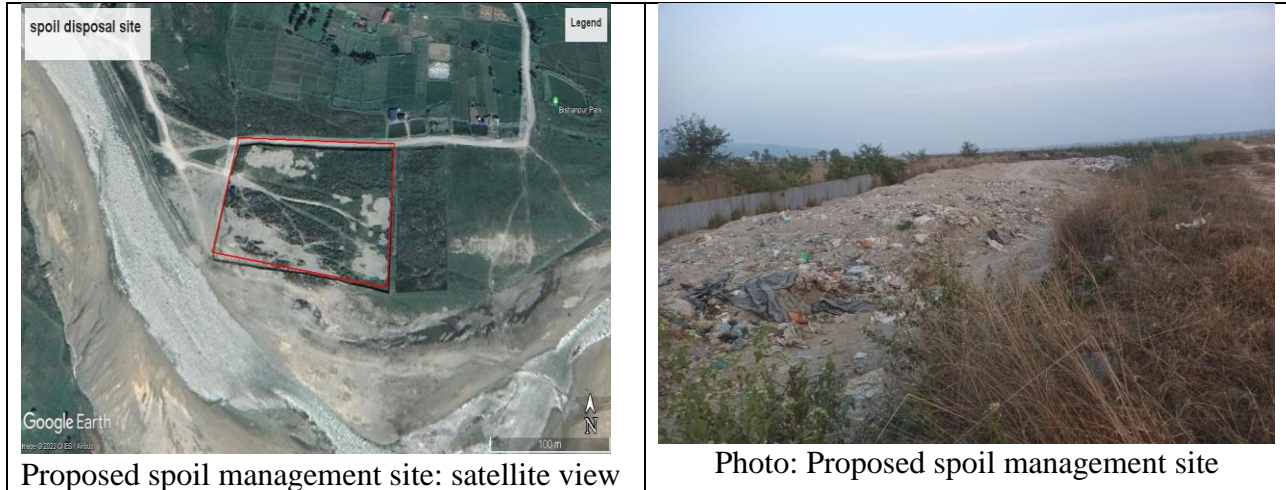


Figure 3-3: Proposed spoil management site

3.5 Quarry Sites

The upgrading of road will require boulders, sand and aggregate for activities like gravelling, construction of retaining walls, cross drainage, side drains, road furniture and other structures. These construction materials will be brought from the established quarry sites of municipality and crusher industry nearby the municipality. These construction materials will be brought from the IEE approved quarry sites at the Baruwa Khola, Triyuga khola and Sunkoshi khola (which have already received the environment clearance, IEE approval) within the Municipality. The details of quarry name and available quantity are presented in **Table 3-4** and the quarry site is shown in below **Map (Figure 3-4)**.

Table 3-4: Quantity of Sand, Aggregate and Stone required for the sub project construction

SN	Particulars	Quantity (m3)	Remarks
1	Sand	1548.49	
2	Stone	0	
3	Aggregate	15292.492	

The Contractor may also obtain required construction materials from the legally operating crusher industries other than proposed quarry sites. So, the direct impact of quarries such as damage to cultivated land, high level of air pollution, noise pollution, cracking of houses nearby structures by heavy equipment movement, affect to the downstream aquatic life etc are not expected in this subproject. However, the quarry sites and amount of quarrying material will be included in Construction Environment and Social Management Plan (CESMP) within 45 days of commencement of works. PIU will check the site requirements and quality of quarrying material and approve it. DSC will also monitor whether the quarry sites has been legally operating or not.

Table 3-5: IEE Approved Quarry site description

SN	Quarry Name	Ward No	Northing	Easting	Available quantity (m3)/year	Remarks
1	Babari (Rato Pahiro)	10	26°49'18.08'' to 26°49'8.37'' N	86°42'1.40''E to 86°42''7.47''E	9660	Approved IEE of these sites is not

SN	Quarry Name	Ward No	Northing	Easting	Available quantity (m ³)/year	Remarks
2	Bisanpur	12	26°46'36.17''N to 26°46'16.76''N	86°43'11.18''E to 86°43'26.15''E	25375	mentioned separate volume of the stone, aggregate and sand to be available
3	Sakela	11, 13	26°48'5.09''N to 26°47'36.63''N	86°42'39.59'' E to 86°42'45.32''E	38080	
4	Dimki	11	26°48'25.65'' N to 26°48'6.42''N	86°42'38.34'' E to 86°42'42.59''E	14280	

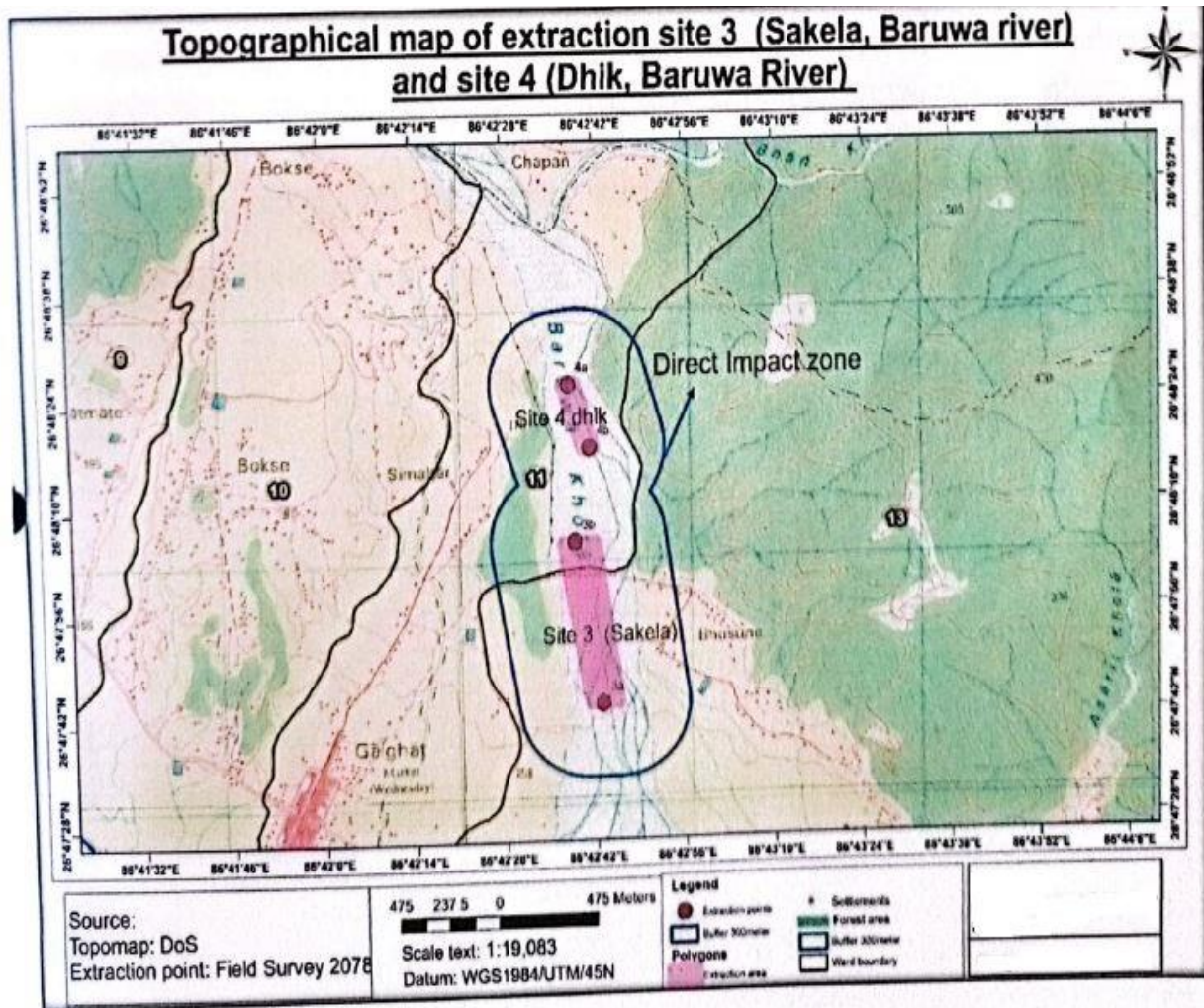


Figure 3-4 : Topographic map of Quarry site

3.6 Borrow Pits

As per DPR, the quantity of earthwork cutting (5956.797m³) is more than earth filling work (5927.343m³), thus there is no need of borrow pits in this sub project however for ease and future possibility of requirement, borrow pits has been identified. The borrow pit is proposed at the Triyuga River which is 500m (Easting 471634.00m E, Northing 2960654.00m N) far from our road chainage 2+140 of section II (near Bisanpur Harid Park) and having IEE clearance by municipality. The letter of municipality for about borrow pits is attached in **Annex II** of this report.

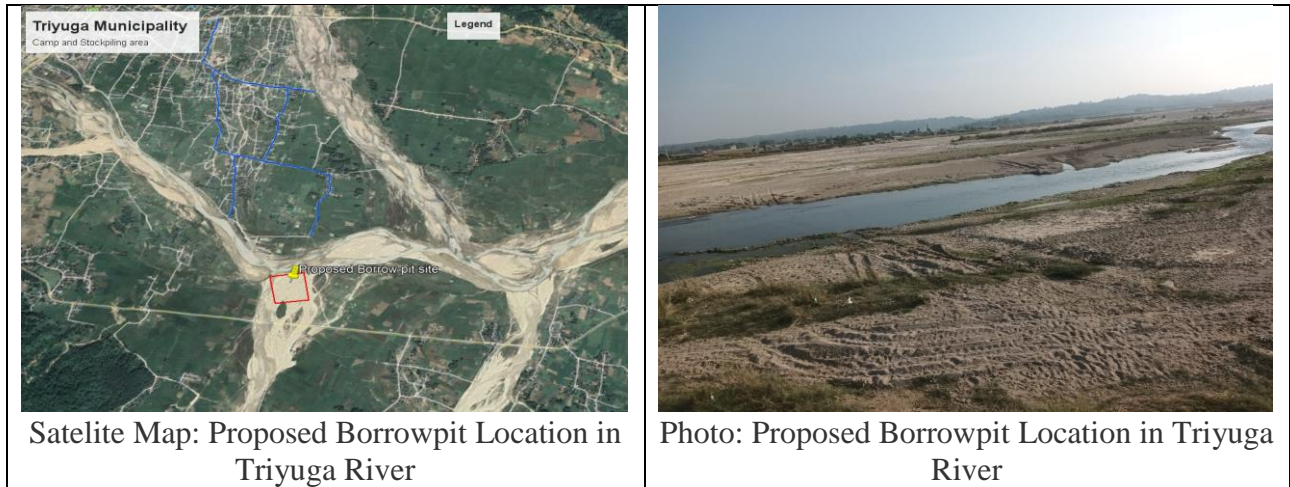


Figure 3-5: Proposed Borrow Pits site

3.7 Other facilities

The facilities like workshop area, concrete batch plant will be established during construction of road. The site is identified during filed study. It is proposed near the bank Baruwa Khola (Easting 472352.00m E, Northing 2961269.00m N) which is 635m far from our site and is a government property. Necessary consultation with Municipality was conducted and minutes is attached in **Annex II**. Further, CESMP will be prepared by contractor contracted within 45 days of contract. Thus, facilities like Workshop area, concrete batch plant establishment details will also be included in contractors CESMP.



Figure 3-6 Proposed other facilities establishment site

4. ENVIRONMENTAL AND SOCIAL MANAGEMENT PLAN (ESMP)

4.1 Social and Environmental Screening:

The Environment and Social Management Plan (ESMP) including monitoring plan provides a road map on how to address the anticipated negative environmental and social impacts associated with the implementation of the proposed development project throughout its project life cycle. The outline of the plan considers the anticipated impacts, the mitigation measures, the parties responsible for the action, time frame for the implementation and cost to address them. The monitoring process aims at generating useful information that would be useful in facilitation of the implementation of the mitigation measures.

The implementation of the ESMP shall be the responsibility of both the Municipality/PIU/DSC and contractor. Proponent will play both implementor and supervisory role for ensuring proposed measures are implemented as outlined in the plan. The frequency of implementation and monitoring of the proposed measures shall be dependent on the nature of the anticipated impact. The cost for the implementation of the ESMP during the construction phase will be factored in the bills of quantities (BOQ) while the cost after completion will be met by the PIU.

The proposed sub-project is an upgradation of an existing road. By using approved format, A social and environmental screening of the sub-project has been done to assess any potential risk associated with the road upgradation work. The checklists of screening of social and environmental safeguards are provided in **Annex I**. The screening shows that the project area doesn't have major social or environmental risks. The land required for road construction is already available and is in public use. Documents regarding declaration of the RoW by the Triyuga Municipality is provided in **Annex II** of this report. As per people consultation during the field study, there are no any disputes regarding land ownership, existing road width and grievances. As per section B & C of Appendix C of Environmental and Social Management Framework (ESMF, August 2020) of NUGIP, it can be concluded that the proposed road upgradation project falls under 'Low to Medium impact project' under Category B. Hence an abbreviated ESMP is sufficient for the proposed project. This ESMP document has been prepared as per Appendix C of the ESMF.

4.2 Scope of Environmental and Social Management Plan:

The scope covers potential impacts, related mitigations and monitoring aspects related to activities under the proposed road upgradation works in Triyuga Municipality. These are summarized in **Table 4-2** below.

4.3 Implementation of Environmental and Social Management Plan

The sub-project ESMP implementation arrangements can be summarized as follows;

Table 4-1: Roles and responsibility for ESMP implementation

SN	Stakeholder	Role & Responsibilities for ESMP implementation
1	Triyuga Municipality/PIU	<ul style="list-style-type: none"> ✓ The overall project environmental and social safeguard management is the responsibility of PIU ✓ The regular monitoring will be carried out by the PIU
2	Contractor	<ul style="list-style-type: none"> ✓ The contractor is required to submit C-ESMP within 45 days of contract signing.

SN	Stakeholder	Role & Responsibilities for ESMP implementation
		<ul style="list-style-type: none"> ✓ Implement the mitigation measures and provisions as per ESMP and DPR of the Project
3	DSC	<ul style="list-style-type: none"> ✓ Preparation of ESIA/ESMP during DPR phase ✓ Supervision support to the Contractor to implement the ESMP ✓ Monitoring of implementation of ESMP and its compliance ✓ The E&S safeguards specialists of DSC will work closely with its technical staff to ensure project implementation in accordance to World Bank's safeguard standards.
4	PCO & PMST	<ul style="list-style-type: none"> ✓ The PCO will have overall responsibility to ensure compliance with pertaining laws, policies, regulation for all sub projects ✓ The PCO with support from PMST will review implementation support of environmental and social safeguard studies/ management plan prepared by PIU/DSC.

4.4 Objectives of ESMP:

The overall objective is to ensure that the environment and its surrounding areas are protected and developed to meet the needs of the local stakeholders and safeguard the requirements of the local people. The basic objectives of the ESMP are to:

- To ensure that all mitigation measures and monitoring requirements will actually be carried out at different stages of project implementation and operation - pre-construction, construction, and operation and maintenance;
- Establish the roles and responsibilities of all parties involved in the project's environmental and social management; and
- Ensure the budget required for implementation of recommended actions aimed at environmental and social management and its enhancement.

4.5 ESMP Matrix:

Following project interventions and related mitigation measures have been planned in relation to the existing status and potentials improvements for the proposed road alignments;

Table 4-2: Environmental and Social Management Plan (ESMP) Matrix

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
Physical-Construction				
1.	Obstruction due to electric poles with in construction width (42 Nos.)	<ul style="list-style-type: none"> Obtain all necessary permits for dismantling and relocation of electric poles from NEA. The cost of shifting should be included in project cost Relocate the electric poles along the alignment in coordination with the Nepal Electricity Authority The process should be completed prior the beginning of the road construction 	Municipality/DSC and Contractor in coordination with NEA.	Included in DPR BOQ (BOQ item no: A-2)
2.	Relocate and re-construction of Water Supply Pipe lines of the Triyuga small town water scheme (2.58km pipelines likely to affected)	<ul style="list-style-type: none"> Obtain permission (Obtained necessary estimation from relocation of pipes from water management committee) for relocate and reconstruction from Water Supply Management Committee. The project must work in close coordination with the Water Supply Management Committee regarding disruption of water supply system in road section, Alternative means of supply during the disruption and reconstruct the system will be arrange Details in section 2.9. 	Municipality/DSC and contractor in coordination with water supply management committee	Included in DPR BOQ (BOQ item no: A-2)
3.	Quarrying Material and Operation	<ul style="list-style-type: none"> Avoid and or minimize adverse environmental impacts arising out of quarry construction material exploitation in line with MOFE guidelines/ conditions / recommendations. Include conditions for selecting borrow sites, timing and use of roads, maintenance of vehicles, selection of sites 	Municipality/DSC, Municipality instructs the quarry operators to reinstate the established quarry sites as per	Covers by municipality/PIU DSC monitoring cost

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
		<p>for material storage, rock blasting and aggregate production, handling hazardous or dangerous materials such as oil, explosives and toxic chemicals.</p> <ul style="list-style-type: none"> • The construction materials will be brought from the established quarry sites located within or outside the municipality. So, the direct impact of quarries is not expected in this Subproject. • The municipality in support of DSC will monitor the quality of quarrying material and state of quarry sites. • The materials will be brought only from licensed vendors having environmental clearance. • Municipality may instruct the quarry operators to reinstate the established quarry sites as per agreed norms during environment clearance. • Prepare a CESMP and include the details of quarrying activities including required quantity, locations and required mitigation within 45 days of commencement of works and submit to the PIU for approval. 	<p>agreed norms during environment clearance (Follow IEE obligations)</p>	
4.	Issues of stockpiling and construction material	<ul style="list-style-type: none"> • Locate and seek approval from the supervising consultant for the use of stockpile sites. The proposed campsite and stockpiling area are a government land having an area of about 31436 Square meters which is situated near the north side of proposed Baruwa Bus Park (Sakela Area). It is 600m far from the proposed site. • Stockpile should not be located on water courses; should not be within 50m of schools, hospitals or public standpipes; and should not affect locals and their properties. 	Contractor	This will be the part of Contractor's Responsibility under Contractor Cost. This will be included in CESMP

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
		<ul style="list-style-type: none"> • Obtain written permission from landowners and local bodies for stockpiling on their land. • Only barren land will be used for stockpiling and proper insulator cover and proper drain will be managed to store the chemical to avoid the leakage of chemicals. • Stock of sand will be set wet to prevent it from blowing with the wind; water sprinkler will be used for this purpose. • The places used for the stockpiling of construction materials will be cleaned promptly after the completion of the project. • The area could be leased or rented based on price not lower than the prevailing market price 		
5.	Borrow Pits	<ul style="list-style-type: none"> • Earth material Quantity required of the filling works, sub-grade, base and sub-base. • The construction materials will be obtained to supply and borrow pits must be legally operating with IEE approval from the concerned authority. • Implementation for the monitoring will be carried through contractor environment mitigation checklist which is prepared before the construction of structures • Reclamation of borrow pits, landscaping and tree plantation along the excavated borrow pits. • The borrow pit is proposed at the Triyuga River which is 500m (Easting 471634.00m E, Northing 2960654.00m N) far from our road chainage 2+140 of section II (near Bisanpur Harid Park) and having IEE clearance by municipality. 	Contractor	This will be the part of Contractor's Responsibility under Contract Cost. This will be included in CESMP

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
6.	Spoil Disposal	<ul style="list-style-type: none"> Disposal of spoil into water bodies will be strictly prohibited and Generated spoil will be disposed only at designated spoil disposal sites. Open burning of solid waste from the Contractor's camps should be strictly banned The scarified spoil will be disposed within the RoW width along the alignment during the construction and required additional filling quantity is direct transport from the borrow pit site which is legally operating with IEE approval from the concerned authority. Implementation for the monitoring will be carried through contractor environment mitigation checklist which is prepared before the construction of structures The identified spoil deposit site is an open public space located around 500 m from Sagarmatha agri road section II end point (near Bisanpur Harid Park ward-12) with area of around 10,000 sq.m has been proposed as spoil disposal site which is the government land and far from the settlement. All the identified spoil disposal sites will be rehabilitated after disposal adopting natural drainage, and covering vegetation. 	Contractor	will be the part of Contractor's Responsibility under Contractor Cost
7.	Air/Dust Management	<ul style="list-style-type: none"> Road construction area shall be maintained damp by periodical spray of water. Bisanpur area will be needed for water spray during construction. Delivery vehicles will be covered. Mixing equipment will be well sealed and equipped as per existing standards. 	Contractor/PIU/DSC	Included under Contractor's Cost The cost for the water sprinkling will be borne by the contract package.

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
		<ul style="list-style-type: none"> All construction vehicles should comply with Motor Vehicles and Transportation Management Act 2049 (Third Amendment) Rules, 2066 (2010). Provide temporary hoardings where required to minimize dust impact on locations of temples (Shiva Mandir ch 0+600, section I, Bisanpur Harid Park chainage 2+140, section II), and Janapremi basic school(Chainage 0+620). Provision of speed control measures in Bisanpur area as well as Janapremi basic school area and working areas to limit traffic speed. Dust emission and air pollution due to construction activities and operation of heavy equipment and movement of transporting vehicles, to mitigate the impacts water will be sprinkled along the proposed road alignment and nearby dust prone area and repair and maintenance of equipment and vehicles regularly. Air pollutant parameters (TSPM, PM10, Sox, NOx, Cox) will be monitored regularly during construction. Conforming NAAQS of Nepal. Air quality monitoring/Sampling (at least 2 times during construction) 		(For Air quality test/sampling, Noise level monitoring and water quality test NRs 250,000)
8.	Noise, vibration due to the use of heavy machinery and other construction activities,	<ul style="list-style-type: none"> Vehicles and equipment used will be fitted with silencer and maintained to keep noise at minimum levels and Select equipment and machinery with lower sound power levels for the use. Workers will be provided with appropriate ear muffs/plugs specially at crusher site 	Contractor/DSC	Cost of noise level monitoring comes within the Air Quality monitoring cost above Costs as a result of damage from vibrations will be borne by contactor.

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
		<ul style="list-style-type: none"> • Sensitive locations i.e., Janapremi basic school, Bisanpur Harid Park area etc. will avoided while placing the noise generating equipment. • Restrict activities with significant noise impacts to outside school (no horn sign). • Activities involving heavy machinery with significant noise impacts should be restricted to outside school hours. • Noise levels (1 hr Leq dB(A)) levels will be monitored regularly. Conforming WHO standards. 		
9.	Water Pollution	<ul style="list-style-type: none"> • Hazardous materials shall not be stored near surface waters sources • Used lubricants and oils shall be collected and recycled or disposed off site. • Plastic sheeting shall be placed under hazardous material storage area to collect and retain leaks and spills. • Contaminated runoff from storage areas shall be captured in ditches or ponds with an oil trap at the outlet. • Contaminated and worn plastic sheeting shall be packed into drums and disposed off site. • Water Quality (EC, PH, DO, TSS, Oil and Grease). Conforming WHO standards. 	Contractor/DSC	Cost of water pollution monitoring comes within the Air Quality monitoring cost included in above SN 7 of this table
10.	Labor Camp Location and Management	<ul style="list-style-type: none"> • Camps area should be well fenced having entrance gate and provided with a 24-hour guard having access to entry for authorized personnel only. Also, will maintain record of labor in proper way. • Camps shall not be located near settlements; near water supply intakes; or sites that affect the access by local people to drinking water. 	Contractor	Included in Contractor's Cost, it is the responsibility of Contractor.

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
		<ul style="list-style-type: none"> • Camp shall not be in the vicinity of landslide and flood plains. • Provide adequate health care facilities within construction sites and provide first aid facility round the clock • Install sanitary facilities for workers to avoid open defecation by construction of temporary toilet. • Separate Camps shall be constructed for Male and female workers with separate, safe and easily accessible facilities like separate toilets, Locker room etc. • Provide and maintain proper drinking water, sewerage and waste disposal facilities at the camps. • Camps shall be provided free of cost, with electricity and regulator & adequate fuel supplies of LPG or Kerosene. • After use, sites shall be cleared and restored to near natural or stable conditions with vegetative cover. 		
11.	Solid waste generation/ Waste from labor camp	<ul style="list-style-type: none"> • Waste minimization and waste segregation will be prioritized; 3R approach will be promoted. • Composting of organic waste generated from the camps will be disposed within the proposed camps. • Coordination with municipality for final disposal into the municipality's waste collection & disposal system • Awareness raising event will be carried out. • Decommissioning waste will be re-used, sold to local scrap dealers. Every week, it will be monitored for the management of waste from the camp site. • Further detailed plan will also be provided in C-ESMP with in 45days of contract agreement. 	Contractor	<p>Included in Contractor's Cost, It is the responsibility of Contractor.</p> <p>For awareness raising activities NRs 25,000</p>

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
12.	Road safety and Community safety	<ul style="list-style-type: none"> • Cross-Roads: Development Stretch of up to 15 m of each cross-road will be developed under this project. • Installation of Road markings at all major as well as minor intersections. Road Signs and Markings Road Markings has been provided as per Traffic Sign and Marking manual as per DPR • The signage/hoarding board needed area are Bisanpur and Janapremi basic school area. • Retaining/ Breast wall: Stone Masonry Retaining wall has been provided along the alignment where embankment is required. • Awareness activities (minimum one event, minimum participants 30nos.) will be conducted to inform & aware locals regarding potentials risks and proposed safety measures related to the project activities 	DSC/Contractor	Included in DPR BOQ (BOQ item no: B-7, H-41, H-42, H-43, H-44, H-46, H-47, H-48)
13.	Construction Safety	<ul style="list-style-type: none"> • Provide barricades and green net in sites for restriction of local people movement during construction in densely populated area such as Bisanpur, Janapremi school area. • PPE (helmets, masks, boots, gloves, ear plugs and goggles) should be provided for workers safety • Storm water collected should be disposed through the nearest culvert sections through side drains. • The contractor will assign a safety officer and the PIU's safeguard specialist will monitor the implementation of the OHS measures. • Adequate lighting and safety signal devices be installed for work safety. 	Contractor	<p>It will be the part of Contractor's Responsibility under Contract Cost</p> <p>NRs 50,000 for worker safety materials.</p>

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
		<ul style="list-style-type: none"> • Adequate warning signs, safety barriers, traffic calming measures and persons with flags to control traffic will be provided for work safety. • Protective clothing including helmets, masks, boots, gloves, ear plugs and goggles should be provided for workers safety. • At every work place, a readily available first aid unit including an adequate supply of dressing materials will be provided. • Maintain health care system at construction camps including regular visits by trained medical staff for routine checkup of workers and avoidance of communicable disease. • Temporary diversions will be provided wherever necessary, with proper drainage facilities. • Electrical Equipment will be checked and certified regularly. • Provide and install all road signs as per design. • Impart road safety education to all community, schools, clubs and drivers of construction vehicles. • Hazards will be identified, and workers will correctly wear PPE, will properly use safety equipment, and will follow work safety arrangements. Safety signs and information will be provided and the work space will be barricaded to prevent unauthorized entry. Workers and people at the construction site will be provided with proper training, and to help ensure that workers are trained on what to do in the event that an accident occurs on site. 		

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
14.	Traffic Management	<ul style="list-style-type: none"> • Emergency traffic management plan should be included in CESMP by the contractor and approved by the PIU/DSC. This is required to cope up with the restriction on the vehicular movement due to closure of road for reasons including construction. The plan may include informing about the scheduled road closure and the alternative routes identified to divert the normal traffic flow, transport material during off-peak time, • provide advance notice to stop vehicles by erecting indicator signs at a necessary distance in order to reduce congestion at the site of work, thus enabling making of proper security arrangements, or lane wise traffic management. • Traffic management for the Janapremi Basic School, Chainage 0+620, Section I will be maintaining traffic diversion for the teachers/students within the sub-project up to construction period. Also, alternative routes will be identified. • Traffic Safety such as street lights, traffic control devices and other features shall be covered through “Traffic Signs Manuals Vol-I and Vol II” and “Road safety manual” published by the DOR. • Provision of alternative routes to ease the congestion and built up of traffic. 	PIU/ DSC and Contractor	Included in DPR BOQ (BOQ item no: B-7, H-41, H-42, H-48)
15.	Demobilization of contractor	<ul style="list-style-type: none"> • construction sites are cleaned from residual waste and fully reinstated; • Enabling environment is provided for natural regeneration of work base and construction sites to natural condition. 	Contractor and monitored by PIU/DSC	Contractor cost.

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
16.	Risk associated with others facilities like Crusher, asphalt plant	<ul style="list-style-type: none"> The Contractor will develop and maintain emergency response system in order to address any accidents or other emergency situation or disaster at site such as fall of workers from height, collapse of pier, flood, earthquake, accident, etc. The Contractor will dispose all the chemical wastes generated during the time of construction safely without interrupting the existing nearby settlements, water bodies, forests and wildlife. 	Contractor and monitored by PIU/DSC	Contractor cost.
Physical-Operation				
1.	Road Stability and Drainage Management	<ul style="list-style-type: none"> Road side tree plantation to mitigate possible degradation of land along the project alignment, Ensure proper compaction as per design. Proper drainage should be maintained and monitoring should be done for clogging. 	Contractor/Municipality	Municipality Regular program during Operation.
2.	Air and Noise pollution	<ul style="list-style-type: none"> Maintain signs and speed restrictions on the road section within settlements area to reduce vehicle speed, dust generation, and where horns will not be blown and traffic speed will be regulated Strict enforcement of vehicle emission standards. Maintain road side tree plantation Air pollutant parameters (TSPM, PM10, SOx, NOx, COx, Pb). Conforming NAAQS of Nepal. Water quality (EC, PH, DO, TSS, Oil and Grease). Conforming WHO Standards. Noise levels (1 hr Leq dB(A). Conforming WHO standards 	Transportation entrepreneur, local people, Municipality	Municipality Regular program during Operation

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
3.	Water pollution	<ul style="list-style-type: none"> The operation of proposed work doesn't pose serious threat on water bodies; however, washing vehicles on fresh water streams will be avoided. 	Drivers, Ward, local people	Municipality will monitor
4.	Climate change and Disaster Risk	<ul style="list-style-type: none"> Trees must be planted along the alignment so as to enhance the greenery and beauty of the city and to reduce erosion, draught, flood etc. 	Contractor along with municipality	
Biological-Construction				
1.	Slopes stabilizations/ Landslide, Vegetation clearing.	<ul style="list-style-type: none"> Bio-engineering has been proposed as mitigation for the stabilization of slopes along the road. Civil structures like masonry wall, gabion walls are constructed during construction There is no any vegetation loss during construction. 	Contractor	Included in BOQ (BOQ item no G-38, G-39, G-40)
Biological-Operation Stage				
2.	Impact on Vegetation	<ul style="list-style-type: none"> Encouraging local people for protection of roadside plantation carried out during construction. 	Municipality	No additional cost
Social- Construction Stage				
1.	Land use change	<ul style="list-style-type: none"> Land will be required for the establishment of labor camps, storage of construction materials, workshops etc as rented basis which is the temporary in nature. All the temporarily rented land will be rehabilitated into previous state or better than the earlier state maintaining natural drainage and acceptable to the land owner/DSC. 	Contractor.	Contractor cost.
2.	Pedestrian and houseowner Safety: General people, children and elderly people.	<ul style="list-style-type: none"> Diversions should be safe for children, the elderly, differently-able people and other general pedestrians during construction to ensure their mobility. School areas (Janapremi basic school) should be emphasized and adjacent transitions should be ensured. 	Contractor	Included in BOQ (BOQ item no B-7, H-41, H-42)

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
		<p>Appropriate signs and signage are used during project execution and construction to raise awareness of potential safety hazards of construction.</p> <ul style="list-style-type: none"> • 39 nos. of Zebra crossing are provided throughout the alignment in certain interval and major crossings. 		
3.	Health and Sanitation	<ul style="list-style-type: none"> • Proper awareness on use of sanitation facility for worker, • A chlorine solution will be provided to workers to purify drinking water. • Regular health checkups, records of illness of the workers, monitoring of drinking water. 	Contractor	NRs 50,000
4.	Child labor and forced labor	<ul style="list-style-type: none"> • No child (below 16 years) and forced labor will be employed in project. • Age verification document of workers will be documented and contractor will maintain the record of labor employed. 	Contractor	No additional cost
5.	Occupational Health and Safety	<ul style="list-style-type: none"> • PPE including mask, gloves and First aid kit will be provided to the worker and also kept in Camp. • Worker will be given clear instruction to follow safety rules. • Orientation on use of PPE during construction will be provided to workers and in regular basis, Induction and refresher training to the workers will be provided. • Safe Drinking water will be provided to the camp for worker use. • Provision of insurance to cover physical damage to workers • Contractor will be responsible to maintain the records of each and every accident and incidence and will make available to DSC/PCO/PMST as and when required. 	Contractor	Included in BOQ (BOQ item no B-8)

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
		<ul style="list-style-type: none"> Contractor's team, staff and laborers can also make use of the GRM to raise complaints / grievances if any. 		
6.	Community Health, Safety and Security	<ul style="list-style-type: none"> Display appropriate signage for use during construction and implementation of the project to enhance awareness creation on the potential hazards of the project. <ul style="list-style-type: none"> Carry out site management practice such as the fencing around work area and road signage. Increase public awareness of safety, health and environmental issues by providing information directly and indirectly through campaign. 	Contractor/ Public awareness campaign by the municipality	Included in the BOQ (BOQ Item no A-3)
7.	Grievance Redress, GBV, SEA/SH risks and Stakeholder Consultation	<ul style="list-style-type: none"> Clear and timely dissemination of project information, and safeguards aspects including anti-harassment cell; Coordination with the local stakeholders Awareness raising, information and dissemination about GRM and GBV (meetings, monitoring and logistic costs@ 1 meeting every month) SEA/SH awareness raising activities, trainings and stakeholder engagements such as: community based-awareness program, School based awareness program, Awareness program for women and against the gender-based violence (Domestic violence, Sexual violence) GRM will include mechanism for referring SEA/SH-related grievances Production of leaflets, and information dissemination through local media of ESMP, project provisions including GRM and SEA/SH and ESMP document in ward offices. 6 numbers of awareness/ orientation/ trainings (awareness on domestic violence (2nos.), 	PIU/DSC	NRs 750,000.00 (Further breakdown presented in table 4-3 below)

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
		<p>Rape/Sexual violence (1nos.), Child marriage (1nos.) CoC (2nos.) with at least 30 participants in each activity and will conduct during first quarter of 1st year and 3rd quarter of first year)</p> <ul style="list-style-type: none"> • Skill enhancement training targeting the women, vulnerable, disadvantaged groups including IPs like Rai, Bote and Chaudhary @ 1 event. 		
8.	SEA/SH risks	<ul style="list-style-type: none"> • Separately Camps for man & women and maintain minimum distance from one camp to other camp. • Providing female labor-centric facilities such as separate female toilets, separate female camps, separate family camps and mother's rooms on the site. • GRM will include mechanism for referring SEA/SH-related grievances. • Formulating and adopting Code of conduct including sections on the safety of women and girls (CoC should be included in all contracts and training on CoC should be provided to all workers) • CoC are understood through orientations and signed by workers. 	Contractor	This will be part of contractor's cost
Social Operation Stage				
	Encroachment of ROW	<ul style="list-style-type: none"> • The municipality will work with wards to discourage encroachment into the RoW. 	Municipality	Cost will be borne by municipality
	Air and Noise Pollution	<ul style="list-style-type: none"> • Maintain signs and speed restrictions on the road section within settlements area to reduce vehicle speed, dust generation, and where horns will not be blown and traffic speed will be regulated. • Strict enforcement of vehicle emission standards. 	Municipality	Municipality will be responsible during operation stage.

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
		<ul style="list-style-type: none"> • Maintain road side tree plantation • Air pollutant parameters (TSPM, PM10, SO_x, NO_x, CO_x, Pb). Conforming NAAQS of Nepal. • Water quality (EC, PH, DO, TSS, Oil and Grease). Conforming WHO Standards. • Noise levels (1 hr Leq dB(A). Conforming WHO standards. 		
	Traffic accidents and associated risks	<ul style="list-style-type: none"> • Raise awareness of traffic rules, pedestrian / cycle lanes and installation of speed bumps to control speed near pedestrian crossing areas • Traffic management plan will be developed, especially along congested locations. • Traffic control measures, including speed limits will be enforced strictly. • Further encroachment and squatting within the ROW will be prevented. 	Municipality	Cost will be borne by municipality
	Limited access for elderly and differently-able people	<ul style="list-style-type: none"> • Provide training on the use of facilities; maintain signboards, lights, instructions in strategic locations. 	Municipality	Cost will be borne by municipality

*Note: * The DSC team will responsible for monitoring the abovementioned construction phase works*

4.6 Summary of Costs of ESMP Activities

The summary of costs for the ESMP activities is outlined in **Table 4-3** below

Table 4-3: Summary of Cost of ESMP Implementation

SN	Items & Headings	Unit	Qty	Rate	Total, NPR	Remarks
1	Water quality test	Samples	2		250,000	
2	Air quality monitoring/sampling	Samples	2			
3	Noise level monitoring	Samples	2			
4	Awareness/trainings on GBV, SEA/SH and will conduct during first quarter of 1st year and 3rd quarter of first year) with at least 30 participants in each event	Events	6	50000	300,000	
i	Awareness program on reduction of sexual violence/rape cases	Events	1			
ii	Awareness program regarding domestic violence (physical/ mental abuse)	Events	2			
iii	Awareness for reduction of Child marriage	Events	1			
iv	Awareness/Training on CoC	Events	2			
5	Solid waste management	Events	1	25000	25,000	
6	Awareness on Health and sanitation of workers and provision of chlorine solution	Events	1	50000	50,000	
7	Awareness on Road safety and community safety	Events	1	25000	25,000	
8	Construction safety (PPE, Protective clothing including helmets, masks, boots, gloves, ear plugs, goggles etc)	LS	1	50000	50,000	
9	Skill enhancement training targeting the women, vulnerable, disadvantaged groups including IPs like rai, bote and chaudhary @ 1 event (NRs 100,000 per event)	Event	1	100000	100000	
10	Social safeguards (grievance meetings, site monitoring, etc)	Meetings/Events	18	12500	225,000	
11	Information dissemination materials and medium					
	(a) Leaflets and ESMP document printing				50,000	
	(b) PSA (local media)		3	25,000	75,000	
	Total				1,150,000	

Table 4-5: Implementation schedule for Environmental measures

SN	Impact	Mitigation Measures	2024												2025					
			1	2	3	4	5	6	7	8	9	10	11	12	1	2	3	4	5	6
1	Air Pollution (Dust)	Water Spray																		
2	Other air pollution parameters	Instrumental analysis																		
3	Water pollution	Lab analysis																		
4	Noise pollution	Instrumental analysis																		
5	Spoil disposal	Proper Supervision																		
6	Borrow pits	Excavation as specified, Reclamation and rehabilitation																		
7	Road Traffic safety	Installation of safety sign boards, campaigning																		
8	Campsite monitoring	Supervision																		
9	Occupational Health & Safety	Safety awareness programme, PPE																		
10	Solid Waste Management	Disposal practice, 3R principal awareness for worker																		
11	Loss of vegetation; greenery promotion	Road side plantation																		
12	Quarry sites	Excavation as specified, Reclamation and rehabilitation																		

4.7 Environmental & Social Monitoring

Environmental and social monitoring is an essential tool to make ensure the implementation of mitigation measures and to know the effectiveness of those measures. ESMP monitoring is necessary for the following purposes;

- to track the impacts,
- to evaluate the effectiveness of proposed mitigation measures, and
- to suggest improvements, if any new circumstances arise.

The following table summarizes the plan for environmental and social monitoring for the proposed project;

Table 4-6: Environment and Social monitoring Plan

SN	Monitoring Aspects	Location	Parameters	Methodology	Frequency	Responsibility
A	Construction Phase					
1.	Air quality monitoring	Settlement areas near road alignment	At least TSP; PM ₁₀ /PM _{2.5}	Air Sampler / Detector	Quarterly	DSC
2.	Noise Levels	Settlement areas near road alignment	Average noise levels (L _{eq})	Noise Meter / Android Application	Every Month	DSC
3.	Water Quality	Near disposal site and Campsite	Parameters as per ESMF of the project	Laboratory Analysis	Quarterly	DSC
4.	Spoil Disposal	Along road alignment	Spoil tip sites; road sections where spoils are generated	Site verification	Every month	Contractor/ DSC
5.	Road Traffic safety	Along road alignment	Status of road for use; road accidents records, Management of diversion, traffic signboards, flow of existing traffic, accessibility to the locals, grievances etc.	Use of Logs; Records of complaints	Every Month	DSC, PIU
6.	Camp site monitoring	Campsite	Space for workers; Potable water; Sanitation facilities; waste management, First Aid, rehabilitation after closure of the camp sites etc.	Site verification; records of provisions of WASH materials;	Every Month	DSC, PIU

SN	Monitoring Aspects	Location	Parameters	Methodology	Frequency	Responsibility
7.	Occupational Health & Safety	Active construction sites; camp site	Provision/Use of PPEs; First Aid/treatment; Awareness/orientations conducted for workers	Site verification; records of supply of PPEs; records of events	Every Month	DSC, PIU
8.	Solid Waste Management	Camp site and along the alignment and other construction areas	Types of waste generation, quantity, disposal practice	Observation	Every Month	DSC, PIU
9.	Loss of vegetation; greenery promotion	Active construction sites; road sections passing through vegetated area	Site clearance at vegetated areas; plantation works, maintenance etc. as per EMP.	Site verification; records of trees cut; records of newly planted trees	Every Month	DSC, PIU
10.	Impact upon physical structures due to vibration of heavy machinery	Along the alignment	Public and private structures affected/cracks shown, loss of business, rehabilitation and relocation, etc.	Site verification, observation, documents in place.	Every Month	DSC, PIU
11.	Quarry sites	All the quarry sites	Land slide, water logging, impact upon public, impact upon water bodies, dust, impact upon physical structures, complain and grievances, rehabilitation of the sites etc.	Observation	Every Month	DSC, PIU
12.	Borrow pits	All the borrow pits excavated for the project	Land slide, water logging, dust, and impact upon private land, impact upon physical structures, complain and grievances, rehabilitation of the sites etc.	Observation	Every Month	DSC, PIU
13.	GBV and SEA/SH	Work sites; settlement areas near camp site	Laborers' records; Cases of GBV in relation to project works; any unrecorded cases	Community consultation; GRM records	Every Month	DSC, PIU
14.	Other facility sites established by the contractor	Project area		Observation	Every Month	DSC, PIU

SN	Monitoring Aspects	Location	Parameters	Methodology	Frequency	Responsibility
B	Operation & Maintenance Phase					
1	Road stability	Road alignment	Status of slopes along road alignment; status of road components	Site verification; ward records; traffic police office records	Every 3 months - 1 st year; Bi-annually after that	Municipality
2	Water pollution	Surface flows (natural drainages & irrigation canals)	Parameters as per standards (Annex IV)	Laboratory Analysis	Every 6 Months	Municipality
3	Vegetation/tree survival rate	Plantation sites	Number of surviving trees; status of planted trees	Site verification; interaction with road-side communities	Every 6 Months	Municipality
4	Road safety	Road alignment and adjacent settlements	Road accidents; status of road components	Site verification; interaction with road users	Every Month	Municipality

Monitoring activities during O&M Phase will need to be scheduled based and site requirements. Contractor will be responsible for monitoring during DLP period. After the DLP period, Triyuga Municipality will be responsible for the Operation and Maintenance phase of sub project.

5. STAKEHOLDER ENGAGEMENT AND INFORMATION DISSEMINATION

5.1 Stakeholder Engagement

The local community, Municipality office, ward offices, water supply user committees, School, labourers and farmers were consulted during the preparation of the ESMP. Consultations were also carried out with the women and indigenous communities living along the proposed road alignment. The main objectives of these consultations were to aware public on environment and social issues, find the issues/mitigations to be included in ESMP report. Focused group discussions (FGDs), formal consultation meetings, and Key informant interview (KII) were used to engage as many locals as possible during the public consultation process. The details of design aspects of the road, social & environmental safeguards, and GBV aspects were discussed during the public consultations. The list of people and institutions consulted are presented in **Table 5-1, 5-2**, minutes are attached in **Annex II** and issues raised/discussion during consultations are listed in **Annex VIII**.

Consultation summary/issues raised during consultations

Consultation with locals and stakeholders were carried out to gather information and interact local people to implementation process, procedure and community roles and responsibilities during and after construction as well as preparation phase. People in the project area have shown their commitment to supporting the project. There is no other objection except the completion of construction on time and maintaining quality during construction.

During the consultation, peoples mainly emphasized on the quality of road and the material that will be used during the construction as well as proposed components of the roads, other concerns of locals are summarized below.

- Water must be sprayed during the construction phase
- Women empowerment must be taken into consideration.
- Road safety must be taken seriously during construction phase.
- Willing to work as construction helper as per need.
- Quality of the road construction works

Table 5-1: People consulted during field study

SN	Category	Total people consulted		
		Male	Female	Total
1	Brahman	28	9	37
2	Kshetri	9	9	18
3	Tharu	19	14	33
4	Rai	16	8	24
5	Tamang	2	0	2
6	Bishwokarma	2	9	11
7	Pariyar	2	0	2
8	Bhujel	1	0	1
9	Thebe/Magar	4	2	6
Total		83	51	134
%		61.94	38.06	100

Source: field study 2023

Table 5-2: Details of participants during consultations

SN	Date	Name of the Road	Name	Organization	Consultation type	Remarks
1	2080-05-25	Mayor	Basanta Kumar Basnet	Triyuga Municipality	KII	Mayor
2	2080-05-24	Deputy Mayor	Maheshwori Rai	Triyuga Municipality	KII	Deputy Mayor
3	2080-05-20	Engineer	Nirmal Chaudhary	Triyuga Municipality	KII	Engineer
4	2080-05-24	W-11 Chair	Shumanta Koirala	Triyuga Municipality	KII	W-11 Chair
5	2080-05-24	W-13 Chair	Nabin Kumar Chaudhary	Triyuga Municipality	KII	W-13 Chair
6	2080-05-25	W-12 Chair (Acting)	Anita Thapa	Triyuga Municipality	KII	W-12 Chair (Acting)
7	2080-05-25	Ward Member, W12	Ramkali BK	Triyuga Municipality	KII	Ward Member, W12
8	2080-05-22	W-10 Chair	Kiran Panday	Triyuga Municipality	KII	W-10 Chair
9	2080-05-24	Chairperson, Himali tole	Shahi kumar Tuladhar	Triyuga Municipality	KII	Chairperson, Himali tole
10	2080-06-10	CAO, Triyuga Municipality	Bishnu Bhakta Sigdel	Triyuga Municipality	KII	CAO, Triyuga Municipality
11	2080-06-09	Sagarmatha Road	Gopal Pokhrel	Triyuga Municipality	KII	Principal, Janapremi Basic school, Ward-13
12	2080-05-07	Sagarmatha	Saraswoti Mahatara and other tole members	Raudidas tole bikas	FGD	
13	2080-05-10	Sagarmatha	Kshab Prasad Pokhrel and other tole members	Hawaifield tole bikas, Ward-13	FGD	
14	2080-05-13	Sagarmatha	Aadesh Kumar Rai and other tole members	Yakata Tole bikas, Ward-12	FGD	Rai
15	2080-06-10	Sagarmatha	Gauri Narayan Chaudhary and other tole members	Locals, Ward 12, Bisanpur	FGD	
16	2080-06-09	Sagarmatha	Chandra Bahadur Tamang and other School teacher	Shree Janapremi Basic School, Ward-13	FGD	School
17	2080-05-07	Sagarmatha	Laxman Dhakal and other tole members	Ward and Raudidas tole bikas	Meeting	

SN	Date	Name of the Road	Name	Organization	Consultation type	Remarks
18	2080-05-10	Sagarmatha	Kshab Prasad Pokhrel and other tole members	Chairperson Hawaifield tole bikas and Locals, Ward-13	Meeting	
19	2080-05-13	Sagarmatha	Aadesh Kumar Rai and other tole members	Yakata Tole bikas, Ward-12	Meeting	
20	2080-06-10	Sagarmatha	Shrawan Kumar Chaudhary and other tole members	Bisanpur tole bikas, Ward-12	Meeting	Chaudhary only
21	2080-06-09	Sagarmatha	Gopal Pokhrel, Headteacher and other teacher	Janapremi Basic School	Meeting	

5.2 Information Dissemination

All details regarding the suggested activities and their anticipated outcomes would be made available to the impacted people and other stakeholders. The project will make all necessary disclosures during each step of the project cycle in cooperation with the relevant municipal authorities, NGOs, and other community groups. In order to mutually identify relevant protective or corrective measures, agencies working for environmental and social elements will also be notified about the ongoing and planned activities. The following strategies will be used to ensure that information is available to all parties involved throughout the project cycle.

- Mass Media: Use local media like newspaper, radio and TV.
- Meeting/Workshops
- Distribution of project documents: Certain project documents will be disclosed in Nepali (or other relevant local language). Project-related information materials will be distributed prior to each construction work to local officials, local people, stakeholders and other concerned offices like municipality, Ward, Tole Committee etc.
- Publish ESIA/ESMP, information about grievance mechanism and other project related activities in the municipality's website.

An Information Centre will be established at the municipality office during implementation to disseminate all the documents related to the project activities. Based on the public information disclosure policy, PCO and the municipality will disclose the information through its website. The information dissemination plan for proposed Road sub project is presented in **Table 5-3**.

Table 5-3: Information dissemination plan

Means of Communication	Timeline & Frequency	Responsibility	Resources
Municipality Website (Project details, ESMP, GRM)	At the start of the project, & maintained throughout	PIU/ Information Officer	Information Officer
Newspaper and local Radio (project salient features, dates, GRM etc.)	Project implementation phase Weekly basis	PIU, municipality Information Officer	Radio-program, FM Radio Clip
Project leaflets and Fact Sheet,	Project details, Implementing agencies, project period - 2 times	PIU, Information Officer	Double-sided colour A4 (500 copies)
Face to face engagements - meetings, focus group discussion with relevant stakeholders	Project Main Activities, Financial Assistance, Implementing agencies, project period etc. 2 time in year	PIU, Information Officer	
Ward offices and Municipal social section/Library (Hard copy of ESMP)	Project period- 2 times	PIU/Municipality	Focal person

5.3 Public/Community Consultation Plan

During project implementation phase, all consultations on social and environmental issues will be conducted in a way that includes women and vulnerable social groups (such as low-income households,

members of certain castes, and people with disabilities, among others). Details of the Project Consultation Plan are presented in **Table 5-4**

Table 5-4: Stakeholder consultation Plan

Objective and Target Goal	Method	Responsibility
I. Build Local Ownership		
Introduce Project DPR Report and its components	Group Meeting/Workshops	DSC/ Municipality
Maintain efforts for two-way communication with relevant stakeholders through the project	Face to face meeting with concerned stakeholders	DSC, Ward Level Representatives.
II. Start Consultation Process with Potentially Affected Communities by construction and operation of road		
Identify communities to be potential affected by project	Electronic and face to face communication with relevant stakeholders and implementing agencies	DSC, Municipality Ward Representatives
Consult with community representatives and ensure that their concerns with the proposed project are addressed	Face to face meeting with community representative Meeting will take place following protocol for meeting.	DSC, Municipality Ward Representatives
Ensure that the views and needs of vulnerable segment (if required) of communities, including but not limited to poor, women, elderly, and are addressed by the subproject	Face to face meeting with affected communities' representative (including social officer of Municipality, women's representative etc.)	DSC, Municipality Ward Representatives.
III. Implementation Phase		
Maintain effective communication with PIU	Electronic and face to face communication with representative of relevant agency /organization	PCO, Design and Supervision Consultant Municipality Ward Representatives
Raise awareness of project activities among potential beneficiaries	Media advertisements and targeted campaign	DSC/ Municipality
Maintain consultation process with a potential affected communities and beneficiaries	Face to face meeting with affected communities' representative (including social officer of Municipality, women's representative etc.)	Design and Supervision Consultant Municipality Ward Representatives
Monitoring and evaluation community involvement	Face to face meeting with affected communities' representative	Design and Supervision Consultant Municipality Ward Representatives
Reports outlining progress of activities related to engagement and communication	Collation of progress report, self-evaluation by PCO	PCO

Objective and Target Goal	Method	Responsibility
Agreement on operation and maintenance system	Electronic or face to face communication with relevant stakeholder Face to face meeting with local authority	Design and Supervision Consultant Municipality Ward Representatives
Implementation of ESMP	The contractor will prepare the various standalone plans to comply with ESMP requirements By including all the stand alone plans, the contractor will prepare Contractor's Environmental and Social Management Plan (ESMP) and submit it to PIU. This requirement will be included in the contract BOQ	The contractor's bid document must include the requirements outlined in the ESMP. Before work starts, the contractor will draft the stand-alone plans, present them to the PIU, and get permission. The standalone plan comprises, among other things, a management plan for the environment, health, and safety; a strategy for traffic; a plan for handling complaints; a plan for managing spoils; a plan for disaster readiness; a plan for managing camps; a plan for managing labor; and a plan for managing air, water, and noise.

5.4 Grievance Redress Mechanism (GRM)

As part of the implementation stage the PIU, the project municipality, project engineers and environment and social staffs will directly interact and consult with the people of project alignment. The people of project alignment will be able to raise any grievances related to the sub project with the subproject grievance redress mechanism (GRM), to help ensure the successful implementation of resettlement measures. At first instance, the project-affected grievant should raise their grievance with the information office of the project, and the information office will determine whether it can be resolved within the project, at the ward level, or whether another mechanism should be used. PAPs will be exempt from all administrative fees incurred, pursuant to the grievance redressed procedures except for cases filed in court. More details regarding the GRM are discussed in the following sections.

5.4.1 Structure of the GRC

Two-level Grievance Redress Committee (GRC) has been formed for the sub-project. The composition of the proposed GRC is given below. The GRC formation related municipality letter is attached in **Annex-II**.

a. The first level of GRC sits at the project site level. It is composed of the following team:

Chairperson of ward 11, Mr. Sumanta Koirala	– Coordinator
Ward Chairperson, ward-10, Mr. Kiran Pande	--Member
Acting Ward Chairperson, ward-12, MRs. Anita Thapa	--Member
Ward Chairperson, ward-13, Mr. Nabin Kumar Chaudhary	--Member
Ward member, ward-10, MRs Urmila BK	--Member
Ward member, ward-11, MRs Nila Kakri Dholi	--Member
Ward member, ward-12, MRs Ram kali Biswokarma	--Member
DSC (Supervision Engineer/Social/Environmental Safeguard Specialist)	– Member Secretary

b. The second level GRC at the municipality level. It will comprise of the following team.

Mayor	– Coordinator
Deputy mayor	– Member
Chief, Administrative section	– Member
NUGIP focal person	– Member
Municipality’s Grievance Officer	– Member
Chief, Municipality’s Social Development section	– Member
DSC (Supervision Engineer/Social/Environmental Safeguard Specialist)	–Member Secretary

c. The third level GRC will be established at PCO Level.

5.4.2 Processes of the GRM

Grievances shall be submitted through various mediums, including in person, in written form to a noted address, through a toll-free phone line or through direct calls to concerned officials, and emails. The PCO will appoint a person (Operator) at PCO- Kathmandu to receive such calls and online messages. The person (Operator) based on nature of complaint, will forward the same to the information office or ward committee.

A ticket or a unique number will be generated for all such call, messages and letters. The complainant will follow up based that unique number with Operator at PCO-Kathmandu. All complaints will be responded within two weeks at any level. In case response is not received from 1st level within 15 days, the complaint will be escalated to next level. If complaint remains unaddressed at 1st and 2nd within maximum 30 days after registering the complaint, it will be elevated to 3rd level at PCO level. The PCO within 7 days of time should instruct the concerned person at Triyuga municipal level to arrange for a hearing within maximum 5 days of time. Effort will be given by all levels of GRCs to conduct hearing and resolve the concern at their level up to the satisfaction of complainant within the stipulated timeframe. In case 1st and 2nd level GRCs are unable to resolve the concern up to the satisfaction of complainant, these GRCs’ or Complainant may approach to 3rd level of GRC at PCO Level. After conducting hearing at any level of GRC, the decision will be communicated to complainant within maximum 30 Days of time.

All local contact information and options for complaint submission will be available on site, on Toles, Wards, municipality office, PCO on information boards and the project municipality websites. A half yearly report on Grievance Redress by the subproject project will be prepared and will be sent to the project municipality’s GRCs by Wards’ GRCs and ultimately to GRC of PCO. The PCO will forward the same to the World Bank.

The project GRM will include a process for addressing any SEA/SH-related grievances, SEA/SH cases and to provide training/orientation on SEA/SH cases.

5.4.3 Further details of the GRM

The functions of grievance mechanism include redressing grievances of community / beneficiaries /project affected persons in all project respects, providing rehabilitation and resettlement assistance and related activities, and hearing grievances from workers involved in the project at any level or phase. The system

should be established to report back to the concerned community or persons regarding the decision on the complaint. The grievances related to women should be dealt by women officer. As required, the social mobilizers will be recruited. GRC will deal/hear the issues related to Environment, R&R and individual grievances and will give its decision/verdict within 30 days after hearing the aggrieved person. The final verdict of the GRC will be given by the Head of GRC in consultation with other members of the GRCs and will be binding to all other members. Potential grievances which may need to be addressed are listed below:

- Access to resource /utility/facility
- Ambient air and noise Quality
- Impact on water quality/resource
- Grievance from vulnerable community
- Gender related issues
- Grievances from workers
- Safety and risk repeated to project development

5.4.4 Other Mechanisms for Grievance Redress

All complainants have the option to approach court/judiciary or the World Bank's Grievance Redress Service in case he or she is not satisfied with the verdict provided.

6. LIST OF REFERENCES

- Environment Protection Act, Government of Nepal, 2019
- Environment Protection Regulations, Government of Nepal, 2020 (and amendments)
- Environmental and Social Management Framework, Nepal Urban Governance and Infrastructure Project, September 2020, the World Bank
- Final Detailed Project Report on Upgradation of Sagarmatha Agriculture bazar west to south Madan Bhandari Hall to Janpremi tole Bisanpur ward 12 Road, Triyuga Municipality Road, August 2023
- Nepal Human Rights Year Book 2023, Informal Sector Service Center (INSEC), Kathmandu.
- Project Implementation Manual, Nepal Urban Governance and Infrastructure Project, December 2022, the World Bank
- <https://censusnepal.cbs.gov.np>
- IEE Report of Triyuga, Baruwa River
- Municipality Profile of Triyuga Municipality 2019
- <https://Triyugamun.gov.np/>

7. LIST OF ANNEXES

Annex I: Environmental and Social Screening Checklists

Annex II: Minutes, Public Notice and Letters

Annex III: Proposed Typical Cross Sections

Annex IV: GoN Permissible Environmental limits/standards

Annex V: Air, Noise and Water Quality Test Report

Annex VI: List of Zebra crossings

Annex VII: Code of Conduct (CoC) on GBV

Annex VIII: Stakeholder Consultation Details

Annex IX: Photographs

Annex X: Cadastral Maps of Alignment

Annex XI: Alternative route

Annex I: Environmental and Social Screening Checklists

Table 7-1: Environmental Screening Checklist

SN	Particulars	Yes	No	Can't Say	Remarks
1	Is the site vulnerable to major or induced hazards such as: Landslides, Flooding, Storm surge, Severe wind damage, Earthquakes, Fire, Explosion, Other (specify)				There will be chances of flooding, earthquake and Natural Disaster.
2	Is the project area adjacent to or within any of the following environmentally sensitive areas? <input type="checkbox"/> <input type="checkbox"/> Cultural heritage site (historical, religious, traditional, or cultural significance) <input type="checkbox"/> <input type="checkbox"/> Protected Area (National Parks, Wildlife Reserve, Hunting Reserve, Conservation Areas, and Buffer Zones etc.) <input type="checkbox"/> <input type="checkbox"/> Wetland/Ramsar Site/Simsar <input type="checkbox"/> <input type="checkbox"/> Forest <input type="checkbox"/> <input type="checkbox"/> Special area for protecting biodiversity/interest <input type="checkbox"/> <input type="checkbox"/> Breeding/nesting ground of wildlife/occurrence of migratory species <input type="checkbox"/> <input type="checkbox"/> Migration route/Wildlife corridor <input type="checkbox"/> <input type="checkbox"/> Any site of national or international importance		No		
3	Likely impact on trees (including Timber & fruit bearing) and vegetation cover		No		
4	Possibility of degradation of land and ecosystems of surrounding?				There is no any unstable surface and land slide prone areas. The road is lies in plain area. Thus, there is less chances to degradation of land and ecosystem.
5	Is the project area densely populated?		No		
6	Heavy with development activities/big industries nearby & type?		No		
7	Alteration of surface water hydrology of waterways due to the project resulting in increased sediment in streams affected by increased soil erosion at construction site?		No		
8	Chance of deterioration of surface water quality due to silt runoff and sanitary wastes from worker-based camps and chemicals used in construction		No		

SN	Particulars	Yes	No	Can't Say	Remarks
9	Does the sub project require significant extraction of surface or ground water?		No		
10	Increased risk of water pollution from oil, grease, fuel spills and other materials				Short term impact during upgradation work.
11	Impact on water quality due to release of sewage/sludge?		No		Not connected to drainage system.
12	Possibility of flooding due to sewage		No		Not connected to drainage system.
13	Possibility of increased air pollution during preconstruction/ construction/operation phase?				Short term impact during upgradation work.
14	Other pollution concerns relating to inconveniences in living conditions that may trigger cases of upper respiratory problems?				Short term impact during upgradation work.
15	Risks and vulnerabilities related to occupational health and safety due to physical, chemical, biological hazards during project construction and operation				Short term impact during upgradation work.
16	Noise and vibration due to blasting and/or other civil works?				Short term impact during upgradation work.
17	Possibility of poor sanitation and solid waste disposal				Campsite management aspect, ESMP measures applicable
18	Creation of temporary breeding habitats for diseases such as those transmitted by mosquitoes and rodents?		No		
19	Accident risks associated with pre construction, construction & operation phases of project				Injuries during construction phase, and traffic accidents during operation phase are potential risks
20	Large population influx during project construction and operation that causes increased burden on social infrastructure and services (such as water supply and sanitation systems)				Suggested to use local worker during construction but Influx of people is anticipated during the time of upgradation. Pressure upon existing natural resources such as water, existing food availability, existing educational facilities are likely to be affected with the increase in the number of outside workers during upgradation.
21	Risks to community health and safety due to the transport, storage, and use and/or disposal of materials such as explosives, fuel and other chemicals during construction and operation?			Partial	Fuel and lubricants pose potential risks, but explosives are not used in the project
22	interference with other utilities and blocking of access to resource/utility				Short term impact during upgradation work.


SN	Particulars	Yes	No	Can't Say	Remarks
23	Generation of solid waste and/or hazardous waste during construction/operation of project?				Short term impact during upgradation work.

Table 7-2: Social Screening Checklist

SN	Particulars	Remarks
1	Proposed Site Location	
	a Land requirement for the project	It is an up-gradation of an existing road. There will be no additional land requirement. Due to the narrow section (6.3m) i.e. from CH 0+000 to CH 0+040 & from CH 0+068 to CH 0+712, the construction width of proposed road remains same.
	b Landownership of the project area: Govt. / Private lands	Land within the proposed road width is already in use by the public. As per discussion with locals and municipality, there is no any outstanding issues related to grievance, compensation etc
	c Is the project requires acquisition of Govt. land/structures? If yes please mention the area of land, number of affected structures, Households	No
	d Present use of Govt. Land that will be used for the project activities with Persons/Households using	No
	e Is the project requires acquisition of private land/structures? If yes please mention the area of land, number of affected structures, Households	No
	f Present use of Govt. Land that will be used for the project activities with Persons/Households using <input type="checkbox"/> Agricultural purposes <input type="checkbox"/> Residential purposes <input type="checkbox"/> Commercial purposes <input type="checkbox"/> Other purposes (Indicate)	Public use
	g Is the project requires relocation of encroachers/squatters If yes please elaborate number and nature	No
	h Is the project requires relocation of community facilities/Govt. establishment or any object that are of religious, cultural and historical significance	No
	i Proposed project located in an area where residents are- <input type="checkbox"/> All Mainstream <input type="checkbox"/> All Indigenous peoples	Majority Indigenous peoples

SN	Particulars	Remarks
	<input type="checkbox"/> Majority Mainstream or Non-indigenous peoples <input type="checkbox"/> Majority Indigenous peoples	
2	Potential Social Impacts- Will the Project cause	
a	Involuntary resettlement of people? (Physical displacement and/or economic displacement)	No
b	Impacts on the poor, women and children, Indigenous Peoples or other vulnerable groups?	No such impact on poor women and children, indigenous people, and/or economic displacement.
c	Will community facilities require relocation?	Yes, Triyuga small town Water supply pipeline and electric poles are likely to affected.
d	Will the sub-project disturb any traditional activity on adjoining or nearby?	No
e	poor sanitation and solid waste disposal in construction camps and work sites	Yes, there will be concern of sanitation and solid waste disposal in construction camp and work sites
f	Possible transmission of communicable diseases (such as STI's and HIV/AIDS) from workers to local populations?	May have chances to transmission of diseases. Local peoples have knowledge on such communicable diseases but labourers' understanding may be low about possible transmission of communicable diseases
g	Large population influx during project construction and operation that causes increased burden on social infrastructure and services (such as water supply and sanitation systems)?	yes
h	Social conflicts relating to inconveniences in living conditions where construction interferes with preexisting roads	Yes, change in road morphology and disruption in the infrastructure like drinking water, sewer system will cause inconvenience. Traffic management plan will be prepared by the contractor and will be included in Contractor's Site specific ESMP.
i	Describe any other impacts that have not been covered in this screening form	No permanent structure lies within the construction width of indigenous people, hence there is no adverse impact on indigenous group
j	Describe alternatives, if any, to avoid or minimize displacement from private and public lands	No displacement of private and public land is required.
k	RAP/ARAP Requirement	Not required

Annex II: Minutes, Public Notice and Letters
ROW Declaration Letter



त्रियुगा नगरपालिका

नगर कार्यपालिकाको कार्यालय

गाईघाट, बिसेपुर्
कोशी प्रदेश, नेपाल

फोन संख्या :- ०७९१०८०
फालाठी नम्बर :- २६९०

फै ०२५- ४२०१०८
फै ०२५- ४२०१०८
triyuga.mun@gmail.com

मिति :- २०८०/०२/०७

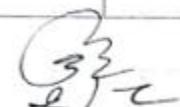
विषय:- सडकको ROW सम्बन्धमा ।

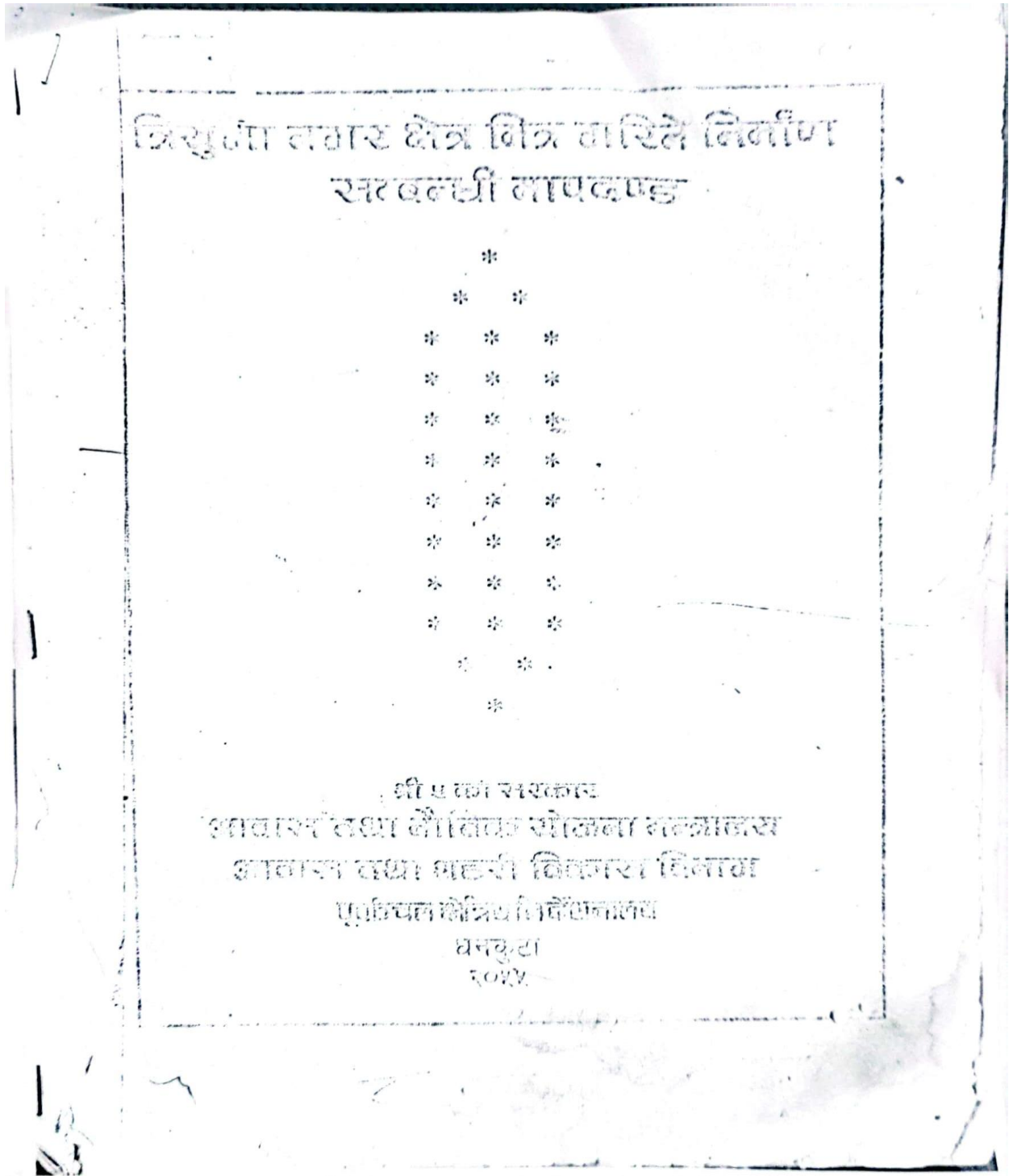
श्री शहरी विकास तथा भवन निर्माण विभाग,
नेपाल शहरी शासकीय तथा पूर्वाधार आयोजना आयोजना समन्वय कार्यालय, चब्रमहल, काठमाडौं ।
प्रस्तुत विषयमा NUGIP कार्यक्रम अन्तर्गत यस नगरपालिकाबाट योजनाहरूको प्रथम चरणमा मिति २०७९/०६/०९ गते बसेको नगर स्तरीय समन्वय समितिको निर्णयानुसार तपसिल सडकहरूको सम्भाव्यता अध्ययनका लागि प्राथमिकतामा छुनोट भएको तपसिल सडकहरूको लम्बाई र सडक क्षेत्राधिकार (राईट अफ वे) को विवरण तपसिल बमोजिम रहेको व्यहोरा हुन अनुरोध छ ।

तपसिल
पहिलो प्राथमिकता (९।९५० किमी)

सडकको नाम	त्रियुगा नगर क्षेत्र भित्र गरिने निर्माण सम्बन्धि मापदण्ड	प्राथमिकता नं.
१. गाईघाट टाउन प्लानिङ क्षेत्र भित्र भएको सडकहरू त्रियुगा स्कूल पूर्व उपोत्तनगर सडक विनपा ११ समेत (ROW= 12m, 18 m and 25m) लम्बाई १।५०० कि.मी.	जिल्ला विकास समितिले गाईघाट बजारमा प्लानिङ सरी ८० फिट, ६० फिट र ४० फिट चौडाई कायम सडकहरूलाई यथावत कायम गर्नुपर्नेछ पेज नं. ११	१
२. सिटीहल देखि सिमान्टार गाईघाट दिक्तेल सडक Access road सहित (ROW = 20m) लम्बाई २।८८० कि.मी.	शिव मन्दिर प्रोक्सि रालमाटे सडक २० मिटर पेज नं. ७	१
३. बापारानी देखि डि एम गेट सम्म सडक (ROW = 8 m) लम्बाई ०।६५० कि.मी.	डि. एम. गेट देखि पश्चिम उदयपुर डिप्टिलरीसम्म जाने बाटो ८ मिटर पेज नं. १०	१
४. फुलचोक देखि सि जी चोक सडक बाटेटोल त्रियुगा कोरीडर (ROW = 8m) लम्बाई ०।८६० कि.मी.	फुलचोक देखि दक्षिण मुसहरी टोल हुदै बाटिसम्म जाने सडक ८ मिटर सहित पेज नं. १०	१
५. सिटीहल देखि राम चन्द्र बस्नेतको घरसम्म सडक (ROW = 8 m) लम्बाई ०।२८० कि.मी.	प्रवेश माध्यमको चौडाई ८ मिटर र लम्बाई १००० मिटर पेज नं.१२	१
६. गाईघाट शिवालय टोल आर्देश टोल रमाईलो डाडा मनमोहन स्कूल देखि करमगाडी गाईघाट दिक्तेल सडकसम्म (ROW = 8 m) लम्बाई १।६१० कि.मी.	प्रवेश माध्यमको चौडाई ८ मिटर र लम्बाई १००० मिटर पेज नं.१२	१
७. प्रगति टोल स (क्वाटर अगाडी) को सडक हुदै सकेला जाने सडक बडा नं. ११ (ROW = 10m) लम्बाई २।१७० कि.मी.	प्रवेश माध्यमको चौडाई ११ मिटर र लम्बाई १००० मिटर भन्दा माथि पेज नं.१२	१
८. सगरमाथा कृषि बजार पश्चिमबाट दक्षिण मदन भण्डारी सभाहल हुदै जनप्रेमी टोल बिसनपुर बडा नं. १२ (ROW = 8 m and 14 m) लम्बाई ३।७५० कि.मी.	डि.एम.गेट देखि दक्षिणबाट पुब तुम्नमा चोक पुगने गाईघाट नवप्रेमी प्रा.वि. लक्ष्मीपुर मा.वि. हुदै बेहदवासम्म जाने बाटो सडक चौडाई १२ मिटर	२
९. राजाजी मन्दिर बडा नं १२ बाट उत्तरको सडक हुदै मदन भण्डारी राजमार्ग जोडने सडक (ROW = 8 m) लम्बाई ०।४६० कि.मी.	प्रवेश माध्यमको चौडाई ८ मिटर र लम्बाई १००० मिटर पेज नं.१२	३

योधार्थः
श्री BOTEK-GOEC JV


किरण कुमार झाकी
 नि.प्रमुख प्रशासकीय अधिकृत
 नि. प्रमुख प्रशासकीय अधिकृत



त्रियुगा नगर क्षेत्र गरिने निर्माण सम्बन्धी मापदण्ड तयार गर्ने टोली:

१. श्री कविन्द्र बहादुर विष्ट
क्षेत्रिय निर्देशक, आवास तथा श.वि.वि.
पूर्वाञ्चल क्षेत्रिय निर्देशनालय
धनकुटा
२. श्री महेश चन्द्र न्यौपाने
ईञ्जिनियर, जिल्ला विकास समितिको कार्यालय
उदयपुर
३. श्री कोपनाथ अधिकारी
ईञ्जिनियर, जिल्ला आवास तथा शहरी विकास कार्यालय
उदयपुर
४. श्री ब्रह्म देव चौधरी
ओभर सियर, त्रियुगा नगर पालिकाको कार्यालय
उदयपुर
५. श्री केशव नाथ कट्टेल
सुर्भेयर, आवास तथा शहरी विकास विभाग
पूर्वाञ्चल क्षेत्रिय निर्देशनालय
धनकुटा

भूमिका (Preface)

श्री ५ को सरकारवाट वि.स. २०५२ साल चैत्र १६ गते गठित गाईघाट नगर विकास समिति क्षेत्रभित्र दिन प्रति दिन बढ्दो घना जनआवादी, उद्योग-वाणिज्य र व्यापारमा तीव्रता, यातायात तथा संचारका साधनहरूको विकास, आधुनिक वैज्ञानिक साधनहरूको बढी उपयोग, शिक्षा, स्वास्थ्य जस्ता सामाजिक पक्षको उन्नती जस्ता कुराहरूले यस क्षेत्रको आर्थिक तथा औद्योगिक गतिविधिलाई प्रत्यक्ष प्रभाव पार्न सक्ने कुरा सर्व विदित छ । यसरी मानव सभ्यता र संस्कृत लगायत सामाजिक, आर्थिक सम्पूर्ण क्षेत्रमा नै आउने परिवर्तनले हाथा गाई घर एवं बजार केन्द्रहरू क्रमशः शहरीकरण तर्फ उन्मुख हुदै जाने क्रम बढदै गएको वर्तमान परिप्रक्ष्य भित्र नै यो नगर विकास समिति पनि पर्दछ ।

बढ्दो शहरीकरणले कतिपय कुराहरूमा जनधारणालाई हित पुग्न सक्ने भएता पनि अव्यवस्थित बसोबासका कारण फोहरमैला बढ्ने, विजुली, दुरसंचार, ढल विकास, सडक निर्माण आदि जस्ता भौतिक पूर्वाधारहरू तयार गर्ने सन्दर्भमा विविध समस्याहरूले समष्टिमा शहरको संरचनालाई नै विगान्न सक्ने हुदा त्यस्ता विद्तीहरूलाई समयमा नै निराकरण गर्नु अपरिहार्य भएकाले सम्बन्धित क्षेत्रभित्रका नगर विकास समिति, नगर पालिका, पेशागत संघ संस्था, बुद्धिचित्री, गन्धमान्य व्यक्तिहरू लगायत सर्वसाधारणहरू समेतबाट संकलित राय सुझावहरूलाई आधार मानि विस्तृत छलफल र गहन अध्ययन पश्चात प्रस्तावित भू-उपयोग क्षेत्र र सडक तथा भवन निर्माण सम्बन्धी मापदण्ड तयार पारिएको छ ।

अन्तमा यस कार्यमा सहयोग गर्ने सम्पूर्ण व्यक्तिहरू धन्यवादको पात्र हुनुहुन्छ ।

विषय सूची

	पेज नं.
भूमिका	१
त्रियुगा नगर क्षेत्रभित्र गरिने निर्माण सम्बन्धी मापदण्ड	२
परिभाषा	२
घडेरीको लागि न्यूनतम, लम्वाई, चौडाई र क्षेत्रफलको ईकाई	३
घर/भवन बनाउँदा त्यस्ता घर/भवनले ओगट्नु पाउने घडेरी क्षेत्र	४
घर/भवनको दायाँ बायाँ छोड्नु पर्ने घडेरी क्षेत्र	४
घर/भवनको तला, उचाई, मोहडा, छत तथा निर्माणको ढाँचा	४
घर/भवनका मुख्य भागहरूको निमित्त आवश्यक व्यवस्थाहरू	५
सडकको लागि छोड्नु पर्ने जग्गा (राईट अफ वे)	७
नदी किनारा, सार्वजनिक जग्गा आदि नजिक निर्माण गर्दा छोड्नु पर्ने जग्गा	११
पेटी, बलेसी, ढल आदिको लागि छोड्नु पर्ने जग्गा	११
प्रत्येक घर/घडेरीमा आवश्यक पर्ने पर्याप्त प्राकृतिक प्रकाश र हावाको व्यवस्था	११
घडेरी विकास भई सो उपभोग गर्न आउने जनसमुदायका लागि आवश्यक पर्ने सामुदायिक सेवा र आधारभूत पूर्वाधारहरूको व्यवस्था तथा निजी क्षेत्रका जग्गा विकासकर्ताले पालना गर्नुपर्ने मापदण्ड तथा शर्तहरू	१२
घडेरी वा जग्गा विकास योजना संचालन गर्दा छुट्याउनु पर्ने खुल्ला क्षेत्र तथा अन्य सार्वजनिक, सामुदायिक प्रयोजनका लागि छुट्याउनु पर्ने जग्गा	१३
होर्डिङ बोर्ड अस्ता सूचना विज्ञापन पार्टी राख्ने ठाउँ	१३
नदक्षता पास गर्दा संलग्न गर्नुपर्ने कगजातहरू	१३
विशेष	१४
सन्दर्भ सामग्री	१५
नक्साहरू	

त्रियुगा नगर क्षेत्रभिन्न गरिने निर्माण सम्बन्धी मापदण्ड

यस मापदण्डले जग्गाको उपयोग सम्बन्धी वर्गिकरण, आवत जावतको निर्मित आवश्यक पर्ने जग्गा (Means of access) खुला भाग (Open Space) निर्माण गरिने प्लट (Plot) को स्थिति, क्षेत्रफल र उचाईको सिमाहरको साथै पार्किङ्ग लगायत निर्माण सम्बन्धी सम्पूर्ण आवश्यकताहरूलाई समेट्ने प्रयास गरेको छ ।

परिभाषा:

विषय वा प्रसंगले अर्को अर्थ नलागेमा यस मापदण्डमा प्रयोग भएका निम्नलिखित शब्दहरूले निम्न अर्थ जनाउने छन् ।

भवन वा निर्माणको उचाई:

समतल छाना (Flat Roof) को हकमा जमीनको सालाखाला सतहबाट भवन वा निर्माणको माथिल्लो सतहसम्मको उचाई वा सम्बन्धित अधिकारीले तोकी दिए वगैजिमको उचाईलाई स्वीकृत उचाई मानिने छ ।

कार्पेट एरिया (Carpet Area):

कुनैपनि तलाको उपयोग हुने कोठाहरूको क्षेत्रफललाई कार्पेट एरिया मानिने छ ।

ढाकिएको क्षेत्रफल (Covered Area):

कुनैपनि प्रकारको निर्माणद्वारा ढाकिएको सतह भन्दा लगत्तै माथीको पुरा भागलाई ढाकिएको क्षेत्रफल मानिने छ ।

खुला भाग (Open Space):

प्लटको खुला छान्डिएको भागलाई खुला भाग मानिने छ ।

तला (Storey):

भवनको भुईँ देखि सिलिङ्ग विचको भागलाई तला मानिने छ ।

आवासीय क्षेत्र (Residential Area):

यो क्षेत्र आवासको लागि हुनेछ र यस्तो क्षेत्रमा सेवा सुविधाहरू विस्तार गर्न सकिने छ । यो क्षेत्रमा बासु प्रदुपण, ध्वनी प्रदुपण, वातावरणमा पर्ने अन्य प्रदुपण गर्ने कारखाना वा उद्योग वा अन्य कुनै क्रियाकलाप राख्न पाइने छैन ।

भन्दाइ (Staircase):

भन्दाइको न्यूनतम चौडाईहरू निम्नानुसार हुनुपर्नेछ:

क) आवासिय भवन (घरहरू)	०.९० मी.
ख) आवासिय (होटल भवनहरू)	१.५० मी.
ग) मानिसहरू जम्मा हुने (अडिटोरियम, नाचघर, सिनेमा घर आदी) भवन	१.५० मी.
घ) शैक्षिक भवन	१.५० मी.
ङ) संस्थागत भवन	२.०० मी.
च) अन्य भवनहरू	१.५० मी.

खुडकिला (Step) को चौडाई (Tread):

आवासिय भवनहरूको लागि खुडकिलाको चौडाई २५ से.मी. र अन्य भवनहरूका लागि ३०.० से.मी. भन्दा कम हुनुहुदैन । एउटा फ्ल्याईटमा वहीमा (Step) संख्या १२ वटा र कमतमा ३ वटा हुन सक्नेछ ।

खुडकिला (Step) को उचाई (Riser):

(Riser) को उचाई १५-१९ से.मी. सम्म हुन सक्नेछ । यसको सख्या एउटा फ्ल्याईट (Flight) मा वहीमा १२ वटासम्म हुन सक्नेछ । देस्ट स्लाब (Waist Slab) को त्थोप २५° देखि ४२° सम्म हुनु पर्नेछ ।

लिफ्ट (Lift):

भवनको उचाई ४ (चार) तला भन्दा धेरै बढाउनु पर्ने भएमा लिफ्टको व्यवस्था गर्नुपर्नेछ ।

६. सडकको लागि छोडनु पर्ने जग्गा (राईट अफ वे):
सगरमाथा राजमार्गको लागि सडक एनले तोके बमोजिम हुनेछ ।

६.१ बाहिरी चक्रपथ (Outer Ring Road):

भण्टावारी - चुहाडे - जलजले - देहरी - श्रीपुर - घगाहा - पाप - भुल्के - रौजनपुरको - मिलनथोक - फाँताहा - विरानपुर - वेहडवा - अनारी - शिव मन्दिर - कोबसे - रात्माटे - राजादास - चाईकोत्सा - सगरमती हुँदै भण्टावारीसम्मको चक्रपथको लागि:

City hall Simaltar Gaighat Diktel road

राईट अफ वे (Right of Way):

सडक केन्द्र रेखा देखि दुबै तर्फ	१०/१० मी.
सडक बारीपारिका घरहरू बिचको हुनी (Between building lines)	२० मी.

पानी ट्यांक, पारावेट वाल, भन्त्याङ छोप्नका लागि बनाईने उचाई बाहेक निर्माण गर्न सकिने अधिकतम उचाई-१२.२० मीटर ।

६.२ भित्री चक्रपथ (Inner Ring Road):

खरि बजार गैरुन - भिक्केटोल - डिपु चोक - राजावास - साईखोला - सरस्वती - नया भोडा - चुहाडे चोक - जलजले - कुमारी चोक - देउरी - छैरजोडी खोला - भुल्के - वगाहा चोक - मोती गडा - त्रियुगा कजदेवाट गाईघाट बजारको मुख्य चोक हुदै - खसी बजारसम्मको चक्रपथको लागि:

सडक केन्द्र रेखा देखि दुवै तर्फ १०/१० मीटर

सेट व्याक ३/३ मीटर दुवै तर्फ

पानी ट्यांक पारावेट वाल, भन्त्याङ छोप्नका लागि बनाईने उचाई बाहेक निर्माण गर्न सकिने अधिकतम उचाई-१२.२० मीटर

६.३ रेडियल रोड्स (Radial Roads):

- जिल्ला सदरमुकामवाट वाहिरी चक्रपथसम्म जोडने सडक ।
- मेन चोकवाट रजत जयन्ती चोकहुदै उत्तर शिव मन्दिरसम्म जाने सडक ।
- गाईघाट बजार मुख्य चोक देखि पूर्व असारी जाने सडक ।
- मोतीगडा चोक देखि पूर्व भुल्केसम्म जाने सडक ।
- नगर क्षेत्र भित्रको सगरमाथा राजमार्ग ।
- गाईघाट बजार देखि वगाहासम्म ।
- गाईघाट बजार बोक्से हुदै बरुवा खोलासम्म
- सडकको केन्द्र रेखावाट दुवै तर्फ - ८/८ मीटर
- सेट व्याक २/२ मीटर दुवै तर्फ
- पानी ट्यांक, पारावेट वाल, भन्त्याङ छोप्नका लागि बनाईने उचाई बाहेक निर्माण गर्न सकिने अधिकतम उचाई-१२.२० मीटर ।

६.४ शाखा सडक (आर्टेरियल रोड्स):

- चक्रपथ वा रेडियल सडकवाट विभिन्न भू-उपयोग क्षेत्रहरु जोडने सडक ।
- सडकको केन्द्र रेखावाट दुवै तर्फ ६/६ मीटर
- सेट व्याक १/१ मीटर दुवै तर्फ

विभिन्न भू-उपयोग क्षेत्र तथा स्थान:

औद्योगिक क्षेत्र - वगाहा र धीपुर

बजार क्षेत्र - खैरजोडीको मिल्न चोक/गाईघाट/देउरी/जलजले/बैरेनी १६ नं./चुहाडे/वगाहा/बोक्से चम्पापुर ।

- आवास क्षेत्र - बोक्से बाल मन्दिर भएको ठाउँबाट उत्तर पूर्व/गैरुन क्षेत्र/
रात्माटे/पुरानो गाईघाट
- बसपार्क क्षेत्र - बरुवा बाईको उत्तर/शिव मन्दिरको छेउ ।
- हाट बजार क्षेत्र - हवाई फिल्डको जग्गा ।
- गुन्द्री/मण्डी बजार - हालको खसि बजार क्षेत्र ।
देउरीमा पुरानो गा.वि.स. भवनको दक्षिणपट्टि पोखरी जस्तो ठाउँ

खुला क्षेत्र (Open Space): मिलनचोक देखि दक्षिण फर्सेट खोला देखि पूर्व क्याम्पस देखि पश्चिम जंगल भन्दा उत्तरको क्षेत्र ।

स - साना खुला क्षेत्र: छुट्टयाई जिल्ला विकास समितिको दक्षिणपट्टि मन्दिरलाई पुग्ने गरि वाकी रहेको जग्गामा ।

मोतिगडा पन्चलाल खोरिया

जलजले बजार देखि माथिको डाडो (रतनपुर डाडो)

पर्यटन क्षेत्र - ककनी (त्रि.न.पा. १२ र ४ असारी/धार्मिक क्षेत्र)

बृद्ध, अशक्त र अपाङ्गहरुका लागि आवास क्षेत्र: बगाहाको सालधारी (परमानन्दको घर अगाडी बाटो देखि तलतिर)

मुर्दाघाटको स्थल - त्रियुगा कजवे भन्दा एक कि.मी. तल/चपनमा डा. रनेश्वर खोरिया

रंगशाला - त्रि.न.पा. ७ भुल्के

डम्पिङ साईट - जलजले भन्दा दक्षिण पश्चिम जंगलमा भन्ज्याङ जस्तो ठाउँ

स्कूल क्याम्पस - चुहाडे देखि जलजले साईडको विचमा/भण्टावारी/बोक्से खैजनपुर/गाईघाट बजार, देउरी/भुमरजुवा ।

कृषि क्षेत्र - वांसवारी एरिया/असारी/गैरुन/लक्ष्मपुर/चुहाडे १४ नं./बाटो
दायाँ बायाँ बाहेकको
क्षेत्र/धैहडवा/होक्से/इपौती/खैजनपुर/पात्लेवास ।

५.५ प्रशाखा सडक (Connector Roads):

बाहिरी चक्रपथ, भित्री चक्रपथ, रेडियल सडकबाट र आर्टेरियल सडकहरु मध्ये कुनैबाट वसिसम्म जोड्ने सडक ।

सडकको चौडाई १० मीटर

सेट व्याक १/१ मीटर सडकको दुबैतर्फ

Sagarmatha
Agriculture
bazar west to
south Madan
Bhandari Hall
to Janpremi
tole Bisanpur
ward 12 Road

पानी ट्यांक, पारामेट बाल, भन्दाट छोप्नका लागि बनाउने उचाई बाहेक निर्माण गर्न
मिल्ने अधिकतम उचाई ९.२० मीटर

- घम्पापुर चौक देखि कात्ती खोला हुदै राग्गाटे जाने सडक ।
- घम्पापुर चौक देखि रोता बोडिङ्ग स्कूल हुदै नया बसि सम्म जाने बाटो ।
- डि.एम.गेट देखि दक्षिणपट्टियाट पूर्व सुम्निमा चौक .पुरानो गाईघाट .जनप्रेमी प्रा.वि. .लक्ष्मिपुर मा.वि. हुदै बेहडवा सम्म जाने बाटो ।
- चुताहेवाट भण्टावारी हुदै मुकुंचीसम्म जाने बाटो ।

६.६ शहरी सडक (Urban Road):

एक आवासीय तथा व्यापारिक क्षेत्रबाट अर्को आवासीय क्षेत्र वा व्यापारिक क्षेत्रसम्म जोडने सडक ।

सडकको चौडाई - ६ मीटर
गेट व्याक - १/१ मीटर सडकको दुबै तर्फ

पानी ट्यांक, पारामेट बाल, भन्दाट छोप्नका लागि बनाउने उचाई बाहेक निर्माण गर्न
मिल्ने अधिकतम उचाई - ९.२० मीटर ।

- बावरी देखि छरदार टोल - दुम्बल खोला हुदै बासवारीसम्म जाने बाटो ।
- आशी खारे - राग्गाटे प्रा.वि. हुदै कैरुसम्म ।
- आशी खारे चुकोट हुदै कटारी गाईघाटमा मिल्ने सडक ।
- गाईघाट दिक्तेल सडकबाट फाल्गुको गोठे शिव मन्दिरसम्म
- साविक माठ विद्युत समिति गाईघाटबाट उत्तर तानी गुराँस चौकीर सडक जाने बाटो सम्म ।
- डैन धामिनीको घरबाट सुम्निमा चौकसम्म ।
- पुल चौक देखि दक्षिण गुराँसी टोल हुदै चोटे टोलसम्म जाने सडक ।
- मिटी कमवाट पूर्व सगम टोल हुदै बरुवा खोलातिर जाने बाटो ।
- डि.एम.गेट देखि पश्चिम उदरपुर विद्युतसम्म जाने बाटो ।
- मोतिपहावाट थापसम्म जाने सडक ।
- मोतिपहावाट नि.मा.वि. सडकबाट पश्चिम चौधरी टोलबाट सुमी गगरवाडा राजमार्गको क्याम्पस चौकमा मिल्ने बाटो ।
- क्याम्पस चौकबाट क्याम्पससम्म जाने बाटो ।
- फर्नेट खोला भन्दा पूर्व उत्तरबाट क्याम्पस जाने सडक ।

Fulchowk PG Chowk Road Botetole
Triyuga Corridor section

Babarani DM Gate Road Section

६.७ सीर्स सडक (Service Road):

मार्गका दुवैपति सडकबाट प्रति घरसम्म जोडने सडक ।

सडकको चौडाई ४ मीटर
सडकको सम्बाई दूरीमा २० मीटर

Town Planning Road

दिल्लसामा विचलन समितिले माइघाट बजारमा प्लाजिड गरि २०', १०' र ४०' चौडाई कायम गरेका सडकहरूलाई मध्यायत कायम गर्नुपर्नेछ ।

७. नदी किनारा, सार्वजनिक जग्गा बाहिर नजिक निर्माण गर्दा छोडनु पर्ने जग्गा:
नदीमा वर्षाको समयमा आउन सक्ने बाढी (High Flood) ले अवर गर्न नसक्ने क्षेत्रमा सम्बन्धित निवासले स्वलयत निरिक्षण गरि घर बनाउन उपयुक्त हुने भएमा स्वीकृत दिन सक्नेछ तर उक्त निर्माणले हरित क्षेत्रलाई कुनै किसिमको असर नपर्ने हुनुपर्दछ । बाटोको दायाँ बायाँ ३०/२० मीटर (नदीको Course माथेका) हरित क्षेत्रको लागि छुट्टाउनु पर्नेछ । सार्वजनिक जग्गा बाटो छाटो आदिमा भवन निर्माणका लागि आवश्यक पर्ने निर्माण सामग्री पुष्पाडने तथा वेप विचलन गर्ने कार्य गर्न पाइने छैन ।
८. पेट्टी, बलेसी, ढल आदिको लागि छोडनु पर्ने जग्गा:
 - छुट तथा बलेसीको पानी आपत्तो जग्गामा मात्र सञ्चालन पाइनेछ । तथा सार्वजनिक सडक क्षेत्रमा पानी सञ्चालु पर्दा अनिवार्य रूपमा जमिनसम्म पाईपको व्यवस्था गर्नुपर्दछ ।
 - आपत्तो पर्वालहरूमा पनि डिमेन्सिवाई यान्त हुने गरि सो पर्वालमा बलेसीको पानी सञ्चालन पाइने छैन ।
 - घरको पेट्टीको माथ डिमेन्सिवाई घरका पेट्टीसँग मिल्नेको हुनुपर्दछ ।
 - पेट्टीको न्युनतम उचाई १ फिट (०.३० मीटर) हुनु पर्दछ । न्युनतम चौडाई २ फिट (०.६० मीटर) हुनु पर्दछ । घरको पेट्टी सार्वजनिक जग्गा सट्टामा बनाउन हुदैन ।
 - सीसो ढलको छुवाघाट आपत्तो जग्गामा पानी ढले गरि बढीमा १ मीटरसम्म स्लान्टीबिबर स्थापन नियमन पाइनेछ ।
९. घरको घर/घडेरीमा आवश्यक पर्ने पर्वाप्त प्राकृतिक प्रकाश र हावाको व्यवस्था:
 - शान्तिप्र प्रयोगको हिसाबले वर्गाकार गर्ने खोटाहरू बाहिरी वा भित्री कुनै भाग वा सुला वरन्दासँग जोडिएको हुनु पर्दछ ।
 - बाहिरी वा भित्री कुनै भागका खोटाका आवश्यक हुने प्रकाश र भेन्टिलेसनको लागि घरको २ छुट्टी भवन वा निर्माणको अगाडीको भाग, पछाडीको भाग वा बाईँट्टाका बाटोतर्फ पर्लिएको भए त्यस्तो टाउमा जोडिने कुनै भाग भन्ध्यामा बाटोलाई फराकिलो पार्न समेत पर्वाप्त हुने गरी छोडनु पर्नेछ ।
 - प्रकाश र भेन्टिलेसनको लागि भवनको मुहको क्षेत्रफलको दमिनामा १/१० भाग खोटाहरू माथेका भूभाग तथा भेन्टिलेसनले ओगटेको हुनु पर्दछ । दगाँवर भूभागका क्षेत्रफल ३'०" भन्दा बढी सार्इनेको हुनु हुदैन । आयताकार भूभागका क्षेत्रफल ३'०" चौडाई र ५'०" उचाईको हुनु पर्नेछ । चौडाई र उचाईको अनुपात १:२ भन्दा बढी हुनु हुदैन ।

१०. घडेरी विकास भई सो उपभोग गर्न आउने जनसमुदायका लागि आवश्यक पर्ने सामुदायिक सेवा र आधारभूत पूर्वाधारहरूको व्यवस्था तथा निजी क्षेत्रका जग्गा विकासकर्ताले पालना गर्नुपर्ने मापदण्ड तथा शर्तहरू:

युनैपनि व्यक्ति, फर्म, सघ, संस्थाले धरी ५ को सरकारबाट वा खरिद गरि आवश्यक प्लानिङ (Planning) गरि जनसमुदायलाई बेच दिखन गर्नका लागि निम्नानुसार पूर्वाधार कार्यक्रमको व्यवस्था गरि नगर विकास समितिसंग स्वीकृती लिई बेच दिखन गर्न सकिनेछ ।

प्रवेश माध्यमको चौडाई र लम्बाई:

प्रवेश माध्यमको चौडाई (मी.)	प्रवेश माध्यमको लम्बाई (मी.)
४.०	५०.०
६.०	२००.०
८.०	१०००.०
११.०	१०००.० भन्दा माथि

- City Hall Ramchandra Basnet house Road
- Gaighat Shivalay tole-Adarsha tole-ramailo danda manmohan school to karamgachhi diktel connecting Road

Pragati tole kha (Quarter front) ko road to sakela road

- निजी क्षेत्रका जग्गा विकासकर्ताले नगर विकास समितिले दर्शिकरण गरेको भू-उपयोग क्षेत्र मध्ये आवश्यक क्षेत्र भित्रमा मात्र जग्गा विकास कार्यक्रम नगर विकास समितिको स्वीकृती लिई गर्नु पर्नेछ ।
- प्रत्येक घडेरीको क्षेत्रफल ४.७५ (२ आना २ पैसा) धेरै भन्दा कम हुने गरि जग्गा टुक्याउन पाईने छैन ।
- घडेरीको चौडाई कमिमा ६ मीटर हुनु पर्नेछ ।
- न्युनतम भौतिक पूर्वाधारहरू जस्तै प्रत्येक घडेरीमा पुग्ने छण्डास्मिथ (Gravel) काटो बचाको पानी निस्कनको लागि पब्लिक नाला (Strom drain) को व्यवस्था गरिनु पर्नेछ ।
- सडकमा छानेपानीको लागि जि.आई. पाईप जोड्नको लागि कार्य गर्नुपर्ने छ ।
- यदि आवश्यक भवनहरू निर्माण गरि सर्व साधारणलाई बेच दिखन गर्ने भएमा प्रत्येक प्लानको क्षेत्रफलको अधिकतम ७०% छान्दण्ड कभर हुने गरि भवन निर्माण गर्न पाइने छ ।

१०.१ निजी क्षेत्रका जग्गा विकासकर्ताले जग्गा विकास कार्यक्रम संचालन गर्दा विभिन्न प्रयोगका लागि छुट्याउनु पर्ने जग्गा निम्नानुसार हुनु पर्नेछ ।

- सार्वजनिक खुल्दा क्षेत्र (पार्क, खेलकुद, मैदान, हरियाली क्षेत्र, सार्वजनिक गौधरा) जग्गा जग्गाको १० - १५%
- आवासिय क्षेत्र (आवासका लागि मात्र) जग्गा जग्गाको ५५ - ६०%
- मिश्रित उपयोग क्षेत्र (सरोवास तथा व्यापारिक प्रयोगको क्षेत्र) जग्गा जग्गाको ४ - ५%

Formation of GRM (1st Level)



पत्र संख्या :- ०८०८८१
चलाबी नगर :-

त्रियुगा नगरपालिका

गाईघाट, चन्द्रपुर
कोशी प्रदेश नेपाल
२०७३

०१५-४२०७८
फ्याक्स ४२०७८५
triyuga.mun@gmail.com

मिति: २०८०/०६/१७

विषय:- प्रथम तहको गुनासो सुनुवाई समिति गठन गरिएको बारे

मिति २०८०/०६/१७ गतेका दिन यस त्रियुगा नगरपालिकाका नगर प्रमुख श्री बसन्त कुमार बस्नेत ज्यूको अध्यक्षतामा बसेको बैठकले विश्व बैंकको आर्थिक सहयोगमा नेपाल शहरी शासकीय तथा पूर्वाधार आयोजना (NUGIP) मार्फत स्तरोन्नति हुन लागेको यस उदयपुर जिल्ला त्रियुगा नगरपालिका वडा नं. १०, ११, १२, १३ मा पर्ने पहिलो र दोस्रो प्रथमिकतामा परेका बाटोहरूको कुल लम्बाई १४.५ किलोमिटर सडक खण्डको निर्माण अघि र निर्माणका क्रममा कुनैपनि प्रकारका समस्याहरू, गुनाशाहरू आई सोको सामाधानको लागि आयोजनाको मापडण्ड अनुसार तपसिलमा उल्लेखित प्रथम तहको गुनासो सुनुवाई समिति गठन गर्ने निर्णय गरियो।

तपसिल

१. संयोजक: श्री सुमन्त कोइराला, वडाध्यक्ष वडा नं. ११
२. सदस्य: श्री किरन पाण्डे, वडाध्यक्ष वडा नं. १०
३. सदस्य: श्री अनिता थापा, का.बा अध्यक्ष वडा नं. १२
४. सदस्य: श्री नविन कुमार चौधरी, वडाध्यक्ष वडा नं. १३
५. सदस्य: श्री उर्मिला बि, क वडासदस्य वडा नं. १०
६. सदस्य: श्री निला काक्री ढोली वडासदस्य वडा नं. ११
७. सदस्य: श्री राम कली विशकर्मा वडासदस्य वडा नं. १२
८. सदस्य: सामाजिक सुरक्षण/वतावरण विशेषज्ञ/सुपरभिजन ईन्जिनियर (DSC)

बसन्त कुमार बस्नेत

नगर प्रमुख

बसन्त कुमार बस्नेत

नगर प्रमुख

Formation of GRM (2nd Level)



पद संख्या :- ०८०८१
चलानी नगर :-

त्रियुगा नगरपालिका

गाइयाड, उदयपुर
कोशी प्रदेश नेपाल

०२५-४२०१०८
फ्याक्स ४२०१८५
triyuga.mun@gmail.com

सूचना प्रकाशन मिति: २०८०/०७/१५

बिषय:- द्वितीय तहको सुनुवाई समिति गठन गरिएको बारे

मिति २०८०/०७/१५ गतेका दिन यस त्रियुगा नगरपालिकाका नगर प्रमुख श्री बसन्त कुमार बस्नेत ज्यूको अध्यक्षतामा बसेको बैठकले विश्व बैंकको आर्थिक सहयोगमा नेपाल शहरी शासकीय तथा पूर्वाधार आयोजना (NUGIP) मार्फत स्तरोन्नति हुन लागेको यस उदयपुर जिल्ला त्रियुगा नगरपालिका वडा नं. १०, ११, १२, १३ मा पर्ने पहिलो र दोस्रो प्रथमिकतामा परेका बाटोहरूको कुल लम्बाई १४.५ किलोमिटर सडक खण्डको निर्माणका अबधिमा आउने गुनासाहरूको अध्ययन गरी स्थानिय स्तरमानै समाधान गर्न निम्न उल्लेखित व्यक्तिहरू रहनेगरी एक गुनासो सुनुवाई समिति (GRC) गठन गरीएको बारे सम्बन्धित सबैलाई जानकारी गराइन्छ । साथै आयोजना सम्बन्धि कोहिक्सैलाई कुनै गुनासो भए उल्लेखित गुनासो सुनुवाई समितिमा मौखिक वा लिखित रूपमा जानकारी गराउनहुन अनुरोध छ ।

तपसिल

१. संयोजक: श्री बसन्त कुमार बस्नेत त्रियुगा नगरपालिकाका नगर प्रमुख
२. सदस्य: श्री महेश्वरी राई, नगरपालिका उप- प्रमुख
३. सदस्य: नगरपालिका प्रशासन शाखा प्रमुख
४. सदस्य: नगरपालिका आयोजनाका सम्पर्क व्यक्ति (Engineer)
५. सदस्य: नगरपालिका सामाजिक अधिकृत/ सामाजिक विकाश शाखा प्रमुख
६. सदस्य: सामाजिक सुरक्षण, वतावरण विशेषज्ञ, सुपरभिजन, ईन्जिनियर (DSC)


बसन्त कुमार बस्नेत

नगर प्रमुख

बसन्त कुमार बस्नेत
नगर प्रमुख

Letter regarding Anti-Harassment Cell



पत्र संख्या : २०७९/०८०
चलानी नं. : २०५२

त्रियुगा नगरपालिका
नगर कार्यपालिकाको कार्यालय
गाईघाट, उदयपुर
कोशी प्रदेश, नेपाल




☎ ०३५-४२०१०८
www.triyugamun.gov.np

मिति: २०८०/०३/१७

विषय : सम्पर्क व्यक्ति (Focal Person) तोकिएको ।

अधिकृतस्तर सातौं, श्री राजकुमार खड्का (९८५२८३१६००)
सामाजिक विकास शाखा प्रमुख,
त्रियुगा नगरपालिका, गाईघाट, उदयपुर ।

प्रस्तुत विषयमा तपाईंलाई यस अघि तोकिएको जिम्मेवारीको अतिरिक्त यस नगरपालिकाको Anti Harassment Cell को सम्पर्क व्यक्ति (Focal Person) तोकिएको व्यहोरा अनुरोध छ ।


बिष्णु भक्त सिग्देल
प्रमुख प्रशासकीय अधिकृत

बोधार्थ

श्री शहरी विकास तथा भवन निर्माण विभाग,
नेपाल शहरी शासकीय तथा पूर्वाधार आयोजना, आयोजना समन्वय कार्यालय,
बदरमहल, काठमाण्डौ ।

प्रमुख प्रशासकीय अधिकृत

Email: Info@triyugamun.gov.np
Triyuga.mun@gmail.com

Public Notice for ESMP Preparation



पय संख्या :- ०८०१०८१
पलाणी नगर :-

त्रियुगा नगरपालिका
नगर कार्यपालिकाको कार्यालय
गाईघाट, उदयपुर
कोशी प्रदेश नेपाल

०२५- ४२०१०८
पयावस ४२०१८५
triyuga.mun@gmail.com

मिति :- २०८०/०६/१५

सूचना !

सूचना !!

सूचना !!!

विश्व बैंकको आर्थिक सहयोगमा शहरी विकास तथा भवन निर्माण विभाग (DUDBC), नेपाल शहरी शासकीय तथा पुर्वाधार आयोजना (NUGIP) द्वारा स्तरोन्नति हुन लागेको यस उदयपुर जिल्ला त्रियुगा नगरपालिका वडा नं. १२ र १३ स्थित पर्ने दोस्रो प्राथमिकतामा परेको सडक खण्डको स्तरोन्नति गर्ने भएको छ। यसै शिलशिलामा उक्त सडक खण्डको बिस्तृत परियोजना प्रतिवेदन तयारीका चरणमा गरिने वातावारणीय तथा सामाजिक व्यवस्थापन योजना (ESMP) तयारीका लागि परामर्शदाताका DSC Team, नगरपालिका आयोजनाबाट प्रभावित हुने बासिन्दा / जग्गाधनीहरु बिच निम्न लिखित स्थान र मितिमा हुने बिस्तृत छलफल तथा अन्तरकृया कार्यक्रममा उपस्थित हुनका लागि सम्बन्धित सबै सरोकारवालालाई सुचित गरिन्छ।

अन्तरकृया हुने मिति र स्थान :

मिति : २०८०/०६/२६

समय : विहान ११:०० बजे

स्थान : त्रियुगा नगरपालिका सभाहल

ई. शैलेन्द्र कुमार मण्डल

शहरी पुर्वाधार विकास शाखा

ई. शैलेन्द्र कुमार मण्डल

ईन्जिनियर

Notice related to GRM



त्रियुगा नगरपालिका

नगर कार्यपालिकाको कार्यालय

०१५- ४२०१०८
फ्याक्स ४२०१८५
triyuga.mun@gmail.com

पत्र संख्या :- ०८०१०८१
चलानी नगर :-

गाईघाट, उदयपुर
कोशी प्रदेश नेपाल

सूचना प्रकाशन मिति: २०८०/०६/१८

सूचना !

सूचना !!

सूचना !!!

मिति २०८०/०६/१८ गतेका दिन यस त्रियुगा नगरपालिकाका नगर प्रमुख श्री बसन्त कुमार बस्नेत ज्यूको अध्यक्षतामा बसेको बैठकले विश्व बैंकको आर्थिक सहयोगमा नेपाल शहरी शासकीय तथा पूर्वाधार आयोजना (NUGIP) मार्फत स्तरोन्नति हुन लागेको यस उदयपुर जिल्ला त्रियुगा नगरपालिका वडा नं. १०, ११, १२, १३ मा पर्ने पहिलो प्राथमिकतामा गाईघाट टाउन प्लानिङ क्षेत्र भित्र भएको सडकहरु, सिटिहल देखि सिमलटार गाईघाट दित्केल सडक, बाबारानी देखि डि एम गेट सडक, फुलचोक देखि पि जी चौक सडक वोटेटोल त्रियुगा कोरीडोर, सिटिहल रामचन्द्र बस्नेतको घरसम्म सडक, गाईघाट शिवालय टोल आर्दश टोल रमाईलो डाडा मनमोहन स्कूल देखि करमगाछी गाईघाट दित्केल सडक, प्रगति टोल क्वाटर अगाडीको सडक हुदै साकेला जाने सडक र दोस्रो प्राथमिकतामा सगरमाथा कृषि बजार पश्चिमबाट दक्षिण मदन भण्डारी सभाहल हुदै जनप्रेमी टोल विसनपुर परेका बाटोहरुको कुल लम्बाई १४.५ किलोमिटर भएको सडक खण्डको निर्माणका अबधिमा आउने गुनासाहरुको अध्ययण गरी स्थानिय स्तरमानै समाधान गर्न आयोजना स्तरमा वडा ११ का अध्यक्ष श्री सुमन्त कोइराला ज्यूको संयोजकत्वमा निम्न उल्लेखित ब्यक्तिहरु रहनेगरी एक गुनासो सुनुवाइ समिति (GRC) गठन गरीएको बारे सम्बन्धित सबैलाई जानकारी गराईन्छ । साथै आयोजना सम्बन्धित कोहिकसैलाई कुनै गुनासो भए उल्लेखित गुनासो सुनुवाइ समितिमा मौखिक वा लिखित रूपमा जानकारी गराउनहुन अनुरोध छ ।

तपसिल

१. संयोजक: श्री सुमन्त कोइराला , वडाध्यक्ष वडा नं. ११
२. सदस्य: श्री किरन पाण्डे , वडाध्यक्ष वडा नं. १०
३. सदस्य: श्री अनिता थापा, का.बा अध्यक्ष वडा नं. १२
४. सदस्य: श्री नविन कुमार चौधरी , वडाध्यक्ष वडा नं. १३
५. सदस्य: श्री उरमिला बि,क वडासदस्य वडा नं. १०
६. सदस्य: श्री निला काक्री ढोली वडासदस्य वडा नं. ११
७. सदस्य: श्री राम कली विश्कर्मा वडासदस्य वडा नं. १२
८. सदस्य: सामाजिक सुरक्षण/वतावरण विशेषज्ञ/सुपरभिजन ईन्जिनियर (DSC)

ई. शैलेन्द्र कुमार मण्डल
शहरी पूर्वाधार विकास शाखा
ई. शैलेन्द्र कुमार मण्डल
ईन्जिनियर

Minute related to Labor camp, stockpiling area, spoil site location

आज मिति २०८०।०८।१५ गतेका दिन जिपुगा नगरपालिका माईत कार्यालयमा हुने सामरे व्यावहारिक तथा र्कनियत आयोजना (NUPGP) इकायुडर जिल्लाका जिपुगा नगरपालिका पहिलो २ दोस्रो पाठ्यक्रममा मा परीका नारायण गार्डियाल गाउँपालिका प्लाजिङ्ग क्षेत्राधिकार इकायुडर सिटिङ्गले देउरी सिमान्तमा गाईकाट दिक्कलेल सडक, नावाराणी देउरी डि: एन.गोड, फुलगाउँ देउरी जि. ति. चौद बोरोलेल जिपुगा क्षेत्र-डोर, सिटिङ्गले देउरी रामचन्द्र वस्नेतको बाइसा, गाईकाट सिमान्तमा गोल आकरी गले समाईले डाडा प्रनमोत्तन स्कूल देउरी बरमाणा देउरी गाईकाट दिक्कलेल सडक, फुलगाउँ (ख) कौवाट आगादीको सडक हुने साईलेला जोते सडक सगरमाथा कृषि बजार (पश्चिमीकाट) दायाँको सडकमा देउरी व्यावहारिक ईई जतपेनीको जिपुगा कडा ई १२ का स्वयंसेवकहरूको इकायुडर-गोरे जिल्लासिलामा आयोजनाको लागि लेबर कर्मचारी न जिपुगा सामुदायिक सङ्घ जग्गा जिपुगा भएको हुनाले प्रत्येक नगरपालिकाका नगरप्रमुख र आयुक्तको एक सुझावहरूमा पर्ने जग्गा धाने समझौतेको शर्तहरूमा न बाइनेतिने दल लगाएत सुरक्षावालाहरूको सङ्घिकता उपर-जीपुगा गरिइको दालफल तथा परामर्श तथासिले उपर-जीपुगा ग्राफीकको सङ्घिकतामा से हलवपुर्ण र्व इलोलाशका साथ सम्भव भयो।

उपस्थिति

१.	श्री	नवल कुमार बस्नेत (नगरप्रमुख)	(३)
२.	श्री	सुदेवजी बाई (नगरप्रमुख)	(३)
३.	श्री	निर्गु शर्मा सिटिङ्गले (प्रमुख पञ्चायतियुक्त)	(३)
४.	श्री	बुधनाथ कोइराला (सि.सि.सि. - ११)	(३)
५.	श्री	त्रिविक्रम शर्मा चौधरी (वडा अध्यक्ष - १६)	(३)
६.	श्री	आशुतोष थापा (का.वा. सदस्य - १२)	(३)
७.	श्री	तन्वीजा पाण्डे (वडा अध्यक्ष - १०)	(३)
८.	श्री	शैलेश्वर शर्मा (ई.सि.)	(३)
९.	श्री	अमरादेव थापा (ई.सि.) DSC	(३)
१०.	श्री	सिद्धा शर्मा गुरागाँ (अ.सि.)	(३)

(Signature)

प्रस्ताव
१. लेबर क्याम्प सङ्कल्पना ।

विषय

प्रस्ताव नं: १ मागी बलपान गर्दा यस विद्यालय
नगरपालिका भित्र पर्ने पढिलो र दोस्रो प्राथमिकतामा
पर्ने ब्याड आयोजनाको कार्यान्वयनको समयमा लेबर
क्याम्पको आवश्यक पर्ने भएकाले उक्त लेबर क्याम्प
कार्यको लागि आवश्यक पर्ने जग्गा भन्ने विद्यालय नगरपालिका
का वडा नं: १३ स्थित बढेको बरवा बजारको भन्ने
सरकारी स्थानीय जग्गा रहेको हुदा उक्त स्थानीय तहको
लेबर क्याम्प र विद्यालय सङ्गती मन्डारना गर्न दिने विषय
गर्दियो ।

(Signatures)



पत्र संख्या :- ०८०१०८१
चलानी नम्बर :-

त्रियुगा नगरपालिका

त्रियुगा नगरपालिका
गाईघाट, उदयपुर
कोशी प्रदेश नेपाल
२०७३

☎ ०३५- ४२०१०८
फ्याक्स ४२०१०८५
triyuga.mun@gmail.com

मिति:- २०८०/०८/१८

विषय:- निर्माण को क्रममा आवश्यक जग्गा उपलब्ध गरिएको बारे।

यस त्रियुगा नगरपालिकामा विश्व बैंकको आर्थिक सहयोगमा नेपाल शहरी शासकिय तथा पूर्वाधार आयोजना (NUGIP) मार्फत स्तरोन्नति हुन लागेको यस उदयपुर जिल्ला त्रियुगा नगरपालिका वडा नं. १०, ११, १२, १३ मा पर्ने पहिलो र दोस्रो प्रथमिकतामा परेका १४.५ किलोमिटर सडक खण्डको निर्माणको लागि तपशिल बमोजिमको ठाँउ उपलब्ध गराईने व्याहोरा जानकारी गराईन्छ। तपशिल

१. Camp Site , Stockpile, Solid waste Management, Workshop, Concrete Batch Plant - बरुवा बस पार्कको उतर तर्फको नगरपालिकाको खली जग्गा।
२. Quarry Site Borrow Pits -त्रियुगा र बरुवा खोलामा IEE Approved भएको ठाँउ।
३. Spoil Disposal Site- बिसनपुरमा रहेको Dumping site।
४. Crusher- त्रियुगा- बरुवा दोभानको खाली रहेको जग्गा।

बसन्त कुमार बस्नेत

नगर प्रमुख

बसन्त कुमार बस्नेत
नगर प्रमुख

Municipal executive decision about IEE of Quarry site Approval

वैठक संख्या नं. : १९

आज मिति २०८०।०५।०९ गते सनिवारका दिन त्रियुगा नगरपालिकाका नगर प्रमुख श्री वसन्त कुमार वस्नेतज्यूको अध्यक्षतामा तपसिल बमोजिमका वडा अध्यक्ष तथा कार्यपालिका सदस्यज्यूहरुको उपस्थितिमा नगर कार्यपालिकाको आकस्मिक बैठक बस्यो ।

उपस्थिति

सि.न.	नामथर	पद	दस्तखत
१	श्री वसन्त कुमार वस्नेत	नगर प्रमुख	
२	श्री महेधरी राई	नगर उपप्रमुख	
३	श्री मनोज कुमार चौधरी	वडा अध्यक्ष वडा नं. १	
४	श्री घमान सिंह खत्री	वडा अध्यक्ष वडा नं. २	
५	श्री तिलक बहादुर कटुवाल	वडा अध्यक्ष वडा नं. ३	
६	श्री वद्री वस्नेत	वडा अध्यक्ष वडा नं. ४	
७	श्री उद्वन कुमार थापा	वडा अध्यक्ष वडा नं. ५	
८	श्री योगेन्द्र बहादुर दाहाल	वडा अध्यक्ष वडा नं. ६	
९	श्री अमर बहादुर दनुवार	वडा अध्यक्ष वडा नं. ७	
१०	श्री राज कुमार राउत	वडा अध्यक्ष वडा नं. ८	
११	श्री राज कुमार दनुवार	वडा अध्यक्ष वडा नं. ९	
१२	श्री किरण पाण्डे	वडा अध्यक्ष वडा नं. १०	
१३	श्री सुमन्त कोईराला	वडा अध्यक्ष वडा नं. ११	
१४	श्री अनिता थापा	का.वा.वडा अध्यक्ष वडा नं. १२	
१५	श्री नविन कुमार चौधरी	वडा अध्यक्ष वडा नं. १३	
१६	श्री हिम राज मगर	वडा अध्यक्ष वडा नं. १४	
१७	श्री गणेश प्रसाद तिमसीना	वडा अध्यक्ष वडा नं. १५	
१८	श्री मान बहादुर राई	वडा अध्यक्ष वडा नं. १६	
१९	श्री विमल राई	कार्यपालिका सदस्य	
२०	श्री डिल्ली सेर राई	कार्यपालिका सदस्य	
२१	श्री बलदेव विष्टकर्मा	कार्यपालिका सदस्य	
२२	श्री गीता कुमारी उप्रेती	कार्यपालिका सदस्य	
२३	श्री रेखा थापा मगर	कार्यपालिका सदस्य	
२४	श्री रेणुका श्रेष्ठ	कार्यपालिका सदस्य	
२५	श्री सुनिता गजमेर	कार्यपालिका सदस्य	
२६	श्री कुन्ती परियार	कार्यपालिका सदस्य	

सचिव
श्री विष्णु भक्त सिग्देल

प्रमुख प्रशासकीय अधिकृत

१४	अनिता दनुवार	का.स.	आ.स्वा.से.के.शाहीखोला	६ महिना
१५	इन्दिरा बि.क.	का.स.	आ.स्वा.से.के.मैरुन	४ वर्ष+
१६	कमला आचार्य	का.स.	आ.स्वा.से.के.साकेला	४ वर्ष+
१७	कमला भट्टराइ	का.स.	आ.स्वा.से.के.बुढचौक	१ वर्ष+
१८	शुभ कुमारी राइ	का.स.	आ.स्वा.से.के.बहेडवा	५ वर्ष+
१९	उमा कुमारी बुढाथोकी	का.स.	आ.स्वा.से.के.करमगाछी	२ वर्ष +
२०	सरणमति राई	का.स.	स्वास्थ्य चौकी साउने	५ वर्ष+
२१	तारा तामाङ माझी	का.स.	आ.स्वा.से.के.वाजगरा	३ महिना
२२	कुमारी राइ	का.स.	स्वास्थ्य चौकी चिलाउने	४ वर्ष+
२३	चित्र बहादुर मगर	का.स.	आ.स्वा.से.के.जति	२ वर्ष +
२४	छुलीमाया प्रजा	का.स.	आ.स्वा.से.के.सुकौरा	४ वर्ष+
२५	कमला अधिकारी	का.स.	आ.स्वा.से.के.सुकौरा	३ वर्ष+

स्वीपर:

सि.न.	नाम थर	पद	कार्यरत संस्था	कैफियत
१	राजेश मरिक्	स्वीपर	स्वास्थ्य शाखा	४ वर्ष+
२	नितु मलिक	स्वीपर	स्वास्थ्य चौकी जोगीदह	३ वर्ष+
३	ललिता मरिक्	स्वीपर	स्वास्थ्य चौकी देउरी	४ वर्ष+
४	इधरी सदा	स्वीपर	स्वास्थ्य चौकी भुमरसुवा	४ वर्ष+

निर्णय नं.४८३

त्रियुगा नगरपालिकाको मुख्य आन्तरिक स्रोतको रूपमा रहेको त्रियुगा, बरुवा र सुनकोशी नदिको नदिजन्य पदार्थको बिक्रि कर संकलनको लागि प्रारम्भिक वातावरणीय परिक्षण (IEE) स्वीकृत गर्नु पर्ने प्रावधान रहेकोमा पूर्वाञ्चल जियो सोलुसन प्रा.लि., विराटनगरसँग IEE का लागि कार्यालयबाट सम्झौता भएकोमा सो फर्म काट पेश भएको कार्यसूची तथा IEE को तपसिल बमोजिमको परिमाण स्वीकृत गरी नियमानुसार ठेका प्रकृत्यामा जानको लागि आवश्यक व्यवस्था गर्ने कार्यालयलाई निर्देशन दिने निर्णय गरियो ।

तपसिल

Handwritten signatures and stamps of various officials, including the Municipal Engineer and other members of the committee, are present below the text.

प्याकेज १: वरुवा खोला र सुनकोशी नदी						
उत्खनन् क्षेत्र / प्लट नं.	क्षेत्र	वार्षिक निकालन सकिने नापजाँच		वार्षिक जम्मा हुने घ.मि.	वार्षिक निकालन सकिने घ.मि.	दैनिक निकालन सकिने घ.मि.
		क्षेत्रफल वर्ग मि.	गहिराई मि.			
१	मुक्तान छहरा (बाँझगरा)	३१७८२	०.९	२८६०३.८	९५३४.६	३५.३१३३३
२	दिम्की	५१६५४	०.८	४१३२३.२	२०६६१.६	७६.५०४४४
३	साकेला	५१३०८	०.९	४६१७७.२	२३०८८.६	८५.५१३३३
	जम्मा	१३४७४४		११६१०४.२	५३२८४.८	१९७.३५११

प्याकेज १: वरुवाखोला / सुनकोशी नदी						
क्र.स	उत्खनन् गरिने स्थान	ढुङ्गा	प्राभेल	वालुवा	साटो	अनुमानित राजस्व(रु.)
१	मुक्तान छहरा (बाँझगरा)	३१७८८	६३५७		-	२३५६९७८.५५
२	दिम्की	३८७.४	२०२७४		-	५१०७६०२.६२
३	साकेला	७६९.६	२२३१९		-	५७०७५६३.४९
	जम्मा	४३३५	४८९५०			१३९७२१४४.७

Handwritten signatures and stamps are present below the tables, indicating official approval or verification of the data.

प्याकेज २: त्रियुगा खोला

उत्खनन् क्षेत्र / प्लट नं.	क्षेत्र	वार्षिक निकालन सकिने नापजाँच		वार्षिक जम्मा हुने घ.मि.	दिगो रुपमा वार्षिक निकालन सकिने नापजाँच घ.मि.	दिगो रुपमा दैनिक निकालन सकिने नापजाँच घ.मि.
		क्षेत्रफल वर्ग मि.	गहिराई मि.			
१	बोजे	३८१९०	०.६	२२९१४	११४४७	४२.४३
२	उत्तराई टोल	४७२७६	०.५	२३६३८	९४५५.२	३५.०१
३	देउरी	१९७९७	०.६	११८७८.२	४७५१.२८	१७.५९
४	फसेट देखी त्रियुगा बसपार्क	२२४५६	०.६	१३४७३.६	५३८९.४४	१९.९६
५	त्रियुगा लुहाले दोभान	५२९६९	०.५	२६४८४.५	१०५९३.८	३९.२३
६	लालभित्ति (त्रियुगा-वैरागी खोला दोभान)	८५८५९	०.५	४२९२९.५	१७१७१.८	६३.५९
७	जोगिदह (त्रियुगा कंग दोभान)	८६५०७	०.५	४३२५३.५	१७३०१.४	६४.०७
	जम्मा	३५३०५४		१८४५७१.३	७६११९.९२	२८१.९२

Handwritten signatures and initials are present below the table, including names like 'Laxman', 'Joshi', and others, indicating official approval or verification of the data.

प्याकेज २: त्रियुगा खोला

प्लट नं.	उत्खनन् क्षेत्र	वार्षिक जम्मा हुने घ.मि.	ढुङ्गा घ.मि.	ग्रावेल घ.मि.	वासुवा घ.मि.	अनुमानित राजध्व ढुङ्गा/ग्रावेलबाट रु.	अनुमानित राजध्व वासुवाबाट रु.	कुल राजध्व प्लट अनुसार रु.
१	बोजे	११४५७		११४५७	-	२८३२२२८	-	२८३२२२८
२	उत्तरार्ड टोल	९४५५.२	१३१४	८१४१.२	-	२३३७३७३	-	२३३७३७३
३	देउरी	४७५१.२८	-	४७५१.२८	-	११७४५४०	-	११७४५४०
४	फसेट देखी त्रियुगा बसपार्क	५३८९.४४	-	५३८९.४४	-	१३३२२९७	-	१३३२२९७
५	त्रियुगा लुहाले दोभान	१०५९३.८	-	-	१०५९३.८	-	२२४४७२०	२२४४७२०
६	लालभिति (त्रियुगा-वैरागी खोला दोभान)	१७१७१.८	-	-	१७१७१.८	-	३६३८५३३	३६३८५३३
७	जोगिदह (त्रियुगा केग दोभान)	१७३०१.४	-	-	१७३०१.४	-	३६६५९९४	३६६५९९४
	जम्मा	७६११९.९२	१३१४	२९७३८.९२	४५०६७	७६७६४३७	९५४९,२४७	१७२२५३८४

Handwritten signatures and stamps are present below the table, including a circular stamp with the text 'Lukung' and '137'.

आज गिने २०८०१०५१७० जतेडा दिन चख त्रियुगा नगरपालिकामा खैबालत भैरहोडा नेपाल शाहिर शासदिय प्रकाशर प्रमोजन (NUPRP) आरम्भ नगरपालिका हाप कनेट गरिडेो सुधारणा कवि कनार वरुनीय नर दामिहा मदन गुरुवाजी सुवाल इई इवरेमी रोल विसतपुट कनर्न १५.६.१२ कारोडो स्वामाजीड प्रकृति रणभैरव जावडागीको लमो फलकनार्न गण्डका (Batek-0900-37000000000) विद्या नारायणीडा न १३ कद समौलप प्रतिविदी र दृषणीपु स्वार्डिनड रोल विडाव र्खि का नार्थान्दा निव चत्पल गर्दयो ।

उपदक्षीन

क्र.सं.	नाम	देगाना	पद	हस्ताक्षर
१.	केशव प्रसाद पखरेल	त्रि.स.पा.१३	रोल विडाव हस्ताक्षर	<i>(Signature)</i>
२.	रवडा नन्द पीडेल	"	सचिव -	<i>(Signature)</i>
३.	उदयव कुंगेल	"	सल्लाहकार -	<i>(Signature)</i>
४.	अनन्त क राई	"	"	<i>(Signature)</i>
५.	राम प्र २ राई	"	"	<i>(Signature)</i>
६.	मनी कुमार्	"	"	<i>(Signature)</i>
७.	चन कुमार् राई	"	"	<i>(Signature)</i>
८.	श्याम कुमारी राई	त्रियुगा.१३	रोल वि. सेवाकर्मी -	<i>(Signature)</i>
९.	मनि कुमार्	"	"	<i>(Signature)</i>
१०.	प्रदिप अधिकारी	"	"	<i>(Signature)</i>
११.	लक्ष्मी चौधरी	"	"	<i>(Signature)</i>
१२.	सीता राई	"	सौज	<i>(Signature)</i>
१३.	गुणी चौधरी	"	खी	<i>(Signature)</i>
१४.	विद्या खत्री	"	द्विषा	<i>(Signature)</i>
१५.	अमरिप बाण	यादल	DSC Engineer	<i>(Signature)</i>
१६.	माला राय	environmental expert		<i>(Signature)</i>
१७.	विनायक पीडेल	सामाजिक विज्ञान	डिप्टी DSC	<i>(Signature)</i>

रूपायणका विषय क्या निर्वाण हवः

१) यस काङ्ग्रेष आगोपना अनन्तगको यस रोलको रूपायण प्रयासि हवः आगोपनाको बारेमा पालकापी जवाडियो ।

- २) यस काङ्ग्रेष आगोपनाको निरूपायण परिशेषना उचितवन (DPA) तथा गर्फा ताजातरणिप तथा सामाजिक उपकारण पन पोषता तथा रीको बाणी तपपानुः संकलन तथा लेखतापोखाना गर्न DSC होखीवः स्वगत गते भगि निर्णय गर्दिगो ।
- ३) काङ्ग्रेषको आलेखार क्षेत्र (COT) स्वगतवना । - यस काङ्ग्रेष आगोपना मित्र कुरीपनि औचित्य संरचना हवः तपने र वकरव विकवाहव पनि तपने हुनाको यस काङ्ग्रेष आगोपनाको निता गणे प्रमथा यत्र क्षेत्रको रूपायण काङ्ग्रेषको कुरीपनि वाद्या अपनो द्य गर्दिगो भकि निर्णय गर्दिगो ।

(Signature) खी

आम्र सिमि २०७०/७१ गतेको फिवा भन्ने त्रियुगा न.पा.सा. संस्थाको अर्देको नेपालका लागि सांकेतिक मुकौट्याङ्ग आयोजना (NUGEP) अन्तर्गतको गन्तु सगरमाथा टुपी लजार पश्चिम - लट फडिण्डा गन्तु ७७०००/१/२/३/४/५/६/७/८/९/१०/११/१२/१३/१४/१५/१६/१७/१८/१९/२०/२१/२२/२३/२४/२५/२६/२७/२८/२९/३०/३१/३२/३३/३४/३५/३६/३७/३८/३९/४०/४१/४२/४३/४४/४५/४६/४७/४८/४९/५०/५१/५२/५३/५४/५५/५६/५७/५८/५९/६०/६१/६२/६३/६४/६५/६६/६७/६८/६९/७०/७१/७२/७३/७४/७५/७६/७७/७८/७९/८०/८१/८२/८३/८४/८५/८६/८७/८८/८९/९०/९१/९२/९३/९४/९५/९६/९७/९८/९९/१००/१०१/१०२/१०३/१०४/१०५/१०६/१०७/१०८/१०९/११०/१११/११२/११३/११४/११५/११६/११७/११८/११९/१२०/१२१/१२२/१२३/१२४/१२५/१२६/१२७/१२८/१२९/१३०/१३१/१३२/१३३/१३४/१३५/१३६/१३७/१३८/१३९/१४०/१४१/१४२/१४३/१४४/१४५/१४६/१४७/१४८/१४९/१५०/१५१/१५२/१५३/१५४/१५५/१५६/१५७/१५८/१५९/१६०/१६१/१६२/१६३/१६४/१६५/१६६/१६७/१६८/१६९/१७०/१७१/१७२/१७३/१७४/१७५/१७६/१७७/१७८/१७९/१८०/१८१/१८२/१८३/१८४/१८५/१८६/१८७/१८८/१८९/१९०/१९१/१९२/१९३/१९४/१९५/१९६/१९७/१९८/१९९/२००/२०१/२०२/२०३/२०४/२०५/२०६/२०७/२०८/२०९/२१०/२११/२१२/२१३/२१४/२१५/२१६/२१७/२१८/२१९/२२०/२२१/२२२/२२३/२२४/२२५/२२६/२२७/२२८/२२९/२३०/२३१/२३२/२३३/२३४/२३५/२३६/२३७/२३८/२३९/२४०/२४१/२४२/२४३/२४४/२४५/२४६/२४७/२४८/२४९/२५०/२५१/२५२/२५३/२५४/२५५/२५६/२५७/२५८/२५९/२६०/२६१/२६२/२६३/२६४/२६५/२६६/२६७/२६८/२६९/२७०/२७१/२७२/२७३/२७४/२७५/२७६/२७७/२७८/२७९/२८०/२८१/२८२/२८३/२८४/२८५/२८६/२८७/२८८/२८९/२९०/२९१/२९२/२९३/२९४/२९५/२९६/२९७/२९८/२९९/३००/३०१/३०२/३०३/३०४/३०५/३०६/३०७/३०८/३०९/३१०/३११/३१२/३१३/३१४/३१५/३१६/३१७/३१८/३१९/३२०/३२१/३२२/३२३/३२४/३२५/३२६/३२७/३२८/३२९/३३०/३३१/३३२/३३३/३३४/३३५/३३६/३३७/३३८/३३९/३४०/३४१/३४२/३४३/३४४/३४५/३४६/३४७/३४८/३४९/३५०/३५१/३५२/३५३/३५४/३५५/३५६/३५७/३५८/३५९/३६०/३६१/३६२/३६३/३६४/३६५/३६६/३६७/३६८/३६९/३७०/३७१/३७२/३७३/३७४/३७५/३७६/३७७/३७८/३७९/३८०/३८१/३८२/३८३/३८४/३८५/३८६/३८७/३८८/३८९/३९०/३९१/३९२/३९३/३९४/३९५/३९६/३९७/३९८/३९९/४००/४०१/४०२/४०३/४०४/४०५/४०६/४०७/४०८/४०९/४१०/४११/४१२/४१३/४१४/४१५/४१६/४१७/४१८/४१९/४२०/४२१/४२२/४२३/४२४/४२५/४२६/४२७/४२८/४२९/४३०/४३१/४३२/४३३/४३४/४३५/४३६/४३७/४३८/४३९/४४०/४४१/४४२/४४३/४४४/४४५/४४६/४४७/४४८/४४९/४५०/४५१/४५२/४५३/४५४/४५५/४५६/४५७/४५८/४५९/४६०/४६१/४६२/४६३/४६४/४६५/४६६/४६७/४६८/४६९/४७०/४७१/४७२/४७३/४७४/४७५/४७६/४७७/४७८/४७९/४८०/४८१/४८२/४८३/४८४/४८५/४८६/४८७/४८८/४८९/४९०/४९१/४९२/४९३/४९४/४९५/४९६/४९७/४९८/४९९/५००/५०१/५०२/५०३/५०४/५०५/५०६/५०७/५०८/५०९/५१०/५११/५१२/५१३/५१४/५१५/५१६/५१७/५१८/५१९/५२०/५२१/५२२/५२३/५२४/५२५/५२६/५२७/५२८/५२९/५३०/५३१/५३२/५३३/५३४/५३५/५३६/५३७/५३८/५३९/५४०/५४१/५४२/५४३/५४४/५४५/५४६/५४७/५४८/५४९/५५०/५५१/५५२/५५३/५५४/५५५/५५६/५५७/५५८/५५९/५६०/५६१/५६२/५६३/५६४/५६५/५६६/५६७/५६८/५६९/५७०/५७१/५७२/५७३/५७४/५७५/५७६/५७७/५७८/५७९/५८०/५८१/५८२/५८३/५८४/५८५/५८६/५८७/५८८/५८९/५९०/५९१/५९२/५९३/५९४/५९५/५९६/५९७/५९८/५९९/६००/६०१/६०२/६०३/६०४/६०५/६०६/६०७/६०८/६०९/६१०/६११/६१२/६१३/६१४/६१५/६१६/६१७/६१८/६१९/६२०/६२१/६२२/६२३/६२४/६२५/६२६/६२७/६२८/६२९/६३०/६३१/६३२/६३३/६३४/६३५/६३६/६३७/६३८/६३९/६४०/६४१/६४२/६४३/६४४/६४५/६४६/६४७/६४८/६४९/६५०/६५१/६५२/६५३/६५४/६५५/६५६/६५७/६५८/६५९/६६०/६६१/६६२/६६३/६६४/६६५/६६६/६६७/६६८/६६९/६७०/६७१/६७२/६७३/६७४/६७५/६७६/६७७/६७८/६७९/६८०/६८१/६८२/६८३/६८४/६८५/६८६/६८७/६८८/६८९/६९०/६९१/६९२/६९३/६९४/६९५/६९६/६९७/६९८/६९९/७००/७०१/७०२/७०३/७०४/७०५/७०६/७०७/७०८/७०९/७१०/७११/७१२/७१३/७१४/७१५/७१६/७१७/७१८/७१९/७२०/७२१/७२२/७२३/७२४/७२५/७२६/७२७/७२८/७२९/७३०/७३१/७३२/७३३/७३४/७३५/७३६/७३७/७३८/७३९/७४०/७४१/७४२/७४३/७४४/७४५/७४६/७४७/७४८/७४९/७५०/७५१/७५२/७५३/७५४/७५५/७५६/७५७/७५८/७५९/७६०/७६१/७६२/७६३/७६४/७६५/७६६/७६७/७६८/७६९/७७०/७७१/७७२/७७३/७७४/७७५/७७६/७७७/७७८/७७९/७८०/७८१/७८२/७८३/७८४/७८५/७८६/७८७/७८८/७८९/७९०/७९१/७९२/७९३/७९४/७९५/७९६/७९७/७९८/७९९/८००/८०१/८०२/८०३/८०४/८०५/८०६/८०७/८०८/८०९/८१०/८११/८१२/८१३/८१४/८१५/८१६/८१७/८१८/८१९/८२०/८२१/८२२/८२३/८२४/८२५/८२६/८२७/८२८/८२९/८३०/८३१/८३२/८३३/८३४/८३५/८३६/८३७/८३८/८३९/८४०/८४१/८४२/८४३/८४४/८४५/८४६/८४७/८४८/८४९/८५०/८५१/८५२/८५३/८५४/८५५/८५६/८५७/८५८/८५९/८६०/८६१/८६२/८६३/८६४/८६५/८६६/८६७/८६८/८६९/८७०/८७१/८७२/८७३/८७४/८७५/८७६/८७७/८७८/८७९/८८०/८८१/८८२/८८३/८८४/८८५/८८६/८८७/८८८/८८९/८९०/८९१/८९२/८९३/८९४/८९५/८९६/८९७/८९८/८९९/९००/९०१/९०२/९०३/९०४/९०५/९०६/९०७/९०८/९०९/९१०/९११/९१२/९१३/९१४/९१५/९१६/९१७/९१८/९१९/९२०/९२१/९२२/९२३/९२४/९२५/९२६/९२७/९२८/९२९/९३०/९३१/९३२/९३३/९३४/९३५/९३६/९३७/९३८/९३९/९४०/९४१/९४२/९४३/९४४/९४५/९४६/९४७/९४८/९४९/९५०/९५१/९५२/९५३/९५४/९५५/९५६/९५७/९५८/९५९/९६०/९६१/९६२/९६३/९६४/९६५/९६६/९६७/९६८/९६९/९७०/९७१/९७२/९७३/९७४/९७५/९७६/९७७/९७८/९७९/९८०/९८१/९८२/९८३/९८४/९८५/९८६/९८७/९८८/९८९/९९०/९९१/९९२/९९३/९९४/९९५/९९६/९९७/९९८/९९९/१०००

क्र.स. उपविभाग

१.	आदिवासी कृषक नारी अखण्ड हो. वि. रत त्रियुगा-१२	सुन्दरी
२.	हेकुवहापुर मगर स्वयंसेवक " " " "	सुन्दरी
३.	सिप्लवा पौडेल विजये कोषाचार्य " " " "	सुन्दरी
४.	राम कृष्ण वि. क. स्वयंसेवक हो. वि. स. " " "	सुन्दरी
५.	प्रतिष्ठा राई " " " "	सुन्दरी
६.	प्रतिष्ठा राई " " " "	सुन्दरी
७.	शान्ता राई " " " "	सुन्दरी
८.	तारा राई " " " "	सुन्दरी
९.	सुनिता वि. क. " " " "	सुन्दरी
१०.	गिता वि. क. " " " "	सुन्दरी
११.	रेखा मगर राई " " " "	सुन्दरी
१२.	नयाज वि. क. " " " "	सुन्दरी
१३.	ध्यान क. सुपेल " " " "	सुन्दरी

१. उद्योगीहरूलाई (पालक) संरक्षणमा : यस कार्य उप आयोजनाको कर्मचारीहरूको सहयोगले गर्दा सम्पन्न भएको प्रमुख कार्यहरूको जानकारी गरिनेछ।

२. कर्मचारीहरू तथा वा ताव रातोपु व्यवस्थापन तहसीलाको लागि तथा उप आयोजनाको कर्मचारीहरूको वातावरणको व्यवस्थापन तथा रोकथामको आवश्यक तथा संरक्षण गर्ने कार्य सम्पन्न गरिनेछ।

३. घर कर्मचारी (कर्मचारी) - यस कार्य उप आयोजना-को पनि घर कर्मचारी, बस, लिफ्ट, र व्यवस्थापन पत्राचार गर्ने हुनाले पनि निम्न कार्यहरूको प्रमुख कार्यहरूको कर्मचारीहरूको साथै निम्न कार्यहरूको प्रमुख कार्यहरूको पनि साथै गर्नुपर्ने भन्ने सबै सम्पन्न निर्णय गरिनेछ।

४. लक्ष्य तथा कार्य संकलन गर्ने संरक्षणमा : यस कार्य उप-आयोजनाको पत्राचारको लागि सम्पन्न कार्यहरूको साथै लक्ष्य तथा लक्ष्य संरक्षणमा सम्पन्न कार्यहरूको संकलन गरिनेछ।

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Consultation with IPs: Chaudhary

आज मिति २०८०/११/१० तिता दिन शब्द त्रिभुगा नगरपालिकामा खानामा संकेतिक नेपाल गवर्न (NUCHDP) अनुदान नगरपालिका द्वारा कोशिका गदिरो, सगरमाथा कोषि नगर पञ्चायत मा २ द्रुपिका गदर गवर्न प्रमाण इड जगुडी नोल विचारपुर म १ १२ नगरी खानाडी, प्रमोड, मिति २ तमा नालनकोशे वागवडीको नगरे अधुने एते आरडा (Bike-Dec - JV - Consultant) त्रिभुगा नगरपालिका, १२ न गडा कार्यालय जगपतिनिधी र विचारपुर हाल विडाए खानाडी येल बासीको गलप्ल गदिरो।

उपस्थित

क्र.सं	नाम	पेशा	दस्तावेज
१.	अनपकुमा	म.प.प.१२	...
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हालपलका विषय

१) शब्द सगरमाथा कोषि वडा न.१२ पञ्चायत नर देहिना गदर प्रवादी संकाय इड जनप्रयोग विनि पुर कौको विद्युत परियोजना प्रतिबन्धे त्रिभुगा नगरपालिका लागी आगुड ति सुडाए संकलन गर्ने काम सक्नु परियो।

२) शब्द सडक प्रोजेक्ट खोडि र अर्कोदिप कन्स्ट्रुक्सन्स।

३) शब्द सडक प्रोजेकामा दुनै विधा नरको र कुनै पाने क सत्यता सडको इन्जिनियर नरको।

४) शब्द सडक प्रोजेकामा दुनै उक्त प्रागाडी र नगरपालिका प्रार नरको। कडाले विने मदी विने काम सञ्चालन हुने जने।

(Signatures)

Meeting minutes with Janpremi Basic School

आज मिति २०८१०६/१५ गतेका दिन मल त्रियुगा नगरपालिका
 सचिवालय नैरेहोडो स्थित शिवाजी प्राथमिक स्कुलमा आयोजना
 (NUGEP) भन्सार नगरपालिका द्वारा खोला गरिएको
 सगरमाथा कोच क्लब पानीपु बन्ध दक्षिण अन्त गण्डकी यन्त्र
 न. १६६ जनप्रेमी टोल निश्चित गर्नका निम्न बाबुको सामाजिक
 आर्थिक प्रतिष्ठे तथा वातावरणीय जायकारीको लागि अन्वय
 गर्दा आरडा (BOT-Geo-IV-Consultant) त्रियुगा नगरपालिका
 र १२ नौ वटा समुदायका प्रतिनिधि र जनप्रेमी आवाकान
 विद्यालय का शिक्षकहरूको सहभागिता गरियो।

उपस्थिति

क्र.सं.	नामधर	पद	हस्ताक्षर
१.	गोपाल पोखरेल	प्र.अ.	<i>[Signature]</i>
२.	चन्द्र महादुर तामाङ	शिक्षक	<i>[Signature]</i>
३.	अर्जुन कुमार कार्की	शिक्षक	<i>[Signature]</i>
४.	लेख डुमाली गण्डाली	"	<i>[Signature]</i>
५.	अद्विजा शर्मा	"	<i>[Signature]</i>
६.	चन्द्र डुमाली पोखरेल	"	<i>[Signature]</i>
७.	बिना कार्की	"	<i>[Signature]</i>
८.	शोभा रिजाल	"	<i>[Signature]</i>
९.	बिना कार्की सामाजिक विकास समिति		<i>[Signature]</i>
१०.	शोभा तामा वातावरण सिला २०८		<i>[Signature]</i>
११.	अमरप राण महेब DSC Engineer		<i>[Signature]</i>

सुभाष तथा हस्तपत्रको विषयमा

१. सडक निर्माण गर्दा वातावरणलाई अक्षर नगरी विद्यालयको पढन-पाठन भएको सुझावको हवाला प्रदुषण न्युन गराई कायम गर्ने सक्ने सम्बन्धित विषयमा कायम नगरी सिद्धान्त कलुका कायम गर्ने।
२. सुभाष सिना भित्री दुवै स्तरिय सडक बनाउनु पर्ने।

[Signatures]

Stakeholder Consultation
Nepal Urban Governance and Infrastructure Project (NUGIP)
Triyuga Municipality
Gajinath, Udayapur

ESIA and ESMP of सगरमाथा कृषि ठाउँ पश्चिमबाट पश्चिम मदन भण्डारी सडक देउ जनप्रेमी टोल
विस्तार सडक

Attendance Sheet of FGD

Date: २०२०-०९-१३ Venue: कुला टोल बिकास Ward No. १२

SN	Name	Organization/Title	Contact no.	Signature
१	आदरा कुमार थापा	उदयपुर लेभोरेटरी	९८४२८९९५०१	[Signature]
२	रमेश कुमार थापा	कृषि	९८४२२२८२८८	[Signature]
३	सिमाना पौडेल	काठमाडौं	९८२५७४३३५५	[Signature]
४	नाम कुमार थापा	सदरमुकाम	९८२५९५६५५५	[Signature]
५	पारसनाथ थापा	"	९८५२८५९५५५	[Signature]
६	मानिषा थापा	"	९८४२२८५५५	[Signature]
७	मानना थापा	"	९८४२५५५५५	[Signature]
८	नाम थापा	"	९८४२५५५५५	[Signature]
९	सुनिता थापा	"	९८४२५५५५५	[Signature]
१०	सुनिता थापा	"	९८४२५५५५५	[Signature]
११	सुनिता थापा	"	९८४२५५५५५	[Signature]
१२	सुनिता थापा	"	९८४२५५५५५	[Signature]
१३	सुनिता थापा	"	९८४२५५५५५	[Signature]
१४	सुनिता थापा	"	९८४२५५५५५	[Signature]
१५	सुनिता थापा	"	९८४२५५५५५	[Signature]

Suggestions/Recommendations:

नोट: यस टोल विकास संचालनलाई सहज गरी
डिजिटल समूहलाई उदाहरण दिने अर्थ दिने
उदाहरणमा सबै काम गरे दिन हुने अनुभव
सबै शक्ति, टोल विकास संचालनमा सबै काम
जनसुधार गर्ने पनि अवसरलाई बाँधेर नहुने गरी
टोल विकास गर्न सहयोग गर्न लगाएर बस्ने हो।
सबै प्रायःलाई सहज गरी सबै काम गर्न हुने
सबै बिना कालो पत्रको जोडफार गर्न गर्नु।

Stakeholder Consultation
Nepal Urban Governance and Infrastructure Project (NUGIP)
Triyuga Municipality
Gajinath, Udayapur

ESIA and ESMP of सगरमाथा कृषि ठाउँ पश्चिमबाट पश्चिम मदन भण्डारी सडक देउ जनप्रेमी टोल
विस्तार सडक

Attendance Sheet of FGD

Date: २०२०-०९-१० Venue: विस्तार Ward No. १२

SN	Name	Organization/Title	Contact no.	Signature
१	सुनिता थापा	उदयपुर लेभोरेटरी	९८४२८९९५०१	[Signature]
२	रमेश कुमार थापा	कृषि	९८४२२२८२८८	[Signature]
३	सिमाना पौडेल	काठमाडौं	९८२५७४३३५५	[Signature]
४	नाम कुमार थापा	सदरमुकाम	९८२५९५६५५५	[Signature]
५	पारसनाथ थापा	"	९८५२८५९५५५	[Signature]
६	मानिषा थापा	"	९८४२२८५५५	[Signature]
७	मानना थापा	"	९८४२५५५५५	[Signature]
८	नाम थापा	"	९८४२५५५५५	[Signature]
९	सुनिता थापा	"	९८४२५५५५५	[Signature]
१०	सुनिता थापा	"	९८४२५५५५५	[Signature]
११	सुनिता थापा	"	९८४२५५५५५	[Signature]
१२	सुनिता थापा	"	९८४२५५५५५	[Signature]
१३	सुनिता थापा	"	९८४२५५५५५	[Signature]
१४	सुनिता थापा	"	९८४२५५५५५	[Signature]
१५	सुनिता थापा	"	९८४२५५५५५	[Signature]

Suggestions/Recommendations:

१. गरीब किसानलाई सबै काम गर्न सहज गरी
डिजिटल समूहलाई उदाहरण दिने अर्थ दिने
उदाहरणमा सबै काम गरे दिन हुने अनुभव
सबै शक्ति, टोल विकास संचालनमा सबै काम
जनसुधार गर्ने पनि अवसरलाई बाँधेर नहुने गरी
टोल विकास गर्न सहयोग गर्न लगाएर बस्ने हो।
सबै प्रायःलाई सहज गरी सबै काम गर्न हुने
सबै बिना कालो पत्रको जोडफार गर्न गर्नु।

Stakeholder Consultation
Nepal Urban Governance and Infrastructure Project (NUGIP)
Triyuga Municipality
Gaighat, Udayapur

ESIA and ESMP of सागरमाथा कृषि बाजार, सगरमाथा नगरपालिका, सप्तरी Road

Attendance Sheet

Date: २०७५/१२ Venue: त्रियुगा नगरपालिका Ward No. ११

Name: कमल कुमर कुमल

Organization/Occupation: नगर प्रमुख त्रियुगा नगरपालिका

Comments/Suggestions:

- चौडा अड्डा चौडा गुणस्तरिय सडक बन्नु पर्ने।
- त्रियुगा नगरपालिकाले सगरमाथाको सडकको स्तरोन्नति गर्न त्रियुगा नगरपालिकाले सडकको मापदण्ड २०२६ बनाउनुको हुँदा गर्न पनि सडकमा मापदण्ड सा. अड्डा स्वरुप सिक्का नपार्ने र गर्ने नीतिमा ध्यान हुनुपर्ने।
- चौडा अड्डा चौडा लेन/निकासा काम हुनु गत सर्त पल्टोको पहल गर्नुपर्ने।
- कठोरपि प्रविधामा हिला भै रहेको हर यसबाट consultant र सिडर बेन्ड काट्न सकिने गर्नुपर्ने।
- स्थानीय स्तरबाट सडक बनाउन हिलाइ गएको अड्डा चौडा हुनुको मापदण्डको फल।


Signature:

कमल कुमर कुमल
नगरप्रमुख त्रियुगा न.पा.

Stakeholder Consultation
Nepal Urban Governance and Infrastructure Project (NUGIP)
Triyuga Municipality
Gaighat, Udayapur

ESIA and ESMP of सागरमाथा कृषि बाजार, सगरमाथा नगरपालिका, सप्तरी Road

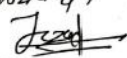
Attendance Sheet

Date: २०७५/१२ Venue: त्रियुगा नगरपालिका Ward No. ११

Name: महेन्द्र बस्नेत

Organization/Occupation: नगर प्रमुख त्रियुगा नगरपालिका

Comments/Suggestions:

- ① प्रकृयाकालागी यथासिद्ध आर्षी कलेड दिन होला।
 - ② हिलो भाग जल नालो आसपासको योजनाको प्रकृया पूरमा हुनु सियो। हिलो पिछीकट आर्षी कलेड दिन होला।
- अबलमा विमोचन योग्य,



Signature:

Stakeholder Consultation
Nepal Urban Governance and Infrastructure Project (NUGIP)
Triyuga Municipality
Gaighat, Udayapur

ESIA and ESMP of Sagarmatha Agriculture Bazar West to South Madan Bhandari Hall to Janpremi tole Bisanpur ward 12 Road
प्रमितिगत एवं (सुधार) आगामीको बाटोको सुर्द, बिक्रम
Attendance Sheet

Date: २०२०/०६/२४ Venue: सिपुगा लोपायुक्तपालिका Ward No. १३
Name: सवित्रा कुमारी शरीरवादी
Organization/Occupation: पशु उपचार विज्ञान शाखा, पाल्पा-१३
Comments/Suggestions:

बडा क्र. १२ धानकोटो हुँदा गामको सुर्द उपकरण अन्तर्गतको
बाटोको रंगभाला नजारे बाटो निर्माण गर्नुको कार्यक्रमको लागि
निर्माण कार्य नयाको को लागि गर्नुको लागि कुनै किसिमको समस्या
होने र को कार्य गणनात्मक रूपमा गर्नु पर्ने र को कार्य रा
गर्ना गर्नुको समताबाट गर्नुको निर्माण हुनुको लागि
प्रगती बोलको वा सुधारको कामको लागि कुनै किसिम
आयोजना नभएको हुँदा र उक्त को बाटो बनेको बाटोको
गर्नुको बाधितहरूको विचार गर्नुको अनुभव हुनुको लागि

Signature:
सवित्रा कुमारी शरीरवादी
पाल्पा



Stakeholder Consultation
Nepal Urban Governance and Infrastructure Project (NUGIP)
Triyuga Municipality
Gaighat, Udayapur

ESIA and ESMP of Sagarmatha Agriculture Bazar West to South Madan Bhandari Hall to Janpremi tole Bisanpur ward 12 Road
प्रमितिगत एवं (सुधार) आगामीको बाटोको सुर्द, बिक्रम
Attendance Sheet of FGD

Date: २०२०/०६/०९ Venue: जनप्रेमी अबाधुत विहारप Ward No. १३

SN	Name	Organization/Title	Contact no.	Signature
1	Chandra B. Tamang	म. गाउँपालिका	९७०११५६१२	[Signature]
2	Arjun K. Karki	"	९७६२३२०१०	[Signature]
3	Kesho Karki	"	९७६०२९८०९	[Signature]
4	Indira Karki	"	९७६०२४३६२०	[Signature]
5	Shambhu Karki Paudel	"	९७४२९६९९९९	[Signature]
6	Bina Karki Karki	"	९८५१५११२२	[Signature]
7	Shobha Rijal	"	९७४२८३७०९२	[Signature]

Suggestions/Recommendations:
रोड बन्नु राम्रो कुरा हो। रोड निर्माण गर्दा वातावरणीय प्रदूषण
नहुने गरी पर्यावरण सन्तुलन कायम राख्नुको लागि विकसित
अवधारणा अन्तर्गत विकास विभागले निर्माण होस् भन्ने
सुझाव तथा सल्लाह दिन चाहान्दौं।

Stakeholder Consultation
Nepal Urban Governance and Infrastructure Project (NUGIP)
Triyuga Municipality
Gaighat, Udayapur

ESIA and ESMP of गाईसात बाजार सडकको अपग्रेडिशन अर्थात् सडकको बजार Road

Stakeholder Consultation
Nepal Urban Governance and Infrastructure Project (NUGIP)
Triyuga Municipality
Gaighat, Udayapur

ESIA and ESMP of पुसपानीको दोस्रो सि. नो. १ र २ को बायाँ कोट्टीको Road
बाजार भागको कच्चा नाला र सडकको अपग्रेडिशन अर्थात्
Attendance Sheet

Attendance Sheet
Date: २०२०/१२/२४ Venue: त्रियुगा नगरपालिका Ward No. ११
Name: सुमन कोइराला
Organization/Occupation: एन एन आर प्रोग्राम सारवाहिका
Comments/Suggestions:

Date: २०२०/१२/२४ Venue: Ward No. १२
Name: अमित शाय
Organization/Occupation: का. सा. नगर विकास त्रियुगा न.पा. १२
Comments/Suggestions:

यस बजारमा गर्ने बिचको ५ को नजारीमा इने ५m कसवासी
मा अक्कास गराइयो. अन इन्सुलना बिचो काम हिनारु दिन
हिला आएको कारण यस नगरपालिका र वडा कार्यालयमा नै राखि
ना कारणले छ। अब चाँडो कामो चाँडो ५-८ वटा
एवा डेममा रोड र निजाली आ सामलाई सिक्का दिन
हुन थपमा सुलभन सम्पुण रिमलाई अडलाई
हाई सुझाव दिन चाहिन्छ।

हाम्रो त्रियुगा नगरपालिका वडा नं. ११ नगरपालिका को नजिकको बजारमा
नजिकको नडा अरुता पनि निवास को सुकालमा अवस्थी पढ्दा
परैको कडा सि. नो. १ र २ बाझ बिचको कडा ले, गो वडा नं. १२ को
विशिन्य लाये नाली स्थाहित भिच हुने कुरा ले न नगरपालिका भन्ने
हाथो नजारीको सुझाव हो, लाये अरुता पनि स्थापना कुरा अरु
करण ले यहामा सम्पुण बजार कार्यालय, निष्कर्षित, अंण्डित भइस
काठिन्छ जो इमीलकोयो अमेका गर्ने धन आनत जालका अरु
सामग्री अरुको कुरा, तर अहिले पिच्यो भन्ने लाजे कुरा ले हामीलाई
अरु बजारको लाई बिचको र पढ्नुको आपदा जस्ता कुमो भइसुछ
अरुको छ। नाली ले गर्दा पानि ले बिचासा भिच ले गर्दा अरु
को सुर्यतीका अण्ड भइवस्थी शनिभे खेलेको छ। लाये पिचले
गर्दा धार्मिक गहन, परमेश्वरीको लक्षणावता सोझको राजो बडको
आर्थिक र धर्ममा पनि रेना पुग्ने भिक्तात छ। बिचको कडा
लाई अरु धेरै अलग्गै अरुको र अरुको बडाको बजारको को तर्फ तय
सुझावपन समुझोत हुने निष्काष र प्रतिबद्धता पाले अरु
गर्नु चाहेछु।



Signature:
का. सा. नगरपालिका
त्रियुगा-११

Signature:
(Handwritten Signature)
का. सा. वडा नगरपालिका

Stakeholder Consultation
 Nepal Urban Governance and Infrastructure Project (NUGIP)
 Triyuga Municipality
 Gaighat, Udayapur

ESIA and ESMP of गाईबाट (बिवालय) होल आर्टिस्टरोला रोड, बिसनपुर वार्ड १२ Road

Attendance Sheet

Date: २०७३.०१.१२ Venue: त्रियुगा नगरपालिका Ward No १०
 Name: किरण पाण्डे
 Organization/Occupation: गाईबाट सिपुगा कार्यालय
 Comments/Suggestions:

१. सडक बनाउने कार्य दिव्य भएको हुँदा यथा सिधु चाँडो सडक बनाउनु पर्नेछ।
२. युवाहरूले र विद्युत बिक्री आपसमा अडुवाको अडुवाको बनाउनु पर्नेछ।
३. यस गाईबाट बिवालय होल आर्टिस्टरोला र माईलेडेमा मनमोहन कल्लु देसि कम्प्याछी गाईबाट दिक्तेवा बाटु बसमा (१०००) सडक अचिकार हेम १० मिटर आएकोले कुनै पनि भौतिक हानि हुने छैन। र कुनै पनि सिपाइ हुने छैन।
४. यो सडक चाँडो बनाइने गरि यस क्षेत्रको ब्यावसायिक विकासले चाँडै साहाय पत्रनेछन कृषी, व्यापार, लडनेछ।


 Signature:

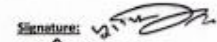
Stakeholder Consultation
 Nepal Urban Governance and Infrastructure Project (NUGIP)
 Triyuga Municipality
 Gaighat, Udayapur

ESIA and ESMP of गाईबाट (बिवालय) होल Road

Attendance Sheet

Date: _____ Venue: बिवालय होल सिपुगा Ward No ११
 Name: डा. वि. कुमार गुब्बारा
 Organization/Occupation: अव्यय डिवाय होल सिपुगा बस
 Comments/Suggestions:

१. यस गाईबाट सडक टक्काले, सडक नगरपालिकाको मापदण्डको नमोजिम १२ मिटर देखि १४ मिटर सम्मको बनाउनु पर्नेछ।
२. यस सडकमा कुनै पनि भौतिक हानि हुने छैन। तपनि हुँदा कुनै पनि खराब नरहेको।
३. ताका बसहरूको पाठि लाग्ने सडक बनाउनु पर्नेछ।


 Signature: डा. वि. कुमार गुब्बारा
अव्यय डिवाय होल सिपुगा



Stakeholder Consultation
Nepal Urban Governance and Infrastructure Project (NUGIP)
Triyuga Municipality
Gaighat, Udayapur

ESIA and ESMP of त्रिभुगा नगरपालिका क्षेत्रका विभिन्न सडक बाटोहरू Road

Attendance Sheet

Date: २०८०/०६/१० Venue: त्रिभुगा नगरपालिका Ward No. १५
Name: विष्णु शर्मा शिरोके
Organization/Occupation: प्रमुख प्रशासकिय अधिकृत

Comments/Suggestions:
→ प्राथमिकतामा परेका सडकहरूमा कुनै किसिमको चर्याकिसवा नभएको कडा बलाबलामा कुनै किसिमको प्रभाव नदेखाउने।
→ त्रिभुगा नगरपालिकाको नगरमित्रता सडक सरोतनी गर्ने त्रिभुगा नगरपालिकाले आफ्नो सडक नगरो सुबन्दाची मापदण्ड २०५५ बनाई काम गरियो कडा कुनै पनि सडकहरूमा ज्यामीजान धर, चरा, जग्गा, छाप किफवा तपी र कुनै भीतिक्रम नभने। सरकोले काम घिरो सुक हुनु पर्ने।
→ प्राथमिकतामा परेका सडकहरूको नाडी गढा चाडी टिकवा प्रकथ।
उर्छे काम सुक हुनु पर्ने।


Signature:
प्रमुख प्रशासकीय अधिकृत


Stakeholder Consultation
Nepal Urban Governance and Infrastructure Project (NUGIP)
Triyuga Municipality
Gaighat, Udayapur

ESIA and ESMP of गाईसात बाटो, देवनाले, भैरु वि. गा.वे. सडक Road
पुस्तकालय देखि वि. वि. नाँउ सडकको टोलो

Attendance Sheet

Date: २०८०/५/१६ Venue: त्रिभुगा नगरपालिका Ward No. १२
Name: राप्रकली विम्वरानी
Organization/Occupation: वडा सदस्य त्रिभुगा-१२

Comments/Suggestions:
म राप्रकली वि. वि. वन १२ को दलित महिला सदस्य हुँ। त्रिभुगा न. पा. वडा न. १२ को हकमा यहा भुजोलास विम्वर कोठ्यामे प्रबधाना पहाडित ह रैफ लर मानविय र सामाजिक विम्वरना पाजे पढाजी परेको ह। मर यहास भौतिक र वा. २ जागमा मा पढुच आर्थिक पहुच्य भागदानीमा दिन दिये उपाला कृमपुपी जापी दैसिमि चलथन पन भएकमा रहेको ह। महिला हड को सकेमा प्रति कम भौतिक र आर्थिक उपायको उपायमा कडाको भए परेकासले जेका होपने, उपायन पढुपने दबिहा कदमपने भएकमा ह लक्ष्य महिलालाई पनि समर्थ पढुपने र परभोगी बना बये उपथन, उनीहरुको लागि आर्थिक लाभ जने सभ्य सिप प्रकथ तालिम, महिला हड लाई सभ्यता सभ्यगिख्यात तालिम, महिला एकाकि करण वा विद्ययाभा लेही तालिम, सहचालन गार्हे पाएमा ठुलो लाभ भई लभजना उपान्तरण जने सकेमा ह भन्ने आशयसले यो पेशा जने चाहे लक्ष्य केही समर्थ भन्ना दलित महिला पिढीसका जातिस महिला ले काप्रमा सिप रहेको सिलप कुनै केही प्रकथन आर्थिकमा पाए उपथनपै लाग्नाको सभाए भन्ना जातिसका, राजगामीमा परिभाषा जपी हकी दलित महिलालाई पढुपने सगत कुरा र। उका समर्थ ले उमा विवेदन जने चाहे।

Signature:
राप्रकली विम्वरानी
वडा सदस्य त्रिभुगा-१२


Municipal documents related to GBV cases

१. हिंसाको प्रकृति विवरण : २०७७-७८




क्र.स	हिंसाको प्रकार	जम्मा संख्या	कैफियत
	➤ घरेलू हिंसा	३०	
१	शारीरिक	१३	
२	बहुविवाहा	३	
३	गालीगलौज	४	
४	मानसिक यातना	२	
५	चारीभिक आरोप	६	
६	स्रोत सुविधाबाट वन्चितीकरण	२	
	➤ सामाजिक हिंसा	१६	
१	कुटुपिट	१	
२	गालीगलौज		
३	बोक्सिको आरोप		
४	जबरजस्ती विवाहा		
५	बाल विवाहा	१५	
	➤ यौन हिंसा	३२	
१	बलात्कार	२१	
२	बलात्कारको प्रयास	२	
३	सामूहिक बलात्कार		
४	सामूहिक बलात्कारको प्रयास		
५	हाडनाता करणी		
६	यौन दुर्व्यवहार	४	
७	अलपत्र	२	
८	गर्भवती भई अलपत्र	३	
९	मानव बेचबिखन		
१०	आत्मा हत्या		
	जम्मा	७८	



१. हिंसाको प्रकृति विवरण : २०७८-७९

क्र.स	हिंसाको प्रकार	जम्मा संख्या	कैफीयत
	➤ घरेलु हिंसा	२९	
१	शारीरिक	२९	
२	बहुविवाहा	६	
३	गालीगलौज	७	
४	मानसिक यानना	६	
५	चारीत्रिक आरोप	०	
६	स्रोत सुविधावाट वन्चितीकरण	४	
	➤ सामाजिक हिंसा	६	
१	कुटुपिट		
२	गालीगलौज	१	
३	बोक्सको आरोप	०	
४	जबरजस्ती विवाहा	०	
५	बाल विवाहा	७	
	➤ यौन हिंसा	११	
१	बलात्कार	७	
२	बलात्कारको प्रयास	४	
३	सामुहिक बलात्कार		
४	सामुहिक बलात्कारको प्रयास		
५	हाडनाता करणी		
६	यौन दुर्व्यवहार		
७	हत्या		
८	हत्याको प्रयास		
९	मानव बेचबिखन		
१०	अन्य, अज्ञपत्र	११	
	जम्मा	६६ जना	

F/Y 2079186



१. हिंसाको प्रकृति विवरण

क्र.स	हिंसाको प्रकार	जम्मा संख्या	कैफीयत
	➤ घरेलु हिंसा	६८	
१	शारिरिक	२५	
२	बहुविवाहा	५	
३	गालीगलौज	२०	
४	मानशिक यातना	१३	
५	चारीत्रिक आरोप	३	
६	स्रोत सुविधाबाट बन्चितीकरण	२	
	➤ सामाजिक हिंसा	५	
१	कुटपिट	०	
२	गालीगलौज	०	
३	वोक्सको आरोप	०	
४	जबरजस्ती विवाहा	०	
५	वाल विवाहा	५	
	➤ यौन हिंसा	१५	
१	बलात्कार	१३	
२	बलात्कारको प्रयास	२	
३	सामुहिक बलात्कार		
४	सामुहिक बलात्कारको प्रयास		
५	हाडनाता करणी		
६	यौन दुर्व्यवहार		
७	हत्या		
८	हत्याको प्रयास		
९	मानव बेचबिखनको प्रयास	१	
१०	अन्य,अलपत्र	५	
	जम्मा	९४ जना	

नोट सुरक्षावासभिन्न बसेर सेवा लिने संख्या ९४ र बाहिरबाट मनोविमर्श सेवा,कानुनी सहयोग,कानुनी परामर्श,पारिवारी परामर्श जस्ता लगाएको सेवालिनैहरुको संख्या गरी जम्मा ६५ जनाले सेवा पाएका छन भने सुरक्षाआवासमा सेवाग्राहि संग आएको आश्रीत वालवालिकाको संख्या ३० जना गरी यस वर्ष १२४ जनालाई सुरक्षावासभिन्न सेवा प्रदान गरीएको छ ।

Letter from Triyuga Small Town Water supply and Sanitation user Committee


द.नं.१/०५३/०५४ फोन नं. : १,०३५-४२०२४३

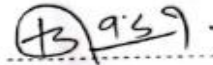
 **त्रियुगा साना शहरी खानेपानी तथा सरसफाई उपभोक्ता समिति (संस्था)**
त्रि.पा. १०, बागेश्वर उदयपुर
२०८०/०५१ मिति : २०८०/०५/२५

पत्र संख्या : ०८०/०५१
चलानी नम्बर : ४
विषय : लागत स्टीमेट गरी पठाईएको सम्बन्धमा ।

श्रीमान् नगर प्रमुख ज्यू,
त्रियुगा नगरपालिकाको कार्यालय, गाईघाट उदयपुर


उपरोक्त सम्बन्धमा तहाँ कार्यालयको च.नं. २५७ मिति २०८० ०४/२९ को पत्रबाट व्यहोरा अवगत भई NUGIP कार्यक्रम अन्तर्गत तहाँ कार्यालयबाट छनीट भएका बडा नं. ११ र १० का विभिन्न सडकहरुमा हाल सञ्चालनमा रहेका खानेपानी पाईपलाईनहरु बन्न नागेका सडकका दायीं बायाँ रहेकामा उक्त पाईपलाईनहरुको लागत स्टीमेट गरी पठाईदिने भन्ने पत्र अनुसार लागत स्टीमेट गराई यसै पत्र साथ संलग्न राखि पठाईएको व्यहोरा पत्रसाथ सादर अनुरोध छ ।




श्रीम बहादुर खड्का
का.वा. अध्यक्ष
का.वा. अध्यक्ष

त्रि.पा. कार्यालय
द.नं. ३४६
मिति २०८०/५/२५
शाखा

Letter from NEA for pole relocation

 **नेपाल विद्युत प्राधिकरण**
(नेपाल सरकारको स्वामित्व)
वितरण तथा ग्राहक सेवा निर्देशनालय
कोशी प्रदेश प्रादेशिक कार्यालय विराटनगर
उदयपुर वितरण केन्द्र
गाइघाट

■ कार्यालय प्रमुख ०३१-४२२६२९
■ प्रशासन ०३१-४२२६४९

प.सं.: २०८०/०८१ च.नं.: १६६ मिति : २०८०/०९/०२

श्री त्रियुगा नगरपालिका
त्रि.न.पा.११, गाइघाट

विषय: लागत इस्टिमेट पठाइएको बारे ।

उपरोक्त सम्बन्धमा तैहा कार्यालयबाट प.स. २०८०/०८१ को च.न. २५८ को पत्रअनुसार NUGIP कार्यक्रम अन्तर्गत तथा कार्यालय बाट छनोट भएका वडा न.१०,११ र १२ का विभिन्न सडकको किनारामा हाल संचालनमा रहेका विद्युतिय संरचनाहरूलाई वन्न लागेका नयाँ सडकको दाया बाया किनारामा रहने गरी नयाँ विद्युतिय संरचनाको लागि लागत इस्टिमेट गरी यसै पत्र साथ संलग्न राखी पठाइएको व्यहोरा पत्रसाथ अनुरोध छ ।

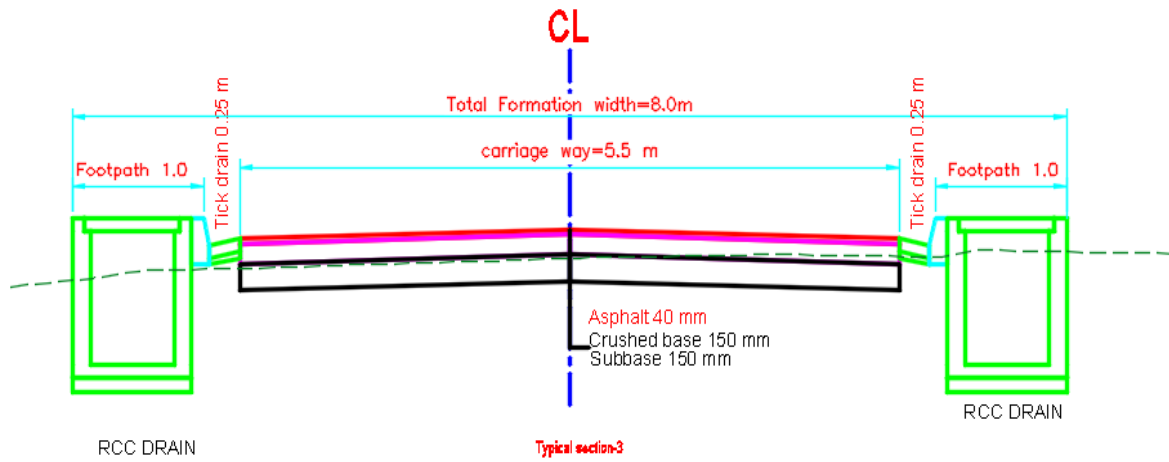
ई. विजय
११/१२

वि.न.पा. ११, गाइघाट
११/१२

ई. विजय बाबु खत्री
केन्द्र प्रमुख

केन्द्र प्रमुख

Annex III: Proposed Typical Cross Sections



Annex IV: GoN Permissible Environmental limits/standards

(A) National Drinking Water Quality Standard, 2079 BS

A-1: Mandatory Parameters to be tested

SN	Parameters	Unit	Limits	Remarks
	Physical			
1	Turbidity	NTU	5	
2	pH		6.5 - 8.5	
3	Colour	TCU	5	
4	Taste & odour		Unobjectionable	
5	Electrical Conductivity	µS/cm	1500	
	Chemical			
6	Iron	mg/L	0.3 (3)	
7	Manganese	mg/L	0.20	
8	Arsenic	mg/L	0.05	
9	Fluoride	mg/L	0.50 - 1.50 (Min. - Max.)	
10	Ammonia	mg/L	1.50	
11	Chloride	mg/L	250	
12	Sulphate	mg/L	250	
13	Nitrate	mg/L	50	
14	Copper	mg/L	1	
15	Zinc	mg/L	3	
16	Aluminum	mg/L	0.20	
17	Total Hardness	mg/L	500	
18	Residual Chlorine	mg/L	0.10 - 0.50 (Min. - Max.)	
	Microbiological			
19	E-Coli	(CFU/10 ml)	0	

A-2: Additional Parameters to be tested based on Risk and Requirement

SN	Parameters	Unit	Limits	Remarks
	Physical			
1	Total Dissolved Solids	mg/L	1000	
	Chemical			
2	Calcium	mg/L	200	
3	Lead	mg/L	0.01	
4	Cadmium	mg/L	0.003	
5	Chromium	mg/L	0.05	
6	Cyanide	mg/L	0.07	
7	Mercury	mg/L	0.001	
8	Nitrites	mg/L	3	
	Microbiological			
1	Total Coliform	(CFU/10 ml)	0 (In 95% samples)	

(B) National Ambient Air Quality Standard, 2069 BS

Parameters	Units	Averaging Time	Concentration in Ambient Air, Maximum
TSP	µg/m ³	24 - hours	230
PM ₁₀	µg/m ³	24 - hours	120
PM _{2.5}	µg/m ³	24 - hours	40
Sulfur Dioxide	µg/m ³	Annual	50
		24-hours	70
Nitrogen Dioxide	µg/m ³	Annual	40
		24-hours	80
Carbon Monoxide	µg/m ³	8hours	10000
Lead	µg/m ³	Annual	0.5
Benzene	µg/m ³	Annual	5
Ozone	µg/m ³	8-hours	157

Ref.: Section 62, Number 19, Nepal Gazette, Part 5, 2069/04/29, Notice 2

(C) National Sound Pressure Level, 2069

Microenvironment	Sound Pressure Level, L _{eq} dB(A)	
	Daytime	Nighttime
Industrial Area	75	70
Commercial Area	65	55
Rural Settlement Area	45	40
Urban Settlement Area	55	50
Mixed Settlement Area	63	55
Pristine Area	50	40

Ref.: Section 62, Number 30, Nepal Gazette Part 5, 2069/7/13

(D) Diesel Powered Generator Emission Limits (g/kWh), 2069

Category, (kW)	CO	HC	NO _x	PM
kW < 8	8	1.3	9.2	1
8 = kW < 19	6.6	1.3	9.2	0.85
19 = kW < 37	6.5	1.3	9.2	0.85
37 = kW < 75	6.5	1.3	9.2	0.85
75 = kW < 130	5	1.3	9.2	0.7
130 = kW < 560	5	1.3	9.2	0.54

Ref.: Section 62, Number 30, Nepal Gazette Part 5, 2069/7/13

The minimum height of the chimney should be maintained not less than 11m for the industrial boiler utilizing solid or liquid fuel.

Annex V: Air, Noise, Water Quality Test Reports



**ENVIRONMENT MANAGEMENT
&
ANALYSIS SERVICES P. LTD**

Regd No.: 127787/071/72
Jwagal, Lalitpur, Nepal
GPO Box No.: 8975, EPC 5296
Contact No.: 977-01-47690266, 9851126060
Email: emas@emas.com.np, emasenv@gmail.com
Website: emas.com.np

ANALYSIS REPORT FOR AMBIENT AIR QUALITY

Report Number	26A/080-81		
Sample Number	17A/080/81		
Client	Bosphorous Technical Consulting Corp (BOTEK), GOEC Nepal P. Ltd		
Sampling location	Triyuga Municipality Ward No. 11, Gaighat townplanning area Road Section (1.5Km)	GPS point: 26°47'30.40"N 86°41'57.31"E	
Project	Upgrading of different inter connected Road Sections(10.59Km), Gaighat townplanning area Road Section (1.5Km), City hall*simaltar Gaighat Diktel road section (2.88 Km), Babarani DM Gate Road Section (0.65Km), Fulchowk PG Chowk Road Botetole Triyuga Corridor section (0.86Km), City Hall Ramchandra Basnet house Road Section (0.28Km), Gaighat Shivalay tole-Adarsha tole-ramailo danda manmohan school to karamgachhi diktel connecting, Road Section (2.25Km), Pragati tole kha (Quarter front) ko road to sakela road ward 11, 13 (2.17Km)		
Sampled by	Environment Management And Analysis Services P. Ltd., Dillibazar, Kathmandu		
Sampling Date	07 - 08 September 2023		
Report Date	20 September, 2023		
Instrument used	Respirable Dust Sampler (GTI 151) and Combined Sampler (GTI 241)		
Result			
Parameter	NAAQS	Observed Values	Method
TSP ($\mu\text{g}/\text{m}^3$)	230.0	164.5	IS 5182 (Part 14)-2000 (reaffirmed 2005)
PM ₁₀ ($\mu\text{g}/\text{m}^3$)	120.0	77.3	IS 5182 (Part -23):2006
PM _{2.5} ($\mu\text{g}/\text{m}^3$)	40.0	32.0	IS 5182: Part 24: 2019
SO ₂ ($\mu\text{g}/\text{m}^3$)	70.0	8.8	IS 5182 (Part 2)-2006
NO ₂ ($\mu\text{g}/\text{m}^3$)	80.0	8.3	IS 5182 (Part 6)-2006
CO ($\mu\text{g}/\text{m}^3$)	10000.0	<230.0	IS 5182 (Part - 10):2006

* - National Ambient Air Quality Standard, 2069, ** - National Ambient Air Quality Standard for TSP for Crusher Industry, IS Indian Standard

Remarks: The observed values are within the prescribed limit of NAAQS.

Sampled by

Checked by

Authorized by

**Environment Management
&
Analysis Services P. Ltd**



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Website: emas.com.np

ANALYSIS REPORT FOR AMBIENT AIR QUALITY

Report Number	27A/080-81		
Sample Number	17A/080/81		
Client	Bosphorous Technical Consulting Corp (BOTTEK), GOEC Nepal P. Ltd		
Sampling location	Triyuga Municipality Ward No. 11, City hall simaltar Gaighat Diktel road section (2.88 Km)		GPS Point: 26°48'11.59"N 86°42'20.16"E
Project	Upgrading of different inter connected Road Sections(10.59Km), Gaighat townplanning area Road Section (1.5Km), City hall simaltar Gaighat Diktel road section (2.88 Km), Babarani DM Gate Road Section (0.65Km), Fulchowk PG Chowk Road Botetole Triyuga Corridor section (0.86Km), City Hall Ramchandra Basnet house Road Section (0.28Km), Gaighat Shivalay tole-Adarsha tole-ramailo danda manmohan school to karamgachhi diktel connecting, Road Section (2.25Km), Pragati tole kha (Quarter front) ko road to sakela road ward 11, 13 (2.17Km)		
Sampled by	Environment Management And Analysis Services P. Ltd., Dillibazar, Kathmandu		
Sampling Date	07 - 08 September 2023		
Report Date	20 September, 2023		
Instrument used	Respirable Dust Sampler (GTI 151) and Combined Sampler (GTI 241)		
Result			
Parameter	NAAQS *	Observed Values	Method
TSP ($\mu\text{g}/\text{m}^3$)	230.0	131.4	IS 5182 (Part 14)-2000 (reaffirmed 2005)
PM ₁₀ ($\mu\text{g}/\text{m}^3$)	120.0	58.4	IS 5182 (Part -23):2006
PM _{2.5} ($\mu\text{g}/\text{m}^3$)	40.0	14.1	IS 5182: Part 24: 2019
SO ₂ ($\mu\text{g}/\text{m}^3$)	70.0	6.3	IS 5182 (Part 2)-2006
NO ₂ ($\mu\text{g}/\text{m}^3$)	80.0	8.9	IS 5182 (Part 6)-2006
CO ($\mu\text{g}/\text{m}^3$)	10000.0	<230.0	IS 5182 (Part - 10):2006

* - National Ambient Air Quality Standard, 2069, ** - National Ambient Air Quality Standard for TSP for Crusher Industry, IS Indian Standard

Remarks: The observed values are within the prescribed limit of NAAQS.

[Signature]
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[Signature]
Authorized by

**EMAS Environment Management
&
Analysis Services P. Ltd**



**ENVIRONMENT MANAGEMENT
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Website: emas.com.np

ANALYSIS REPORT FOR AMBIENT NOISE LEVEL

Report Number	29N/080-81			Sample Number	18-N/080/81				
Sampling Date	07 - 08 September 2023								
Client	Bosphorous Technical Consulting Corp (BOTEK), GOEC Nepal P. Ltd								
Sampling location	Triyuga Municipality Ward No. 11, Gaighat townplanning area Road Section (1.5Km)			GPS point: 26°47'30.40"N 86°41'57.31"E					
Project	Upgrading of different inter connected Road Sections(10.59Km), Gaighat townplanning area Road Section (1.5Km), City hall simaltar Gaighat Diktel road section (2.88 Km), Babarani DM Gate Road Section (0.65Km), Fulchowk PG Chowk Road Botetole Triyuga Corridor section (0.86Km), City Hall Ramchandra Basnet house Road Section (0.28Km), Gaighat Shivalay tole-Adarsha tole-ramailo danda manmohan school to karamgachhi diktel connecting, Road Section (2.25Km), Pragati tole kha (Quarter front) ko road to sakela road ward 11, 13 (2.17Km)								
Sampled by	Environment Management And Analysis Services P. Ltd., Dillibazar, Kathmandu								
Report Date	20 September, 2023								
Instrument used	LUTRON SL - 4033SD								
Test method	Noise Measurement Protocol (Central Pollution Control Board, India)								
Hours	Time	Noise Level (dBA)			Hours	Time	Noise Level (dBA)		
		Lmax	Lmin	Leq			Lmax	Lmin	Leq
06:00 - 07:00	Day	71.3	46.4	51.0	18:00 - 19:00	Night	69.1	44.1	49.1
07:00 - 08:00		80.3	49.4	56.6	19:00 - 20:00		74.4	49.9	48.5
08:00 - 09:00		77.1	55.6	54.6	20:00 - 21:00		67.2	43.1	48.4
09:00 - 10:00		75.8	53.2	52.9	21:00 - 22:00		57.9	41.8	44.6
10:00 - 11:00		83.0	51.4	61.9	22:00 - 23:00		55.5	43.0	47.6
11:00 - 12:00		85.3	54.6	50.4	23:00 - 00:00		59.2	39.3	44.9
12:00 - 13:00		78.3	49.5	52.9	00:00 - 01:00		60.7	38.9	44.2
13:00 - 14:00		82.7	48.3	63.3	01:00 - 02:00		61.3	41.5	47.9
14:00 - 15:00		87.4	46.6	51.8	02:00 - 03:00		62.9	42.5	47.9
15:00 - 16:00		89.0	46.6	49.8	03:00 - 04:00		61.3	46.7	49.6
16:00 - 17:00		81.9	47.1	54.2	04:00 - 05:00		64.3	44.5	50.3
17:00 - 18:00		77.7	48.3	41.0	05:00 - 06:00		59.2	45.6	50.1
Leq average (Day)				54.2	Leq average (Night)				47.8
Permissible limit *				65.0	Permissible limit				55.0

dBA - A-weighted decibels, Lmax : Maximum Sound Level, Lmin: Minimum Sound Level

Remarks: The equivalent noise level during the day and night hours comply with the permissible limit in reference to noise level standard 2069, under category of commercial area.

Sampled by

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**ENVIRONMENT MANAGEMENT
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Website: emas.com.np

ANALYSIS REPORT FOR AMBIENT NOISE LEVEL

Report Number	30/N/080-81			Sample Number	18-N/080/81				
Sampling Date	07 - 08 September 2023								
Client	Bosphorous Technical Consulting Corp (BOTTEK), GOEC Nepal P. Ltd								
Sampling location	Triyuga Municipality Ward No. 11, City hall simaltar Gaighat Diktel road section (2.88 Km)			GPS Point: 26°48'11.59"N 86°42'20.16"E					
Project	Upgrading of different inter connected Road Sections(10.59Km), Gaighat townplanning area Road Section (1.5Km), City hall simaltar Gaighat Diktel road section (2.88 Km), Babarani DM Gate Road Section (0.65Km), Fulchowk PG Chowk Road Botetole Triyuga Corridor section (0.86Km), City Hall Ramchandra Basnet house Road Section (0.28Km), Gaighat Shivalay tole-Adarsha tole-ramailo danda manmohan school to karamgachhi diktel connecting, Road Section (2.25Km), Pragati tole kha (Quarter front) ko road to sakela.road ward 11, 13 (2.17Km)								
Sampled by	Environment Management And Analysis Services P. Ltd., Dillibazar, Kathmandu								
Report Date	20 September, 2023								
Instrument used	LUTRON SL - 4033SD								
Test method	Noise Measurement Protocol (Central Pollution Control Board, India)								
Hours	Time	Noise Level (dBA)			Hours	Time	Noise Level (dBA)		
		Lmax	Lmin	Leq			Lmax	Lmin	Leq
06:00 - 07:00	Day	65.2	48.7	52.2	18:00 - 19:00	Night	61.3	46.2	48.6
07:00 - 08:00		60.8	49.1	51.5	19:00 - 20:00		66.3	47.3	49.4
08:00 - 09:00		66.8	53.1	54.2	20:00 - 21:00		57.2	42.5	48.1
09:00 - 10:00		71.9	52.8	53.7	21:00 - 22:00		55.0	39.1	51.5
10:00 - 11:00		69.8	49.7	52.4	22:00 - 23:00		61.2	37.3	47.7
11:00 - 12:00		65.2	50.7	54.3	23:00 - 00:00		63.2	36.1	47.1
12:00 - 13:00		67.2	51.5	56.2	00:00 - 01:00		59.5	37.2	48.4
13:00 - 14:00		65.0	51.7	55.4	01:00 - 02:00		64.6	42.4	46.6
14:00 - 15:00		63.2	50.3	52.7	02:00 - 03:00		59.4	44.5	49.8
15:00 - 16:00		66.2	49.1	53.2	03:00 - 04:00		62.4	47.3	49.4
16:00 - 17:00		63.5	47.2	50.5	04:00 - 05:00		71.9	46.7	48.6
17:00 - 18:00		71.9	48.7	51.4	05:00 - 06:00		63.2	49.7	51.8
Leq average (Day)				53.1	Leq average (Night)				48.9
Permissible limit *				65.0	Permissible limit				55.0

dB(A) - A-weighted decibels, Lmax : Maximum Sound Level, Lmin: Minimum Sound Level

Remarks: The equivalent noise level during the day and night hours comply with the permissible limit in reference to noise level standard 2069, under category of commercial area.

[Signature]
Sampled by

[Signature]
Checked by

[Signature]
Authorized by

**Environment Management
&
Analysis Services P. Ltd**



**ENVIRONMENT MANAGEMENT
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Email: emas@emas.com.np, emasenv@gmail.com
Website: emas.com.np

Water Analysis Report

Client: Bosphorous Technical Consulting Corp (BOTEK), GOEC Nepal P. Ltd
Sample Location: Triyuga Municipality Ward No. 11, City hall simaltar
Gaighat Diktel road section (2.88 Km)
GPS Point: 26°48'11.18"N 86°42'20.53"E
Report No: 32/W/080-81
Report Date: 20 September, 2023
Sampled by: EMAS P. Ltd.
Project: Upgrading of different inter connected Road Sections(10.59Km),
Gaighat townplanning area Road Section (1.5Km), City hall simaltar
Gaighat Diktel road section (2.88 Km), Babarani DM Gate Road Section
(0.65Km), Fulchowk PG Chowk Road Botetole Triyuga Corridor section
(0.86Km), City Hall Ramchandra Basnet house Road Section (0.28Km),
Gaighat Shivalay tole-Adarsha tole-ramailo danda manmohan school to
karamgachhi diktel connecting, Road Section (2.25Km), Pragati tole kha
(Quarter front) ko road to sakela road ward 11, 13 (2.17Km)

Sample Sources: Hand Pump
Owner: Bidur Katwal
Year of installation: 2006-
Depth of hand pump: 18 feet
Sampling Date: 08 September, 2023
Received Date: 09 September, 2023
Analysis Period: 09 - 12 Sept, 2023

Parameters	Unit	NDWQS	Observed Values	Test Methods
pH	-	6.5 - 8.5	7.2	4500-H ⁺ B, APHA, 22nd EDITION
Colour	-	5 (15)	<0.1	2120 B, APHA, 22 nd EDITION
Turbidity	NTU	5 (10)	2.5	2130 B, APHA, 22nd EDITION
Electrical Conductivity	µS/cm	1500	148.0	2510 B, APHA, 22nd EDITION
Temperature (Lab)	°C	-	24.0	2550 B., APHA, 22nd EDITION
Total Hardness	mg/l as CaCO ₃	500	68.0	2340 C, APHA, 22nd EDITION
Chloride	mg/l	250	3.2	4500-Cl ⁻ B, APHA, 22nd EDITION
Ammonia	mg/l	1.5	<0.02	4500-NH ₃ C., APHA, 17 th EDITION
Nitrate	mg/l as NO ₃	50	0.5	4500-NO ₃ - B., APHA, 22nd EDITION
Iron	mg/l	0.3 (3)	<0.01	3112 B., APHA, 22nd EDITION
Manganese	mg/l	0.2	<0.02	3112 B., APHA, 22nd EDITION
Arsenic	mg/l	0.05	<0.01	3114 C, APHA, 22nd EDITION
E.Coli	CFU/100 ml	Nil	Nil	9221 C., APHA, 22nd EDITION

NDWQS: National Drinking Water Quality Standard (2079), * - Values are upper and lower limit, () - Values are acceptable only when alternative is not available. APHA: American Public Health Association, N/A : Not Availalable

Remarks: Observed values of the parameters are within the NDWQS.

Analyzed by

Checked by

Authorized Signature

**Environment Management
&
Analysis Services P. Ltd**

Annex VI: List of Zebra Crossings

SN	Name of the Road	Chainage	Nos.	Remarks
1	Sagarmatha_1	0+020.25	1	
2		0+121.5	1	
3		0+163.5	1	
4		0+275.25	1	
5		0+365.25	1	
6		0+413.5	1	
7		0+489.5	1	
8		0+639.75	1	
9		0+549	1	
10		0+600.75	1	
11		0+728.5	1	
12		1+146	1	
13		1+194.5	1	
14	Sagarmatha_2	0+058	1	
15		0+170.5	1	
16		0+199.75	1	
17		0+458.25	1	
18		0+496.5	1	
19		0+942	1	
20		1+011.25	1	
21		1+164.5	1	
22		1+130.5	1	
23		1+457.5	1	
24		1+502	1	
25		1+644.25	1	
26		1+579	1	
27		2+153.5	1	
28	Sagarmatha_3	0+019.983	1	
29		0+231.25	1	
30		0+359.954	1	
31		0+531.483	1	
32		0+568.685	1	
33		0+690.701	1	
34	Sagarmatha_4	0+026.5	1	
35		0+129.25	1	
36		0+236.5	1	
37		0+293	1	
38		0+520	1	
39		0+421.75	1	
Total			39	Nos.

Annex VII: Code of Conduct (CoC) on GBV

नेपाल शहरी शासकीय तथा पुर्वाधार आयोजना

कार्य स्थलमा हुने यौनजन्य तथा महिला हिंसा सम्बन्धी आचार सहिता

व्याक्तिगत आचार सहिता

म,यो आचार सहिता पालना गर्नु मेरो दाहित्व हो भनी स्वीकार गर्दछु। म कुनै पनि यौनजन्य तथा महिला हिंसा जस्ता कार्यमा संलग्न हुने छैन। परियोजना को काम को शिलसिलामा यो आचार सहिता पालना गर्न सहमत छु।

१. म जातजाति धर्म, भाषा, लिङ्ग, उमेर, राजनीतिक वा सामाजिक हैसियत, भौगोलिकता, पहुच, वैवाहिक स्थिती वा अन्य कुनै पनि आधारमा भेदभाव नगरी सबैलाई सम्मानजनक र समान रुपमा व्यवहार गर्नेछु।
२. सामाजिक सन्जालको प्रयोग गरी अश्लील शब्द, दुष्य सामग्री वा कार्यलय समय अधिपछी वार्तालाप मार्फत सहकर्मि/कामदार लाई यौन दुर्व्याहार गर्ने छैन।
३. कार्यस्थलमा सिङ्गी बजाउने, चुम्बन गर्ने, व्याक्तिगत उपहार दिने आदि जस्ता कार्य गरी कर्मचारी, सहकर्मि/कामदार लाई यौन दुर्व्याहार गर्ने छैन।
४. कुनै पनि प्रलोभन/ धम्की देखाई (जस्तै पदोन्नती लोभ देखाएर, जागीर नदिने धम्की दिएर शोषण गरेर आदि) यौन दुर्व्याहार पक्षमा संलग्न हुने छैन।
५. कार्य समयवाधि भित्र कुनैपनि मादराजन्य तथा लागुपदार्थको सेवन गर्ने छैन।
६. परियोजना सरोकारवाला वा वरपरका समुदायका सदस्यहरूलाई कुनैपनि म लैङ्गिक हिंसा तथा यौनजन्य दुर्व्याहार गर्ने छैन।
७. कुनै पनि कर्मचारी/श्रमिक विरुद्ध हिंसा गरिएको दोषी ठहरिएमा प्रचलित साधय, प्रादेशिक, स्थानीय सरकार वर्ल्ड बैंक को कानून, निती नियम अनुसार सजाय/ दण्डित जरिवाना तिर्न तयार हुनेछु।
८. कार्य गर्ने शिलसिलामा सम्मानजनक निर्देशनहरूको पालना गर्दछु (वातावरणीय + सामाजिक)
९. मेरो जिम्मेवारी कुशलता र लगनशीलता पुर्वक पुरा गर्नेछु।

१०. सम्बन्धित कार्यलय / कम्पनीले सन्चालन गरेको विभिन्न प्रशिक्षण कार्यक्रममा सक्रिय रुपमा भाग लिनेछु।
११. परियोजनाका प्रत्यक्ष लाभदायक सदस्य/समुदायमा यौन दुर्व्याहार/शोषण गर्ने छैन।
१२. विश्वासनीयता नैतिक उल्लघनको रिपोर्ट गरेमा कुनै कामदार विरुद्ध बदला लिने छैन।
१३. कार्य स्थलमा लैङ्गिक सम्बेदनशिल भाषाको प्रयोग गर्दछु।
१४. कार्यस्थलमा महिला हिंसा तथा यौनजन्य क्रियाकलाप लाई प्रोत्साहन गर्ने खालका गतिविधी गर्न दिने छैन।
१५. कार्यस्थलमा महिला तथा यौन हिंसा गतिविधीहरूलाई प्रोत्साहन गर्ने छैन।
१६. १८ वर्षभन्दा मुनिका बालिकाहरूमा कुनै डिजीटल मिडीया मार्फत वा कुनै माध्यमबाट /स्वीकृती लिई वा नलिई यौनजन्य क्रियाकलापमा सहभागी हुनेछैन, यदि नाबालिका स्वीकृती लिई यौनजन्य क्रियाकलापमा गरेमा क्षमा हुदैन।
१७. परियोजना कार्यन्वयन को बेलामा यौनजन्य दुर्व्याहार / यौन शोषण भएमा वा आचार सहिता उल्लघन गरेमा वडा/ नगरपालिका स्तरमा रहेको गुनासो सुनवाई सयन्त्रमा तुरुन्त निवेदन/जानकारी दिनेछु।
१८. कार्यस्थलमा कसैले यौनजन्य दुर्व्याहार सम्बन्धी शक्कापद व्यावहार गरेमा वा शक्कापद कार्य गरेमा तुरुन्त टोली प्रमुख /प्रबन्धकलाई जानकारी/निवेदन दिनेछु।

माथि उल्लेखित आचार सहिता राम्ररी पढे र बुझेको छु र कार्यस्थलमा कडाईका साथ पालना गर्दछु भनी हस्ताक्षर गर्दछु।

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व्यवस्थापक/टोली प्रमुख

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कर्मचारी/कामदार

Annex VIII: Stakeholder Consultation Details

Sagarmatha Agriculture bazar west to south Madan Bhandari Hall to Janpremi tole Bisanpur ward 12 Road

Date	Consultation Type	Composition of Participants	Issues Raised	Response from Project
2080/05/07	FGD	Saraswoti Mahatara, and other Raudidas tole members	<ul style="list-style-type: none"> • Very poor condition of the existing road and very difficult for the pedestrians. • No any environmental and social issues. 	We clearly told them that the work will be completed on time. Environmental issues will be address properly during the construction phase.
2080/05/10	FGD	Keshab Prasad Pokhrel and other member of, Hawaiifield tole bikas, Ward-13	<ul style="list-style-type: none"> • Very necessary to start this work as soon as possible. • Must be quality road structures. • Local people must be used for the labor work force. 	Project will be completed on time. Quality of the road will be monitor regularly by the concern authorities. Local manpower will be used as far as possible.
2080/05/13	FGD	Aadesh Kumar Rai and other member of Yakata Tole bikas, Ward-12	<ul style="list-style-type: none"> • Requested for the fast work to be completed. • Willing to help for the project as far as possible. • No GBV and other social related issues. 	Project will address all the environmental and social issues if any arise during the construction stage as well as operational stage. Awareness program will be provided to the local people.
2080/06/10	FGD	Gauri Narayan Chaudhary and other locals, Ward 12, Bisanpur	<ul style="list-style-type: none"> • Mainly emphasized on the quality of road and the material that will be used during the construction. • Must be safe and clean side drain. 	We clarify that project will monitor for the quality and side drain will be made clear.
2080/06/09	FGD	Chandra Bahadur Tamang and other teacher of Shree Janapremi Basic School, Ward-13	<ul style="list-style-type: none"> • Focused on the quality road structure. • Must be completed on time. • Must be drainage on both side of the road 	Project emphasis that there will be no compromise on the quality of the road structure. Proper monitoring will be done to the contractor.

Date	Consultation Type	Composition of Participants	Issues Raised	Response from Project
2080/05/07	Meeting	Laxman Dhakal, and other member of Raudidas tole.(Participants: 13M/9F, including 6 IP members)	<ul style="list-style-type: none"> • Must start work as soon as possible • Must be environmentally friendly • No any obstacles for the completion of the road project 	We made them clear about the completion date of the project and also mention that the environmental and social issues will be address properly.
2080/05/10	Meeting	Keshab Prasad Pokhrel and other member of , Hawaiifield tole bikas and Locals, Ward-13 (Participants: 13M /4F, including 9 IP members)	<ul style="list-style-type: none"> • Public are positive for construction. • Construction of road needs to tender fast. • Women empowerment should be focused • Public are okay and will help for the project to complete smoothly. • Environmental issues should be addressed on timely and properly 	Made clear about the completion date of the project and made sure that all other issues will be address accordingly.
2080/05/13	Meeting	Aadesh Kumar Rai and other member of Yakata Tole Ward-12 (Participants: 5M/8F, including 7 IP members)	<ul style="list-style-type: none"> • ROW is clear and people are very willing to help the project. • Must be good quality of road. • Trading for the local people must be provided. • Dust and other environmental problems must be settled by the project during the construction of the road 	Completion time was stated and quality of the road is assured. Environmental and social problems will be addressed promptly.
2080/06/10	Meeting	Shrawan Kumar Chaudhary and other tole members of Bisanpur tole ,	<ul style="list-style-type: none"> • Road ROW is clear municipality in 2055 BS. • No issues on the environmental and social part. • Women empowerment must be taken into consideration. Road safety must be taken seriously during construction phase. 	Different training program, awareness campaign will be done by the concern authorities. Road safety and safety signboard will be

Date	Consultation Type	Composition of Participants	Issues Raised	Response from Project
		Ward-12 (Participants: 14M/5F, All IPs)		installed on every alignment on the regular interval and as per need.

Annex IX: Photographs

Pictorial highlights of the field work



Consultation with Locals of Bisanpur tole



Meeting with people of Ekta tole



Meeting with local of Hawaii field tole area



Consultation with Locals of Raudidas area of Sagarmatha Road



Meeting with Teacher of Jana Premi Basic School



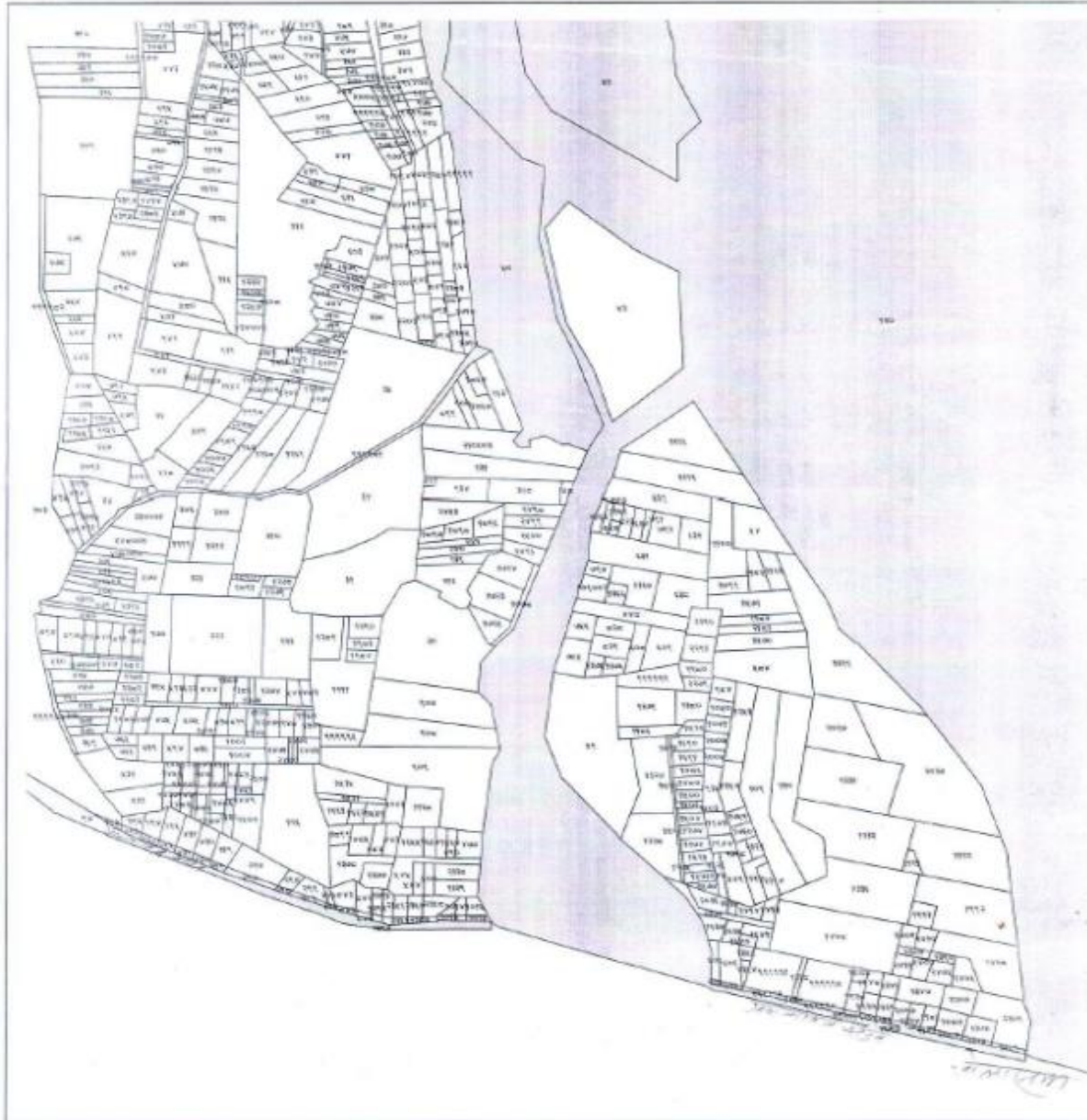
नेपाल सरकार
भूमि व्यवस्था, सहकारी तथा गरिबी निवारण मन्त्रालय
नापी विभाग

र.न००००

मिति २०८०।०६।१२ वार्ड नं.५ख

ब्लु प्रिन्ट र टेसको सटामा उपलब्ध गराइएको

स्केल १ : २,५००



भूमिलिने

प्रिन्ट गर्ने

प्रमाणित गर्ने

नोट: ब्लु प्रिन्ट र टेसको सटामा उपलब्ध गराइएको छ।



भूमि व्यवस्था, सहकारी तथा गरिबी निवारण मन्त्रालय
नापी विभाग

नापी कार्यालय, उदयपुर, साईघाट
गा.वि.स.साईघाट

र.नं०००००

मिति २०८०।०६।२५, बाई नं.६क

ब्लु प्रिन्ट र टेसको सटामा उपलब्ध गराइएको

स्केल १ : २,५००



भूमिलिने

प्रिन्ट गर्ने

प्रमाणित गर्ने

नेपाल सरकार, भूमि व्यवस्था, सहकारी तथा गरिबी निवारण मन्त्रालय, काठमाडौं



भूमि व्यवस्था, सहकारी तथा गरिबी निवारण मन्त्रालय
नापी विभाग

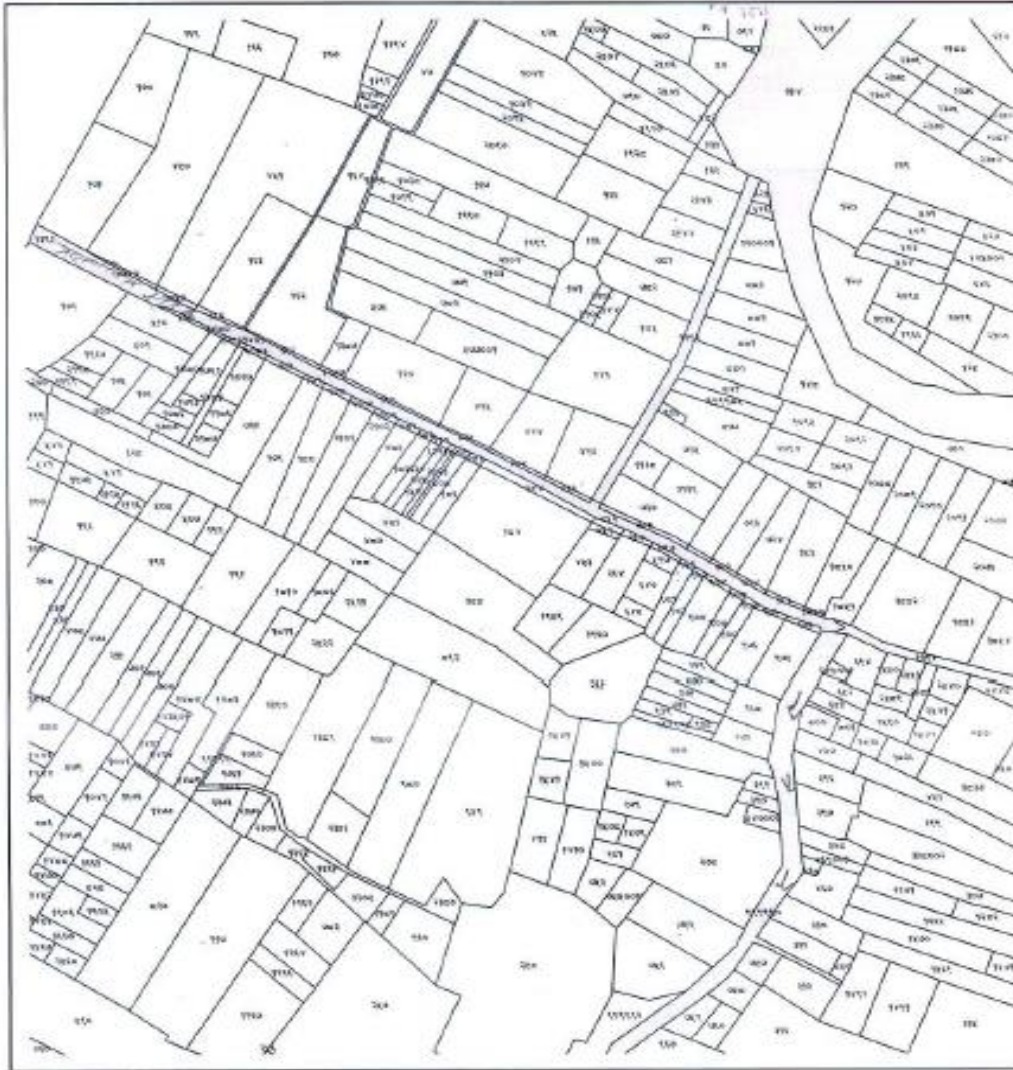
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मिति २०८०/०५/१०

नापी कार्यालय, उदयपुर, गाईघाट
गा.वि.स.गाईघाट
वार्ड नं.१२



जु प्रिन्ट र टेसको सटामा उपलब्ध गराइएको

स्केल: २:५०

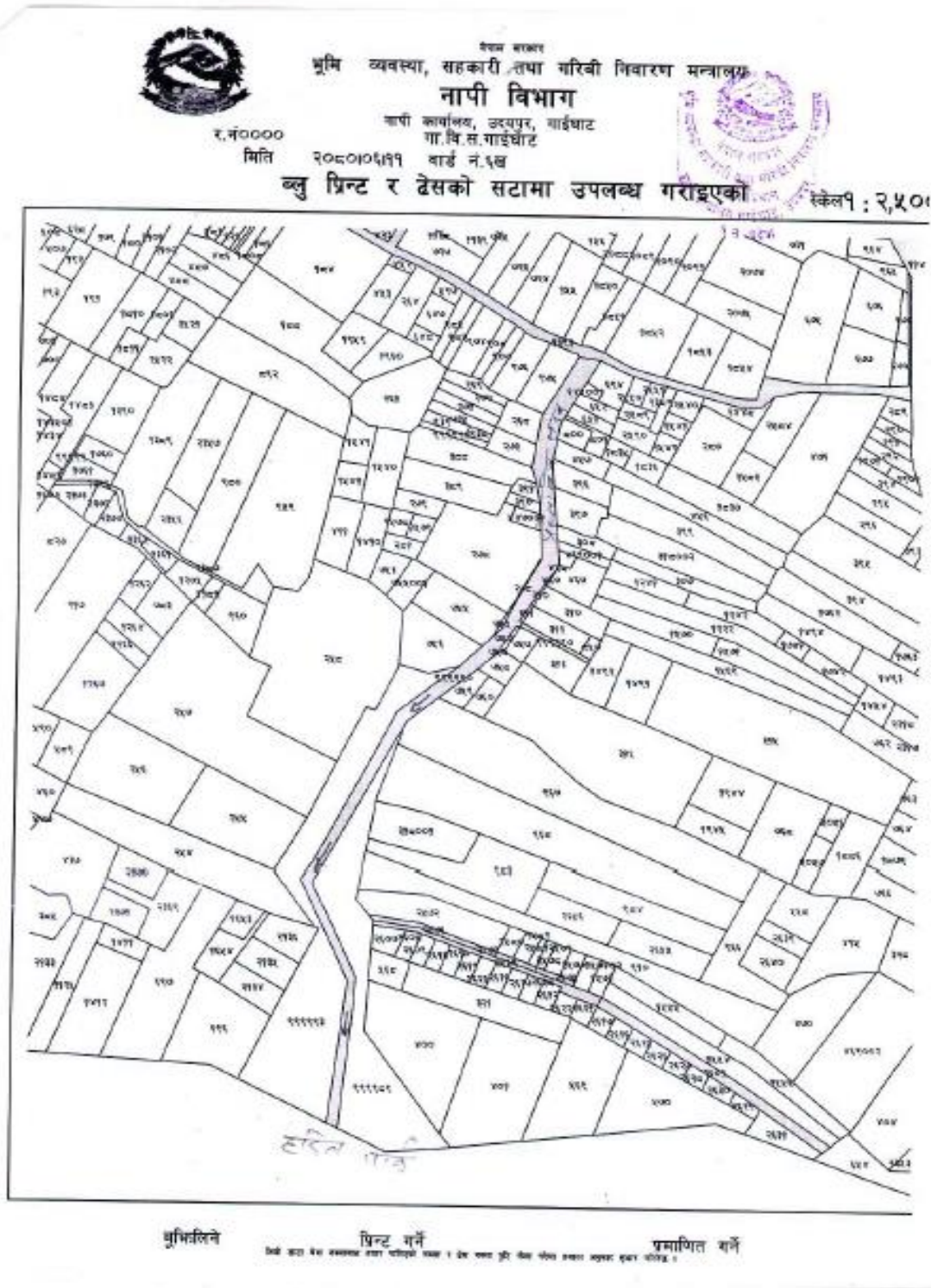


भूमिलिपि

प्रिन्ट गर्ने

प्रमाणित गर्ने

योको प्रिन्ट र टेसको सटामा उपलब्ध गराइएको छ ।



Annex XI: Alternative Route



Note: Red lines are proposed road and green lines are alternative routes.

Environmental and Social Management Plan (ESMP)

Nepal Urban Governance and Infrastructure Project (NUGIP)

**Upgradation of different inter connected Road
(9.929Km)**

Triyuga Municipality
Udayapur District, Koshi Province
Nepal

March 2024

EXECUTIVE SUMMARY

Introduction

The upgrading of 9.929 Km length of different inter connected road sections of Triyuga municipality have different seven interconnected roads namely Gaighat town planning area road (1.534Km), City hall- Simaltar-Gaighat- Diktel road (2.86Km), Babarani DM Gate road (0.622Km), Fulchowk PG Chowk Road Bote Tole Triyuga Corridor road (0.90Km), City Hall- Ramchandra Basnet house road (0.255Km), Gaighat Shivalay Tole-Adarsha Tole-Ramailo Danda Manmohan School to Karamgachhi Diktel connecting road section (1.60Km), Pragati Tole kha (In front of Quarter) road to Sakela road ward 12 (2.158Km) which are interconnected each other through two highways. These roads are starts from highway (Sagarmatha and Madan Bhandari) and intersects in different locations of these two highways and ends at different locations. These roads pass through ward no 10, 11, 12 and 13 of Triyuga Municipality. The proposed subproject is located in plain area of inner Terai and passes mainly through mixed land use having urban settlement and cultivated land. The project alignment does not run through ecologically sensitive area such as protected area, conservation area, wetland, national parks etc.

Baseline Information

The subproject is located in plain area of inner Terai at the average altitude of 192.668 masl with an average yearly temperature is 17.42°C (63.36°F) as well as average annual precipitation of sub project area is 131.8mm. The altitude of the municipality is ranges from 70m to 2509m from sea level. The proposed road alignment lies in the Sub Himalayan range. The alluvial river deposit is the most predominant along the road alignment. Road alignment does not cross any major rivers. Most of the area of present land use of Triyuga Municipality includes forest 76%, cultivated land 20%, barren land 2% and river 2%. In the proposed road alignment and Right of Way (RoW), there is no landslide/flood area and other disaster-prone area. The air and noise quality of surrounding areas is affected by the local city vehicles as well as Highway vehicles. The Noise and air quality of project area were tested and found within government standards. During the construction of the road, vehicular circulation along the road is likely to be disturbed. Trees like Pipal (*Ficus religiosa*), Banana-kvare (*Musa acuminata Colla*), Supari (Areca Catechu), Sissoo (*Dalbergia Sissoo*), Mango (*Magnifera indica*), Bakaino (*Melia azadirach*), Ashoka (*Saraca asoca*), Amba (*Psidium guajava*) and Banana (*Musa balbisiana*) are found in the project area.

The total population of Triyuga Municipality is 102,725 and total household is 25623. The average household size of the Municipality is 4.01 which is lower than that of district household size (4.20). The male and female population of the municipality is 48,463 and 54,262 respectively and sex ratio is 89.31. The total population and household of project affected wards - ward 10, 11, 12 and 13, is 33,958 and 8,618 respectively. The average household size of the project wards is 3.95. Total male population of the project wards is 15,971 and total female population of the project wards is 17,987. The sex ratio of the project wards 88.84. The project area is inhabited by Brahmin, Chhetri, Chaudhary/Tharu, Rai, Magar, Newar, Pariyar, Biswokarma, Muslim, Bote, Majhi, Danuwar, Sharki and Mushar. In terms of religion, Hindus, Christians and Buddhist reside in these communities. There is presence of mixed groups and cluster of groups of castes in the project area. The indigenous peoples in project area are Chaudhary/Tharu, Rai, Magar, Newar, Bote, Majhi, and Danuwar. The total households along the road alignment are 882nos. The majority of the people of this area are involved in business, agricultural work and foreign employment. Water supply pipelines of Triyuga small town water supply committee provides drinking water supply in this area but there is no any integrated wastewater disposal

system in the municipality. Within the construction width of the road, there is 167 nos. of electric poles identified for relocation. There are few issues related to the GBV, SEA/SH were identified from field survey and records of nearby Police Office. Necessary mitigation programs have been planned and will implement during construction of sub project. The most prioritized issues on violence for planning of mitigation program are Domestic violence (physical and mental abuse), child marriage and sexual violence/ rape.

Legal and Regulatory Requirements

The sectoral and cross-sectoral guidelines and standards promulgated by the Government of Nepal (GoN) in various periods are adequate to mainstream the environmental and social safeguard dimensions in the project preparation and implementation phases. The report has included the applicable GoN plan, policies, act, regulations, guidelines, and standards. Similarly, the report has also included the environmental and social Safeguard requirements of the World Bank.

Screening, Scoping, Impact identification, Prediction and Management

The Direct Impact area of the project is considered as the 20m, 18m, 12m, 10m and 8m RoW for different interconnected roads of the proposed subproject for the entire length of the road. Similarly, the indirect impact falls within 50 meters from the edge of the RoW. Environmental and social checklists were used for screening and summarizing the overall impacts. The site-specific impacts in construction and operation phases are included in the ESMP report. Some of the impacts include:

Beneficial Impacts from the Project Implementation

This sub-project prioritizes hiring from the local community, especially those facing economic hardship, poor and belonging to ethnic minorities. It creates jobs, strengthens local businesses, and improves accessibility. Resident participation fosters valuable skills and employability, leading to personal growth. Furthermore, enhanced road infrastructure connects residents to markets, social services, and opportunities, ultimately improving their quality of life. Efficient transportation benefits education, healthcare, tourism, and the broader socio-economic landscape. Additionally, an expanded network may increase land values, further enriching the community.

Adverse Impacts from the Project Implementation

Increase in construction workers could overwhelm local services like roads, water, and sanitation. Dust, noise, and water pollution from construction activities may degrade the environment. Stockpiled materials and solid waste can erode soil during construction and operation. Traffic and rainwater runoff might erode nearby agricultural land. The operation of this road may result in expansion of settlement area and market which may increase encroachment of RoW. Similarly, the increase in dust, smoke and smoke of vehicles can increase air and noise pollution to some extent.

Mitigation Measures

The report proposes various measures to enhance benefits and mitigate adverse impacts, aiming to make this project environmentally friendly. Local people specially poor and vulnerable will be given priority for employment opportunities. Necessary measures like water spray during construction, air, noise monitoring, water sample testing will be taken to reduce the adverse effects on environment. The workers will be insured

and provided with first aid facilities, separate camps and safety equipments like PPE. Provision of safe, clean and hygienic workplace and adequate WASH facilities at campsite. Code of Conduct to be implied for the workforce. For plantation on edge of the road, local species of plant will be given priority. Appropriate bio/civil engineering structures will be adopted to protect the environment. Proper drainage system will be provided to prevent accumulation of water on the nearby agricultural lands during operation. Adequate road safety measures like information and warning signs, diversion during construction will be managed to minimize road accident. Water supply pipelines, and electrical poles to be reinstated without delay.

Sexual exploitation/Assault/Harassment Prevention and response Action Plan

Based on the SEA/SH Risk Assessment checklist and assessment carried out for NUGIP by the World Bank, the Project's SEA/SH risks are assessed to be "Low". An SEA/SH Risk Mitigation Action Plan has been developed for NUGIP based on this assessment and includes specific measures that aim to prevent and mitigate GBV, in particular SEA/SH risks that the project activities might trigger. The Plan has also addressed "Table – 1: Recommended actions to address SEA/SH Risks in IPF Projects" as per the "Good Practice Note" published by the World Bank in September 2018.

Environment and Social Management Plan

The Environmental and Social Management Plan (ESMP) has been proposed with this report including issues identified, possible effects and impacts, measures for their mitigation, monitoring methods. The mitigation cost for environmental and social impacts in construction phases are included in report. In addition, agencies responsible for executing environmental and social mitigation measures and monitoring have been identified in the ESMP. Different monitoring indicators on the physical, biological, socio-economic and cultural environment have also been identified and presented in tabular form. The project along with the stakeholders will monitor during construction and operation phase.

Project-related grievances will be addressed through grievance redress mechanism (GRM) established for the project for the uptake and timely response on stakeholders' query and concerns about sub project. The committee can be provided with grievances through any of the mediums like written, verbal, telephone, letter, etc. and the committee will process it following the procedures of the project, and if not solvable, it will be forwarded to the higher level of GRM. Also, for mitigation of GBV, SEA/SH present with in the project area, the necessary orientation, workshops and trainings has been proposed in this ESMP report.

Institutional arrangements

The Ministry of Urban Development (MoUD) has set up a Project Coordination Office (PCO) under the Department of Urban Development and Building Construction (DUDBC) to implement NUGIP. The PCO is responsible for overall project compliance including compliance with environmental and social measures. The PCO will be supported by a Project Management Support Team (PMST). A Project Implementation Unit (PIU) will be established in Triyuga Municipality for implementation of the subproject at the local level and will be responsible for implementation of the ESMP and other environmental and social instruments. Technical Assistance will be provided through a Design and Supervision Consultancy (DSC) which includes safeguards specialists.

कार्यकारी सारांश

परिचय:

त्रियुगा नगरपालिकाको विभिन्न अन्तरसम्बन्धित सडकखण्डको ९.९२९ किलोमिटर लम्बाइको गाईघाट नगर योजना क्षेत्र सडक (१.५३४ किमी), सिटीहल-सिमलटार-गाईघाट-दिक्तेल सडक (२.८६ किमी), बबरानी डिएम गेट सडक (०.६२२ किमी), फुलचोक पीजी चोक सडक बोटे टोल त्रियुगा करिडोर सडक (०.९० किमी), सिटी हल-रामचन्द्र बस्नेत घर सडक (०.२५ किमी), गाईघाट शिवालय टोल-आदर्श टोल-रमाईलो डाँडा मनमोहन स्कुलदेखि करमगाछी दिक्तेल जोड्ने सडक खण्ड (१.६० किमी), प्रगति टोल ख (क्वार्टर अगाडि) सडकदेखि साकेला सडक वडा नं १२ (२.१५८ किमी) गरी सात वटा अन्तरसम्बन्धित सडक स्तरोन्नति कार्य गर्नको लागि प्रस्ताव गरिएको छ। प्रस्तावित सडकहरू दुइवटा राजमार्गबाट एकअर्कासँग जोडिएका छन् । यी सडकहरू सगरमाथा र मदन भण्डारी राजमार्ग बाट सुरु भएर यी दुई राजमार्गका विभिन्न स्थानहरू पार गर्दै जान्छन र विभिन्न स्थानहरूमा समाप्त हुन्छन्। यो सडक त्रियुगा नगरपालिकाको वडा न. १०,११, १२ र १३ हुँदै जान्छ । सगरमाथा र मदन भण्डारी राजमार्गको सङ्गमस्थलमा रहेको व्यापारिक क्षेत्र गाईघाट बजार हुँदै र मिश्रित भू-उपयोग, ग्रामीण बस्ती हुँदै यि उप-आयोजना जान्छ । प्रस्तावित उप-आयोजनाको सबै खण्ड समतल क्षेत्रमा अवस्थित छ र बस्ती र कृषि भूमि हुँदै जान्छ। यस आयोजनाको बाटोको वरिपरिका वस्तीका स्थानीयहरूको जिवनस्तर र जीविकोपार्जनमा सुधार ल्याउनेछ। संरक्षित क्षेत्र, संरक्षण क्षेत्र, सिमसार क्षेत्र, राष्ट्रिय निकुञ्ज जस्ता पारिस्थितिकीय दृष्टिले संवेदनशील क्षेत्रमा आयोजना पर्दैन ।

विद्यमान अवस्था :

यो उपआयोजना तराईको समथर भू-भागमा समुद्री सतहबाट १९२.६६८ मि. उचाईमा अवस्थित छ। आयोजना क्षेत्रको नगरपालिकाको औसत वार्षिक तापक्रम १७.४२ डिग्री सेल्सियस रहेको छ साथै औसत वार्षिक वर्षा १३१.८ मिलिमिटर रहेको छ । यस नगरपालिकाको उचाई समुन्द्री सतहदेखि ७० मिटर देखि २५०९ मिटर सम्म रहेको छ। प्रस्तावित सडक एलाइनमेन्ट सब हिमालयन रेन्जमा पर्छ । सडक एलाइनमेन्टले कुनै पनि प्रमुख नदीहरू पार गर्दैन। यस नगरपालिकाको हालको भूउपयोगको अधिकांश क्षेत्रफल वन ७६ प्रतिशत, खेतीयोग्य जमिन २० प्रतिशत, बाँझो जमिन २ प्रतिशत, र पानीको क्षेत्र २ प्रतिशत रहेको छ । प्रस्तावित सडक एलाइनमेन्ट र सडकको क्षेत्राधिकारमा पहिरो/बाढी तथा अन्य विपद् जोखिम युक्त क्षेत्र छैन । आयोजना क्षेत्रको हावामा स्थानिय सवारी साधनवाट असर पर्ने र उक्त सवारीसाधन वाट ध्वनिमा असर पर्ने भएपनि ध्वनि गुणस्तर सीमा भित्रनै रहेको पाइएको छ । परियोजना क्षेत्रको आवाज र हावाको परिक्षण गर्दा सरकारी मापदण्ड भित्र नै रहेको पाइएको छ । सडक निर्माणको क्रममा सो सडकमा सवारी साधनको आवागमनमा बाधा पुग्ने सम्भावना रहन्छ । पिपल, केरा, बकाइनो, आसोका र अम्बा उप आयोजना क्षेत्रमा पाइने प्रमुख रूख प्रजातिहरू हुन्।

त्रियुगा नगरपालिकाको कुल जनसङ्ख्या १०२७२५ रहेको छ भने कुल घरधुरी २५६२३ रहेको छ। नगरपालिकाको औसत घरधुरी को आकार ४.०१ छ जुन जिल्लाको परिवारको आकार (४.२०) भन्दा कम छ। यस नगरपालिकाको पुरुष र महिलाको जनसङ्ख्या क्रमशः ४८,४६३ र ५४,२६२ रहेको छ भने लिंग अनुपात ८९.३१ रहेको छ । आयोजना प्रभावित वडा नम्बर १०, ११, १२ र १३ को कुल जनसंख्या र घरधुरी क्रमशः ३३,९५८ र ८६१८ रहेको छ । उप आयोजना पर्ने वार्डहरूको औसत

घरको आकार ३.९५ छ। आयोजना सञ्चालन हुने वडाहरूको कुल पुरुष जनसंख्या १५९७१ र महिला कुल जनसंख्या १७९८७ रहेको छ । परियोजना सञ्चालन हुने वडाहरूको लिंग अनुपात ८८.८४ रहेको छ ।

आयोजना क्षेत्रमा ब्राह्मण, क्षेत्री, चौधरी/थारू, राई, मगर, नेवार, परियार, विश्वकर्मा, मुस्लिम, बोटे, माझी, दनुवार, सार्की र मुसहरको बसोबास छ । धर्मको हिसाबले हिन्दू, इसाई र बुद्धिस्ट समुदायहरू बसोबास गर्छन्। उप-आयोजना क्षेत्रमा जातजातिको मिश्रित समूहको उपस्थिति रहेको छ। आयोजना क्षेत्रमा चौधरी/थारू, राई, मगर, नेवार, बोटे, माझी र दनुवार आदिवासी जनजाति बसोबास गर्दछन्। यस सडकखण्डमा ८८२ घरधुरी रहेका छन्। यस क्षेत्रका अधिकांश मानिसहरू मुख्यतया जीविकोपार्जनका लागि कृषिमा आधारित काम, व्यापार, श्रमिक र वैदेशिक रोजगारीमा संलग्न छन्। त्रियुगा साना नगर खानेपानी समितिको खानेपानी पाइपलाइनबाट यस क्षेत्रमा खानेपानी आपूर्ति भए पनि एकीकृत फोहरमैला व्यवस्थापन प्रणाली छैन । सडक निर्माण हुने क्षेत्र भित्र रहेको १६७ वटा विद्युतको पोल सार्नु पर्ने देखिन्छ। यौन शोषण तथा दुर्व्यवहार, लैङ्गिक हिंसा सम्बन्धित केही विषयहरू फिल्ड सर्भ र नजिकैको प्रहरी कार्यालयको रेकर्डबाट पहिचान गरिएको थियो। घरेलु हिंसा (शारीरिक तथा मानसिक दुर्व्यवहार), बालविवाह र यौन हिंसा/बलात्कार आयोजना प्रभावित क्षेत्रमा बिध्यमान रहेको छ।

ऐन तथा नीति, नियमको आवश्यकता

नेपाल सरकारले विभिन्न समयमा जारी गरेका विषयगत तथा बहुविषयगत निर्देशिका तथा मापदण्डहरू आयोजना तयार गर्न तथा कार्यान्वयन चरणहरूमा वातावरणीय एवम् सामाजिक सुरक्षण आयामहरू मूल प्रवाहीकरण गर्न यथेष्ट छन् । यस प्रतिवेदनले सम्बन्धित नेपाल सरकारका योजना, नीति, ऐन, नियम, निर्देशिका एवम् मापदण्डहरू समेटेको छ । त्यसैगरी यस प्रतिवेदनले विश्व बैङ्कको वातावरणीय तथा सामाजिक मापदण्डहरू पनि समेटेको छ।

वर्गीकरण, क्षेत्र निर्धारण, प्रभाव पहिचान, अनुगमन र व्यवस्थापन

सडकको सम्पूर्ण लम्बाइका लागि प्रस्तावित उप-आयोजनाका विभिन्न अन्तरसम्बन्धित सडकका लागि आयोजनाको प्रत्यक्ष प्रभाव क्षेत्र २० मिटर, १८ मिटर, १२ मिटर, १० मिटर र ८ मिटर सडकको क्षेत्राधिकारलाई मानिएको छ । त्यस्तै, अप्रत्यक्ष प्रभाव सडकको क्षेत्राधिकारको किनाराबाट ५० मिटरभित्र पर्छ। प्रभावहरूको वर्गीकरण तथा संक्षेपीकरण गर्न वातावरणीय तथा सामाजिक चेकलिष्ट प्रयोग गरिएको छ । निर्माण र सञ्चालन चरणहरूमा साइट-विशिष्ट प्रभावहरू यस प्रतिवेदनमा समावेश गरिएको छ। । केही प्रभावहरू निम्नानुसार छन्।

उप आयोजना कार्यान्वयनबाट पर्ने सकारात्मक प्रभाव

यस उप आयोजनाले गरिब, जातीय अल्पसङ्ख्यक र विपन्न स्थानीयका लागि रोजगारीलाई प्राथमिकता दिई स्थानीय बासिन्दाका लागि रोजगारीको अवसर सिर्जना तथा आर्थिक सशक्तीकरणमा योगदान गर्नेछ । अन्य लाभकारी प्रभावहरूमा स्थानीय व्यवसायको वृद्धि, सुधारिएको पहुँच र मानिसहरूको सीप विकास समावेश छ। निर्माणमा सक्रिय रूपमा भाग लिने स्थानीय व्यक्तिहरूले बहुमूल्य सीप र अनुभव प्राप्त गर्छन्। यी सीपहरूले उनीहरूको व्यक्तिगत विकास र रोजगारमा योगदान पुर्‍याउँछ। सडक पूर्वाधारले बजार र सामाजिक सेवाहरूमा पहुँच बढाउन महत्वपूर्ण भूमिका खेल्छ। सुधारिएको यातायात सुविधाले यस क्षेत्रका बासिन्दाहरूको लागि जीवनको राम्रो गुणस्तरको नेतृत्व गर्दछ। कुशल यातायात प्रणालीको उपलब्धताले शिक्षा, स्वास्थ्य सेवा, पर्यटन र अन्य सामाजिक-आर्थिक क्षेत्रमा सकारात्मक प्रभाव पार्न सक्छ। यसबाहेक,

विस्तारित सडक सञ्जालले स्थानीय समुदायलाई फाइदा पुऱ्याउँदै जमिनको मूल्य वृद्धि गर्न योगदान पुऱ्याउँछ। उपआयोजनाको गतिबिधिहरूको परिणाम स्वरूप कुनै भौतिक वा आर्थिक विस्थापन हुने अपेक्षा गरिएको छैन।

उप आयोजना कार्यान्वयनबाट पर्नसक्ने नकरात्मक प्रभावहरु

निर्माण कर्मीहरूको अचानक वृद्धिले सार्वजनिक पूर्वाधार जस्तै सडक, खानेपानी आपूर्ति, परियोजना क्षेत्रको सरसफाइमा असर पार्न सक्छ। धुलो, ध्वनि र निर्माण मेसिनरीबाट निस्कने उत्सर्जनले वातावरणीय प्रदूषणमा योगदान पुऱ्याउन सक्छ । सवारी आवागमन, सडकको छेउछाउको नालीमा बगेको पानीले छेउछाउको खेतीयोग्य जमिनमा माटो कटान हुन सक्छ । यो सडक सञ्चालनमा आएपछि बस्ती क्षेत्र र बजार को विस्तार हुन सक्छ जसले सडकको क्षेत्राधिकारको अतिक्रमण बढाउन सक्छ। त्यस्तै सवारी साधनको धुलो, धुवाँ र धुवाँको वृद्धिले वायु र ध्वनि प्रदूषणलाई केही हदसम्म बढाउन सक्छ।

प्रभाव न्यूनीकरणका उपायहरु

वातावरणीय र सामाजिक प्रभावलाई कम गर्न र निर्माण प्रक्रियाको सुरक्षा र दक्षता सुनिश्चित गर्न यस उप आयोजना निर्माणको लागि न्युनिकरण उपायहरु आवश्यक छन्। प्रतिवेदनले यस परियोजनालाई वातावरणमैत्री बनाउने उद्देश्यले लाभ बढाउन र प्रतिकूल प्रभावहरु कम गर्न विभिन्न उपायहरु प्रस्ताव गरेको छ। रोजगारीका अवसरका लागि स्थानीय जनतालाई प्राथमिकता दिइनेछ । वातावरणमा पर्ने प्रतिकूल प्रभावलाई न्यूनीकरण गर्न आवश्यक कदम चालिनेछ। कामदारको बीमा गरी प्राथमिक उपचार सुविधा र पीपीईजस्ता सुरक्षा सामग्री उपलब्ध गराइनेछ । कामदारहरूलाई आचार संहिता लागू गरिनेछ । वातावरणीय संरक्षण र सामाजिक सुरक्षण सम्बन्धी जनचेतनामूलक कार्यक्रम सञ्चालन गरिनेछ। आयोजनामा १६ वर्षभन्दा कम उमेरका बालबालिकालाई काम लगाउन निषेध गरिनेछ। सडकको किनारामा बृक्षारोपणको लागि स्थानीय जातका बिरुवालाई प्राथमिकता दिइनेछ । वातावरण संरक्षण गर्न र सडकको छेउछाउका ढलानहरूलाई यथास्थितिमा स्थिर गर्न उपयुक्त बायो/सिभिल इन्जिनियरिङ संरचनाहरू अपनाइनेछ। सञ्चालनका क्रममा नजिकैको खेतीयोग्य जमिनमा पानी जम्न नदिन ढल निकासको उचित व्यवस्था मिलाइनेछ । सडक दुर्घटना न्यूनीकरणका लागि पर्याप्त सडक सुरक्षाका उपायहरु जस्तै सूचना र चेतावनी संकेतहरू स्थापना गरिनेछ।

यौन शोषण तथा दुर्वसन एवम् दुर्व्यवहार रोकथाम तथा सम्बोधन कार्य योजना

विश्व बैङ्कले नेपाल शहरी शासकीय तथा पूर्वाधार उपआयोजनाको लागि गरिएको यौनिक शोषण, दुर्वसन एवम् यौन दुर्व्यवहार जोखिम मूल्याङ्कनमा आधारमा यस आयोजनाको SEA/SH जोखिमको "न्यून" मूल्याङ्कन गरेको छ । यस मूल्याङ्कनमा आधारित भई आयोजनाको लागि SEA/SH निरोध तथा सम्बोधन कार्ययोजना आयोजनाको लागि SEA/SH रोकथाम तथा सम्बोधन कार्ययोजना बनाइएको छ । यसमा उपआयोजनाको कार्यक्रमले सिर्जना गर्न सक्ने SEA/SH जोखिमहरु निषेध एवम् रोकथाम तथा न्यूनीकरण गर्ने उद्देश्यका निश्चित व्यवस्थाहरु समावेश गरिएका छन् । यस योजनाले तालिका-१; विश्व बैङ्कले सेप्टेम्बर २०१८ मा प्रकाशित "असल अभ्यास नोट" अनुसार IPF परियोजनाहरूमा SEA/SH जोखिमहरूलाई सम्बोधन गर्न सुझाएको कार्यहरु लाई पनि समावेश गरेको छ।

वातावरण तथा सामाजिक व्यवस्थापन योजना

पहिचान गरिएका मुद्दाहरू, सम्भावित प्रभावहरू, तिनीहरूको न्यूनीकरणका उपायहरू, अनुगमन विधिहरू सहित यो वातावरणीय र सामाजिक व्यवस्थापन योजना रिपोर्ट प्रस्तावित गरिएको छ। निर्माण र सञ्चालन चरणहरूमा वातावरणीय र सामाजिक प्रभावहरूको लागि न्यूनीकरण लागत प्रतिवेदनमा समावेश गरिएको छ। यसबाहेक पर्यावरणीय र सामाजिक न्यूनीकरण उपायहरू कार्यान्वयन गर्न र अनुगमनको लागि जिम्मेवार एजेन्सीहरू यस वातावरणीय र सामाजिक व्यवस्थापन योजनामा पहिचान गरिएको छ। भौतिक, जैविक, सामाजिक, आर्थिक र सांस्कृतिक वातावरण सम्बन्धी विभिन्न अनुगमन सूचकहरू पनि पहिचान गरिएको छ। आयोजनाले पुनर्निर्माण र सञ्चालनको चरणमा सरोकारवालासँग मिलेर अनुगमन गर्नेछ। सरोकारवालाको जिज्ञासा र चासोको उठान र समयमै जवाफ दिनका लागि गुनासो निवारण संयन्त्र (GRC) मार्फत आयोजनासँग सम्बन्धित गुनासाहरूको सम्बोधन गरिनेछ। गुनासो समितिलाई लिखित, मौखिक, टेलिफोन, पत्र आदि कुनै पनि माध्यमबाट गुनासो उपलब्ध गराउन सकिन्छ र समितिले परियोजनाको प्रक्रिया अनुसार आवश्यक प्रक्रिया गर्नेछ र समाधान हुन नसकेमा माथिल्लो तहमा पठाउनेछ। उपआयोजना निर्माणका क्रममा GBV, SEA/SH जोखिमहरूलाई आवश्यक न्यूनीकरण गर्न योजना बनाइ आवश्यक सचेतना तथा तालिमहरूको ब्यवस्था गरिएको छ।

संस्थागत व्यवस्था

आयोजना कार्यान्वयन गर्न शहरी विकास मन्त्रालयले शहरी विकास तथा भवन निर्माण विभाग अन्तर्गत एउटा आयोजना समन्वय कार्यालय स्थापना गरेको छ। वातावरणीय तथा सामाजिक विधिको साथै सम्पूर्ण विधिहरू पालना सम्बन्धी जिम्मेवारीको जवाफदेहिता आयोजना समन्वय कार्यालयमा रहने छ। आयोजना समन्वय कार्यालयलाई एउटा आयोजना व्यवस्थापन सहयोग टोलीले सहयोग गर्नेछ। उपआयोजनाहरूको वातावरणीय तथा सामाजिक व्यवस्थापन योजना कार्यान्वयन स्थानीय तहमा गर्न र तथा अन्य वातावरणीय एवम् सामाजिक संयन्त्रहरूको कार्यान्वयनका जिम्मेवार हुने गरी नगरपालिकामा एक आयोजना कार्यान्वयन इकाइ स्थापना गरिनेछ। सुरक्षण विशेषज्ञ सहितको डिजाइन तथा सुपरिवेक्षक परामर्शदाता मार्फत प्राविधिक साहायाता पुऱ्याइनेछ।

Acronym

BoQ	: Bill of Quantity
CBS	: Central Bureau of Statistics
CESMP	: Construction Environment and Social Management Plan
CoC	: Code of Conduct
Ch	: Chainage
DPR	: Detailed Project Report
DSC	: Design and Supervision Consultant
DUDBC	: Department of Urban Development & Building Construction
EA	: Environmental Assessment
EHS	: Environment, Health and Safety
EPR	: Environmental Protection Rule
ESMP	: Environmental and Social Management Plan
FGD	: Focus Group Discussion
ILO	: International Labor Organization
NUGIP	: Nepal Urban Governance and Infrastructure Project
OHS	: Occupational Health & Safety
OP/BP	: Operational Policy/Bank Policy
PCO	: Project Coordination Office
PIM	: Project Implementation Manual
PIU	: Project Implementation Unit
PPE	: Personal Protective Equipment
RoW	: Right of Way
SEA/SH	: Sexual Exploitation and Abuse/Sexual Harassment
STD	: Sexually Transmitted Disease
WB	: World Bank
WASH	: Water, Sanitation and Hygiene

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1. PROJECT INTRODUCTION

The Department of Urban Development and Building Construction (DUDBC) under Ministry of Urban Development (MoUD) of Government of Nepal has been executing the Nepal Urban Governance and Infrastructure Project (NUGIP) within the strategic framework for urban development envisaged in National Urban Development Strategy since the fiscal year 2077/78 B.S. The Project Development Objective (PDO) of NUGIP is to strengthen institutional capacity in participating municipalities for strategic municipal infrastructure and service delivery. In particular, NUGIP will aim at: a) improving access to core municipal services (includes expansion of coverage, and construction and rehabilitation of basic infrastructure systems, e.g., urban roads & storm water drainage etc) in participating municipalities; b) strengthening planning, budgeting and implementation systems for municipal service delivery; and c) strengthening municipal finances and financial management systems.

The DPR of **Upgradation Works of different interconnected road (9.929Km) sections** in Triyuga Municipality has been prepared as per the contract between the Office of the Municipal Executive of Triyuga Municipality; Nepal Urban Governance and Infrastructure Project (NUGIP) (Client) and JV of Bosphorous Technical Consulting Corp (BOTEK) and GOEC Nepal P. Ltd which have entered into the agreement, for performing work REF No: NP-DUDBC-216329-CS-QCBS to provide services on Detailed Engineering Design and Construction Supervision (DSC) covering the upgradation and rehabilitation works. The project is expected to contribute towards the municipal capacity for urban development planning, infrastructure development and institutional development of the municipality together with the improvement of livelihood of the local people along the settlement.

The study for Environmental and Social Management Plan (ESMP) of Upgradation Works of Different inter connected road sections in Triyuga Municipality was conducted in September/October 2023. The study and preparation of the this ESMP is guided by the Environmental and Social Management Framework (ESMF) for NUGIP.

Sub Project Title	Upgradation of <i>Different inter connected road sections (9.929Km)</i>
Estimated Cost:	NPR. 677,075,718.14 (including VAT and contingency) ESMP cost for Interconnected Road: 1,700,000.00
Start/Completion Date:	March 2024 - July 2025

2. DESCRIPTION OF THE PROJECT SITE

Triyuga is one of the eight municipalities that make up the Udayapur district of Koshi Province. The municipality's headquarters is Gaighat. The name of the municipality is taken from the Triyuga River, which passes through the town. In terms of size, Triyuga is the third-biggest municipality in Nepal and the largest in Koshi Province (area). The municipality has an area of 547.43 square kilometers. It is located between 26° 41' 17'' N to 26° 56' 42'' north latitudes and 86° 32' 11.5'' to 86° 50' 29'' east longitude. The municipality is surrounded by Mahabharat hills in north and Churey hills in south. Udayapurgadhi and Rautamai are in north of the municipality, Khotang District is in north-east and Chaudandigadhi Municipality is in east. Saptari District is in south of Triyuga Municipality.

This city is in the famous Udayapur Valley. Gaighat (other name of Triyuga) is the shadow of a historic kingdom of Sen dynasty in Udayapurgadhi (Chaudandi), this Inner Terai Valley of south-eastern Nepal has a glorious history with a bright future. Gaighat is in the plane of the Valley. Udayapur Valley is the largest valley in eastern Nepal having fabulous natural attractions such as the Triyuga river, Baruwa river flowing through the town. Koshi River is east of the city. According to the census of 2078, there are 25623 HHs having total population of 102,725, with 48,463 males and 54,262 females. This municipality is divided into 16 wards.

2.1 Location of the project

Different inter connected road (9.929 Km) sections is an urban road of Triyuga Municipality. Gaighat Bazar is surrounded by these roads. Gaighat is the Triyuga municipal headquarter as well as district headquarters of Udaypur district. The proposed subproject is located in Terai area and passes mainly through mixed land use having urban settlement and cultivated land. This inter connected road has seven sections, they are presented in **Table 2-1**.

Table 2-1 List of roads in different interconnected road sections

SN	Name of the Road	Length (Km)
1	Gaighat town planning area road section (1.534Km)	
	Section I	1.045
	Section II	0.489
2	City hall- Simaltar-Gaighat- Diktel road section	2.860
3	Babarani DM Gate road section	0.622
4	Fulchowk PG Chowk Road Bote Tole Triyuga Corridor section	0.900
5	City Hall- Ramchandra Basnet house section	0.255
6	Gaighat Shivalay Tole-Adarsha Tole-Ramailo Danda Manmohan School to Karamgachhi Diktel connecting road section	1.600
7	Pragati Tole kha (In front of Quarter) road to Sakela Road ward 12 (2.158Km)	
	Section I	0.788
	Section II	1.370
Total		9.929

The road wise location details are presented below

i. Gaighat town planning area road section (1.534Km)

The proposed road section lies within the Gaighat city. There are two sections of this road. Section I starts from municipality office and make rectangular section and ends at gate of municipality office. Section II starts from Sagarmatha Highway and end near Sunny Mandir.

This road passes through densely populated area of Gaighat main bazar. It passes through Adarshanagar Tole, BP Chowk, Bidhut Marga and Jyoti Marga. It crosses two highways (Sagarmatha and Madan Bhandari) in different locations. This road rounds the main crossing of Sagarmatha and Madan Bhandari Highway. It directly serves the people of ward 11 of Triyuga municipality.

ii. City Hall Simaltar Gaighat Diktel road section (2.86Km)

This road start from main chowk of Gaighat bazar and end at Gaighat-Diktel Road (section of Sagarmatha Highway). This road connects the core Gaighat city area to the Gaighat-Diktel road section via Simaltar. This road passes through major place i.e Pipal Chowk, InnarChowk, Janmukhi Chowk, Patandevi Tole and simaltar area. It is the one of the oldest routes to reach Gaighat-Diktel road from main chowk of Gaighat. It directly benefits the people of ward 11 of Triyuga municipality.

iii. Babarani DM Gate road section (0.622Km)

This road start from Babarani chowk which lies in Madan Bhandari highway and ends at DM Gate of Sagarmatha Highway. This road connects core city of Gaighat-to-Gaighat bus park through Sagarmatha highway. It directly serves people of ward 11 of Triyuga municipality.

iv. Fulchowk PG Chowk Road Botetole Triyuga Corridor section (0.9Km)

This road start from Fulchowk (a point of Purano Gaighat Bisanpur road section) and ends at Triyuga corridor which further connects with Sagarmatha highway. This road passes through Fulchowk, Bote tole, Sagarmatha tole and Sansari marga. This road serves people of ward 12 of Triyuga municipality.

v. City Hall Ramchandra Basnet house road section (0.255Km)

This road starts from Ch 0+260 of City Hall Simaltar Gaighat Diktel road. This road is a branch road of City Hall Simaltar Gaighat Diktel road. It serves the people of Ward 11 of Triyuga municipality.

vi. Gaighat Shivalay Tole-Adarsha Tole-Ramailo danda Manmohan school to Karamgachhi Diktel connecting road section (1.6Km)

An alignment of length 1.60 Km starts from Shivalaya chowk (Ch 1+300) of City Hall Simaltar Road. It passes through Adarsha tole, Ramailo danda, Ncell danda, Manmohan school and ends near Karamgachhi (Ch 1+600) of Ward no. 10.

vii. Pragati Tole kha (Quarter front) road to Sakela Road ward 12 (2.158Km)

This road starts from the PG chowk of Madan Bhandari highway and is further divided into two sections, one section having 788 m length and the other section having 1370 m length. The second section starts from the first sections Ch 0+250. This road passes through Pragati tole, Bishalnagar tole, Sakela tole, Sakela Park, basic health center, football ground and ends at Pragati tole. This road serves the people of ward 12 and 13 of Triyuga municipality.

2.2 Land use

The proposed roads are opened track and vehicles are moving throughout the road sections. The ROW was declared in 2055 BS by Municipality with the help of “Eastern area Directorate” under Ministry of Building and Physical planning, Government of Nepal. According to declared criteria-2055 by “Eastern Area Directorate”, the ROW of different interconnected road sections is Shown in **Table 2-2** and related documents are attached along with a municipal clarification letter in **Annex II** of this report.

The land within the proposed construction width of proposed interconnected road sections is already in use by the public. The upgrading work for the road sub-project will be limited within the existing road width. The project does not run through any ecologically sensitive area such as protected area, conservation area, wetland, national parks etc. No historical or archaeological sites are located within the project area.

Table 2-2: RoW details of the Inter-connected Road Sections

SN	Name of the Road/ Section	Length (Km)	RoW (m)	Existing width (m)	Proposed construction width (m)	Remarks (Cross section type)
a.	Gaighat Town planning area road					
	Section I	1.45	12 for 0+000 to 0+140,	12 for 0+000 to 0+140,	12 for 0+000 to 0+140,	Type_1
			18 for 0+140 to 1+010	18 for 0+140 to 1+010	17.5 for 0+140 to 1+010	Type_2
			10 for 1+010 to 1+045	10 for 1+010 to 1+045	8 for 1+010 to 1+045	Type_3
	Section II	0.489	12 for 0+000 to 0+489	11.5 for 0+000 to 0+240	11.5 for 0+000 to 0+240	Type_4
				8 for 0+240 to 0+489	8 for 0+240 to 0+489	Type_3
b.	City hall- Simaltar-Gaighat- Diktel road section					
		2.86	20	20 for 0+000 to 0+260 13 for 0+260 to 2+820 10.65 for 0+820 to 2+860	20 for 0+000 to 0+260 13 for 0+260 to 2+820 10.65 for 2+820 to 2+860	Type_5 Type_6 Narrow section
c.	Babarani DM Gate road section					
		0.622	8	8 for 0+000 to 0+260 6.3 for 0+260 to 0+420 8 for 0+420 to 0+520 6.75 for 0+520 to 0+610 8 for 0+610 to 0+622	8 for 0+000 to 0+260 6.3 for 0+260 to 0+420 8 for 0+420 to 0+520 6.75 for 0+520 to 0+610 8 for 0+610 to 0+622	Type_3 Narrow Type_3 Narrow Type_3
d.	Fulchowk PG chowk Road Botetole Triyuga Corridor section					
		0.9	8	8	8	Type_3
e.	City hall- Ramchandra Basnet house section					
		0.255	8	8 for 0+000 to 0+160 6.3 for 0+160 to 0+240 8 for 0+240 to 0+255	8 for 0+000 to 0+160 6.3 for 0+160 to 0+240 8 for 0+240 to 0+255	Type_3 Narrow Type_3
f.	Gaighat Shivalaya Tole-Adarsha Tole-Ramailo danda Manmohan school to Karamgachhi Diktel connecting road section					
		1.6	8	8	8	Type_3
g.	Pragati tole kha (Infront of Quarter) ko road to Sakela road ward 12					
	Section I	0.788	10	8	8	
				8 for 0+000 to 0+560 6.3 for 0+560 to 0+600 8 for 0+600 to 0+740 6.3 for 0+740 to 0+760 8 for 0+760 to 0+788	8 for 0+000 to 0+560 6.3 for 0+560 to 0+600 8 for 0+600 to 0+740 6.3 for 0+740 to 0+760 8 for 0+760 to 0+788	Type_3 Narrow Type_3 Narrow Type_3
	Section II	1.37	10	8	8	Type_3

2.3 Topography and Hydrology

Triyuga Municipality located in the central part of Udaypur District is a commercial contact center between the Terai and the mountains and is an entry way of the Sagarmatha highway. It is an important connecting centre of the hill districts of Khotang, Okhaldhunga and Solukhumbu with Terai, Triyuga municipality. The geography of Chure region in the south and Mahabharata range in the north to the Sunkoshi River covers a large area. In this municipality which has a heterogeneous structure, there are scattered and thinned settlements between the Mahabharat range and flat plains between Mahabharat range and Chure area on the southern border. This municipality is considered as inner Madesh as it is a large long shaped land between Mahabharat range and Chure range. The major rivers flowing through this municipality as Koshi, Triyuga and Baruwa. The altitude of the municipality is ranges from 70m to 2509m from sea level. The proposed road alignment lies in the Sub Himalayan range. The alluvial river deposit is the most predominant along the road alignment.

Road alignments does not cross any river. Number of small seasonal small Hume pipe crosses the alignment at different Chainage which details are presented in **Table 2-4**. Department of Hydrology and Meteorology (DHM) data of 24-hour maximum rainfall for Gaighat (Index no. 1227) station is used for analysis of side drain and cross drain. Average annual precipitation of Gaighat is 131.88 mm. The latitude, longitude and elevations of the nearest station are shown in **Table 2-3**.

Table 2-3 :Details of Hydrology Station near Triyuga Municipality

Station Name	Index no.	Latitude	Longitude	Elevation (m)
Triyuga	1227	26°47'N	86°43'E	152

Table 2-4 Natural Drainage Existed Along the Road Alignment

SN	Name of the Road	Chainage	Natural Drainage	Adopted Structures	Remarks
1	Gaighat town planning area Road Section		NA		Highway crossing only.
2	City hall- Simaltar-Gaighat-Diktel Road Section	2+817	Crossing	0.6m dia Hume pipe	Crossing
		1+125	Crossing	Box Culvert (1.5*1)	
		1+938	Crossing	Box Culvert (1.5*1)	
3	Babarani DM gate Road Section	0+621	Drain crossing	Box Culvert (1.5*1)	
4	Fulchowk PG chowk Road botetole Triyuga Corridor section	0+179	Crossing	0.6m dia Hume pipe	Crossing
		0+885	Crossing	1.2m dia Hume pipe	Strome water crossing
5	City hall- Ramchandra Basnet house section	0+255	Crossing	Box Culvert (2*2)	
6	Gaighat Shivalay tole- Adarsha tole-ramailo danda manmohan school to karamgachhi diktel connecting Road Section	0+150	Crossing	0.6m dia Hume pipe	Crossing
		0+630	Crossing	Box Culvert (1.5*1)	Drain Crossing
		1+050	Crossing	Box Culvert (1.5*1)	Drain Crossing
		1+378	Crossing	0.9m dia Hume pipe	CS Structure
		1+598	Crossing	Box Culvert (1.5*1)	Crossing
7	Pragati tole kha (Infront of Quarter) ko road to Sakela road ward 12 Section I				
		0+451	Crossing	0.6m dia Hume pipe	Crossing
		0+787.5	Crossing	Box Culvert (1.5*1)	Drain Crossing

SN	Name of the Road	Chainage	Natural Drainage	Adopted Structures	Remarks
	Section II	0+770	Crossing	0.9m dia Hume pipe	Outlet towards river
		0+978	Crossing	0.6m dia Hume pipe	Crossing
		1+022	Crossing	0.6m dia Hume pipe	Drain crossing
		1+318	Crossing	Box Culvert (1.5*1)	Crossing

2.4 Landslide and unstable slopes

As per field study, there is no any landslide, unstable slopes and flood prone area and other disaster-prone area present within the proposed alignment.

2.5 Climate and Vegetation

Udayapur has a Temperate highland tropical climate with dry winters climate (Classification: Cwb). The district's yearly temperature is 17.42°C (63.36°F) and it is -4.58% lower than Nepal's averages. Udayapur Garhi typically receives about 253.05 millimeters (9.96 inches) of precipitation and has 199.06 rainy days (54.54% of the time) annually. Monsoon rain occurs from the month of Ashad to month of Ashwin. Road alignment does not cross any major river and streams however rainwater is discharged in natural gully.

Since this road sections lies within the municipality city area having dense, scattered settlements and cultural cultivated land. There is no any forest and rivers within the alignments. Therefore there is no direct effect for the biological aspect of the environment. It is not a habitat for terrestrial fauna and avifauna. Major trees, Wildlife and Aquatic species found in this municipality area are presented in **Table 2-5, 2-6 and 2-7** respectively.

Table 2-5 Major Tree Species found in Municipality Area

SN	Scientific Name	Common Name	Use
1.	Ficus religiosa	Pipal	Hardwood
2.	Musa acuminata Colla	Banana (kyare)	Fruit
3.	Areca Catechu	Supari	Fruit
4.	Dalbergia Sissoo	Sissoo	Timber
5.	Magnifera indica	Aanp	Fruit/Fuelwood
6.	Melia azadirach	Bakaino	Fuelwood/Fodder
7.	Saraca asoca	Ashoka	Ornamental
8.	Psidium guajava	Amba	Fruit/Fuelwood
9.	Musa balbisiana	Banana	Fruits

Table 2-6 Major Wildlife found in the Municipality Area

SN	Local Name (Nepali Name)	English Name	Scientific Name	Remarks
Wildlife: Animal				
1	स्यल	Wolf	Canis lupus	
2	बादर	Monkey	Macaca mulatta	
Major Birds				
1	कग	Crow	Corvus brachyrhynchos	

SN	Local Name (Nepali Name)	English Name	Scientific Name	Remarks
2	सुगा	Parrot	Phaethontidae Psittaciformes	
3	भगेरा	Sparrow	Ploceidae Passer	

Table 2-7 Major Aquatic species found in the municipality area

SN	Local Name (Nepali Name)	English Name	Scientific Name	Remarks
1	जुगे माछा	Gangetic mystus	Mystus cavasius	
2	बाम माछा	Cuchia	Monopterusuchia	
3	भ्यागुता	Frog	Rana tigrina	
4	गगटो	Crab	Carcinus maenas	

2.6 Air Quality

Air quality of the project area was tested by instrument in two locations which covers both priority one and priority two roads. Instruments were placed in chainage 0+000 of Gaighat town planning area road section and Chainage 1+460 of City Hall Simaltar Gaighat Diktel road section for air quality monitoring. The real time based 24 hours average TSP, PM10, PM2.5 and CO were 164.5 $\mu\text{g}/\text{m}^3$, 77.3 $\mu\text{g}/\text{m}^3$, 32 $\mu\text{g}/\text{m}^3$ and 230 $\mu\text{g}/\text{m}^3$ respectively at the Gaighat town planning area road section. Similarly, the real time based 24 hours average TSP, PM10, PM2.5 and CO were 131.4 $\mu\text{g}/\text{m}^3$, 58.4 $\mu\text{g}/\text{m}^3$, 14.1 $\mu\text{g}/\text{m}^3$ and 230 $\mu\text{g}/\text{m}^3$ respectively at the City Hall Simaltar Gaighat Diktel road section. The observed values are within the prescribed limit of NAAQS 2012 for the respective air quality parameters. The air quality test report is presented in **Annex V**.

The main construction activities that cause air pollution are earthworks excavation, asphalt plants operations and vehicle movement. These activities generate dust, which directly affect the air quality. In addition, vehicles and machinery emit smoke and fine particles. These substances will increase the local air pollution significantly during the construction stage. Burning of fossil fuels will result air pollution due to emission of sulfur oxides (SOx), nitrogen oxide (NOx), carbon dioxide (CO₂) and particulates.

For maintaining the quality of air in construction site, during construction, water -will be sprayed on the road surface and personal protective equipment (PPE) for the construction workers will be provided. The construction vehicles will be well maintained and will strictly comply with the GoN pollution regulation with compulsion in obtaining green sticker. Similarly, all construction plants should adhere to emission regulation. The vehicles carrying construction materials should ensure that it is well sealed and covered so as to avoid littering. The anticipated cost and specific conditions related to air pollution containment are included in the construction contract

2.7 Water Quality

The construction debris, paints, oil and grease are likely to create water pollution both surface and subsurface. The dust and silt from the construction sites will also create water pollution of the receiving streams. If workers living in tents/camps do not have access to toilet facilities, open defecation may be practiced, which may contaminate water sources, causing health problems. The anticipated impacts on water pollution such as eutrophication, spreading of water borne diseases, effect on downstream aquatic life will be direct in nature, low in magnitude, local in extent and of short-term in duration.

Spoil should be disposed off at designated spoil sites and efforts should be made to minimize such waste as far as possible through reuse, reduction, and recycling concepts. Similarly, the contamination of water by the use of cement and bitumen should be avoided and strongly monitored by contractors, PIU and DSC. The Contractor needs to arrange for sufficient water supplies and proper sanitation facilities for its labor force. Separate arrangements are necessary for work camp and labor camps. The anticipated cost and specific conditions related to water pollution containment are included in the construction contract

The water samples were collected from the project affected areas and tested in laboratory. The water quality report was found complied with National Drinking Water Quality Standards /GoN. The detail of water quality test report is enclosed in **Annex V**.

2.8 Noise level

The daytime and night time observed sound pressure level at the monitoring site at Gaighat town planning area road was 54.2B(A) and 47.8dB(A) respectively and Chainage at City Hall Simaltar Gaighat Diktel road was 53.1B(A) and 48.9dB(A) respectively. The observed daytime and nighttime average sound pressure levels complied the prescribed limits of GoN- Noise level standard 2069 for category urban residential area. The details report of air, water and noise are summarized in **Annex V**.

Noise impacts will be significant in the RoW and vicinity of the proposed subproject upgradation road during construction periods due to increase of vehicular movements and operation of machinery equipment. The heavy construction equipment movement in the site should be maintained in the day time only. Vibrations effects are closely observed and may need to do vibration activities by considering local requirement maintaining prescribed quality. For the safety of construction workers, PPE along with earplugs must be provided while on duty. The anticipated cost and specific conditions related to noise and vibration containment are included in the construction contract

2.9 Existing infrastructures/ utilities

The details of existing infrastructures situated within the RoW width are presented in the tables below.

Water supply pipelines

As per field study, consultation and discussion with the Municipality representatives and Triyuga Small Town Water Supply Committee, there is presence of existing water supply pipeline along the alignment. In most of the sections of interconnected roads, there are two parallel pipelines placed for supply of water for community use. It has been proposed that, water pipes are carefully handled during construction and necessary estimation for pipe relocation and new construction has been proposed in DPR and relocate/new pipelines will be laid in edge of road i.e under the footpath during the construction of road project. Necessary consultation with Triyuga Small Town Water Supply Committee was conducted during field study. The list of existing pipelines along the alignment are listed in **Table 2-8**.

For continuous flow of water, there are existing two sets of pipes running parallel to the alignment. Thus, arrangement should be made for continuous flow of one pipeline at a time during construction. So that, there will be minimal disturbance due to relocation/new construction of water supply pipelines during construction/relocation.

Table 2-8: Existing water supply pipeline details

S N	Name of the road	Chainage		Side	Length (m)	Dia. of pipe	Type of pipe	Quantit y (m)	Remar ks
		From	To						
1	Gaighat town planning area road section								
	Section I	0+000	1+045	Both	1045	4''	CI	2090	
	Section II	0+000	0+489	Both	489	4''	CI	978	
2	City hall- Simaltar- Gaighat- Diktel road section	0+000	0+200	Both	200	6''	CI	400	
		0+200	2+860		2660	8''		5320	
3	Babarani DM gate road section	0+000	0+622	Both	622	110 mm	HDPE	1244	
4	Fulchowk PG chowk Road botetole Triyuga Corridor section	0+000	0+900	Both	900	4''	HDPE	1800	
5	City hall- Ramchandra Basnet house section	0+000	0+255	Right	255	40 mm	HDPE	255	
6	Gaighat Shivalay Tole- Adarsha Tole-Ramailo Danda Manmohan School to Karamgachhi Diktel connecting road section	0+000	1+180	Both	1180	3''	HDPE	2360	
		1+118	1+600	Both	482	4''		964	
7	Pragati Tole Kha (In front of Quarter) road to Sakela road ward 12	0+000	0+788	Both	788	4''	HDPE	1576	
		0+000	1+370		1370			2740	
Total								19727m	

Source: Field survey 2023

Religious Places, Resting Area and Community owned properties

The details of the public owned properties lie alongside of the road alignment are presented below. These structures won't be affected by the project upgradation works however necessary precaution such as careful handling of equipment and safety measures will be taken during construction works.

Table 2-9: List of temples, resting areas and other community owned properties

S. N.	Road Name	Chainage	Name of temples/ resting places/ other community owned structures	Type	Remarks
1	Gaighat town planning area road section				
	Section I	0+570	Pipal Chowk	Chautara	Within the road center but will not be affected
	Section II	NA			
2	City hall- Simaltar-Gaighat- Diktel road section	0+200	Pipal Chowk	Chautara	Within the road center but will not be affected
		1+600	Shiva Mandir	Temple	Right side, will not be affected

S. N.	Road Name	Chainage	Name of temples/ resting places/ other community owned structures	Type	Remarks
3	Babarani DM gate road section	NA			
4	Fulchowk PG chowk Road Botetole Triyuga Corridor section	0+000	Fulchowk	Resting place	Left side, will not be affected
5	City hall- Ramchandra Basnet house section	NA			
6	Gaighat Shivalay tole- Adarsha tole-Ramailo danda Manmohan school to Karamgachhi Diktel connecting road section	1+200	Pipal tree with Shiva Mandir	Temple	Left side, will not be affected
7	Pragati tole kha (In front of Quarter) ko road to Sakela road ward 12				
	Section I	NA			
	Section II	0+700	Sakela Park	Park	Left side, will not be affected
		1+028	Puspalal Statue		Road center, will not be affected

Source: Field survey 2023

Schools

Here is the list of schools which are lies along the alignment and near to the road alignment. The physical structures of these schools won't be affected by upgradation works. Necessary consultation with school management were conducted during field study. Footpath, necessary road safety signs i.e. zebra cross, sign board etc has been proposed. In these sections, the road is wider than other section to ease the student flow during school hours.

Table 2-10: Schools in and near the road alignment

S.N.	Road Name	Chainage	School Name	Place/ Settlement	Remarks
1	Gaighat town planning area road section	NA			None of the structures of school will be affected during construction
2	City hall- Simaltar-Gaighat- Diktel road section	NA			
3	Babarani DM gate road section	NA			
4	Fulchowk PG chowk Road Botetole Triyuga Corridor section	NA			
5	City hall- Ramchandra Basnet house section	NA			
6	Gaighat Shivalay tole-Adarsha tole-ramailo danda Manmohan school to Karamgachhi Diktel connecting road section	1+550	Manmohan school	Khashi Bazar, Karamgachhi	
7	Pragati tole kha (In front of Quarter) ko road to Sakela road ward 12	NA			

Existing Storm water drainage structures*Table 2-11: Existing storm water drain details*

SN	Name of the Road	Chainage		Direction	Remarks
		FROM	TO		
1	Gaighat town planning area road section				Proposed for re-construction
	Section I	0+000	0+060	Left and Right	
		0+080	0+280	Left and Right	
		0+300	0+560	Left and Right	
		0+580	0+840	Left and Right	
		0+860	1+000	Left and Right	
	Section II	0+000	0+489	Left & Right	
2	City hall- Simaltar-Gaighat- Diktel road section	0+000	0+190	Right & Left	
		0+210	0+255	Right & Left	
		0+260	0+310	Right & Left	
		0+315	0+415	Right & Left	
		0+420	1+920	Right	
		1+945	1+965	Right	
		1+990	2+040	Right	
		2+050	2+460	Left & Right	
		2+710	2+750	Right	
3	Babarani DM gate road section	0+000	0+620	Left and Right	
4	Fulchowk PG chowk Road Botetole Triyuga Corridor section	0+000	0+900	Left & Right	
5	City hall- Ramchandra Basnet house section	0+000	0+150	Left	
6	Gaighat Shivalay tole-Adarsha tole-Ramailo danda Manmohan school to Karamgachhi Diktel connecting road section	0+000	1+190	Left	
		0+000	0+250	Right	
		1+050	1+600	Right	
7	Pragati tole kha (In front of Quarter) ko road to Sakela road ward 12	0+000	1+370	Left & Right	
		0+000	0+787	Left & Right	

Source: Field survey 2023

Existing Electric lines

In the whole alignment, the road section has 167 electric poles with in construction width of proposed subprojects which needs to be relocated during construction. Details of electric poles which needs to be relocated during construction are presented in below **Table 2-12** and further chainage wise details are presented in **Annex IX**.

Table 2-12: Road wise existing pole details

SN	Road Name	Number	Remarks
1	Gaighat town planning area road section	47	
2	City hall- Simaltar-Gaighat- Diktel road section	47	
3	Babarani DM gate road section	4	
4	Fulchowk PG chowk Road Botetole Triyuga Corridor section	8	
5	City hall- Ramchandra Basnet house section	5	
6	Gaighat Shivalay tole-Adarsha tole-Ramailo danda Manmohan school to Karamgachhi Diktel connecting road section	29	
7	Pragati tole kha (Infront of Quarter) ko road to Sakela road ward 12	27	
Total		167	Nos.

Source: Field survey 2023

2.10 Demography of the project area

The total population of Udayapur District, according to the Census of Nepal, 2021 (2078 B.S.) conducted by National Statistics Office (NSO) is 340,721 and the number of households is 81,089. The average household size of the district is 4.20, which is lower than that of the national average (4.37). The total population of Triyuga Municipality is 102,725 and total household is 25623. The average household size of the Municipality is 4.01 which is lower than that of district household size (4.20). The male and female population of the municipality is 48463 and 54262 respectively and sex ratio is 89.31.

Table 2-13 Population and Household details of affected wards

Ward No.	Name of the road	House hold	Population			Average Household size	Sex ratio
			Total	Female	Male		
10	<ul style="list-style-type: none"> Gaighat Shivalay tole-Adarsha tole-Ramailo danda Manmohan school to Karamgachhi Diktel connecting road section 	2125	8265	4381	3884	3.89	88.66
11	<ul style="list-style-type: none"> Gaighat town planning area road section, City Hall Simaltar Gaighat Diktel road section Babarani DM Gate road section City Hall Ramchandra Basnet house road section Pragati tole kha (Quarter front) ko road to Sakela road ward 12 	2973	11400	6065	5335	3.83	87.96
12	<ul style="list-style-type: none"> Fulchowk PG Chowk Road Botetole Triyuga Corridor section 	1882	7907	4137	3770	4.2	91.13
13	<ul style="list-style-type: none"> Pragati tole kha (Quarter front) ko road to Sakela road ward 12 	1638	6386	3404	2982	3.90	87.60

Source: NSO, 2021

2.11 Settlements

There is presence of dense and scattered settlement within the project area of interconnected roads. The major settlement along the alignments is listed below.

- **Gaighat town planning area road section:** Gaighat Bazar, Adarshanagar tole, BP chowk, Bidhut marga and Jyoti marga,
- **City Hall Simaltar Gaighat Diktel road section:** Pipal chowk, Innar chowk, Janmukhi chowk, Patandevi tole and Simaltar
- **Babarani DM Gate road section:** Babarani tole
- **Fulchowk PG Chowk Road Botetole Triyuga Corridor section:** Fulchowk, Bote tole, Sagarmatha tole and Sansari marga.
- **City Hall Ramchandra Basnet house road section:** City Hall
- **Gaighat Shivalay tole-Adarsha tole-ramailo danda Manmohan school to Karamgachhi Diktel connecting road section:** Adarsha tole, Ramailo danda, Ncell danda, Manmohan school and Kamgachhi
- **Pragati tole kha (Quarter front) ko road to Sakela road ward 12 road:** Pragati tole, Bishalnagar tole, Sakela tole, Sakela Park

2.12 Land availability

The project area consists of dense settlement in Town planning, City Hall Simaltar and Babarani DM Gate road section and other roads alignment have scattered settlements with cultivated land. All the proposed roads are operating and vehicles have been plying throughout the road sections. The ROW was declared in 2055 BS by Municipality with the help of “Eastern area Directorate” under Ministry of Building and Physical planning, Government of Nepal. The land within the proposed construction width which is the existing road width is already in use by the public. However, due to the availability of existing road width in a few locations the construction width of the roads is reduced. The details are presented in **Table 2-2** (Section 2.2: Land use). Further, the details of RoW, existing width, proposed construction width and carriageway widths are presented in **Table 3-1, Salient features**. The proposed road will be upgraded within existing road width only. Thus, there are no any impacts on structures, private or public/ temporary or permanent structures due to upgradation works. so, the additional land won't be necessary for this sub-project for the upgradation works.

2.13 Physical Structures

There are no any public as well as private structures present along the construction width of this sub-project. Therefor the alignment is clear for the construction of work.

2.14 Caste/Ethnicity

The caste/ethnicity wise details of households are presented in **Table 2-14**. There is presence of mixed groups and cluster of groups of castes in the project area. The necessary consultations with these groups were conducted during field study. The consultations with different groups including IPs were also organized during filed study. The consultation summary with different groups is presented in chapter 5 of this report.

Table 2-14: Caste/Ethnicity of People along proposed interconnected road alignment

SN	Name of the Road	Gaighat townplanning area Road Section	City hall simaltar Gaighat Diktel road section	Babarani DM Gate Road Section	City Hall Ramchandra Basnet house Road Section	Gaighat Shivalay tole-mannohan school to karamgachhi diktel Road	Fulchowk PG Chowk Road Botetole Triyuga Corridor section	Pragati tole kha (Quarter front) ko road to Sakela road ward 12	Total Household Nos.	%	Remarks
		HH	HH	HH	HH.	HH	HH	HH			
1	Brahmin	72	20	1	7	1	8	16	125	13.23	Khas/ Arya
2	Chhetri	88	70	17	4	17	7	18	221	21.33	Khas/ Arya
3	Tharu	20	15	4	0	4	23	3	69	13.07	IP
4	Rai	45	30	7	4	7	14	20	127	16.12	IP
5	Magar	20	5	0	2	0	4	0	31	7.30	IP
6	Madheshi	53	10	15	0	15	9	0	102	9.94	
7	Newar	30	20	5	3	5	6	2	71	5.69	IP
8	Pariyar	10	0	0	0	0	0	0	10	0.80	Dalit
9	Biswokarma	5	3	0	0	0	1	15	24	2.73	Dalit
10	Muslim	5	0	7	0	7	0	0	19	1.52	
11	Pariyar	2	7	0	0	0	1	18	28	2.25	Dalit
12	Bote	0	18	0	0	0	18	0	36	2.89	IP
13	Majhi	0	0	1	0	1	0	0	2	0.16	IP
14	Danuwar	0	0	2	0	2	0	0	4	0.32	IP
15	Mushar	0	0	0	0	0	5	0	5	2.00	
16	Sharki	0	0	0	0	0	0	8	8	0.64	Dalit
Total		350	198	59	20	59	96	100	882	100	

Source: field study 2023

2.15 GBV, SEA / SH

During the field study, group discussions, interviews and meetings revealed that there is gender-based violence in the project area. In the focus groups discussion people said that some domestic (mental) and economic violence is present in the area. The municipal social section record also shows that GBV present within the municipality. There are more than 39 consultations (KII, FGD and meetings) were conducted during preparation of ESMP. During the consultations, 427 numbers of people participated. The male and female participants were 246 (57.61%) and 181 (42.39%). Most of the local women were not aware about the provisions of complaint registering through toll free number 1145 facilitated by National Women Commission. Details of violence present within the municipality are presented in the **Table 2-15** and related documents attached in **Annex-II**. According to municipal data, issues on GBV/SEA/SH are taken for planning of mitigation measures which are presented below. Necessary mitigation programs are planned and implemented during construction of sub project.

As a part of the project compliance, an Anti-Harassment Cell and GRC has been formed in Triyuga Municipality with dedicated focal person. **Mr Raj Kumar Khadka** is appointed for focal person who also serves as GBV focal person of the municipality. The letter regarding deputation of the focal person has been attached in **Annex II**. The main purpose of establishment of Anti-Harassment cell is to deal with cases of sexual harassment in a timely manner by providing empathetic support to the victim and ensuring prevention of harassment through proper redressal of the case. The cells process individual complaints and take appropriate action in accordance with the guidelines provided by the respective authorities.

Table 2-15: GBV Cases of Municipality

SN	Identified Cases	Number			Total	Remarks
		F/Y 2077-078	F/Y 2078-079	F/Y 2079-080		
i.	Physical Abuse	13	29	25	67	
ii.	Polygamy	3	6	5	14	
iii.	Railing (गालीगलौज)	4	8	20	32	
iv.	Mental Abuse	2	8	13	23	
v.	Character Abuse	6	2	3	11	
vi.	Resource violence	2	4	2	8	
vii.	Child Marriage	15	7	5	27	
viii.	Rape/Sexual violence	21	7	13	41	
ix.	Rape attempt	2	4	2	8	
x.	Sexual abuse	4	0	0	4	
xi.	Women trafficking	0	0	1	1	

Source: Municipality Social section, Triyuga Municipality

According to the above presented data, there is presence of different types of GBV within the municipality and project area. Therefore, issues on GBV, SEA/SH issues are taken for planning of mitigation measures. Necessary mitigation programs will be planned and implemented during construction of sub project. The most prioritized issues on violence are

- Physical & mental abuse
- Sexual violence/rape cases
- Child marriage

For mitigation of SEA/SH, and GBV during the construction phase, the following activities are proposed and will be implemented which needs to continue in the operation stage also. For carrying out these activities, the mitigation action plan is prepared, budget is estimated and included in ESMP. The overall responsibility of carrying out these activities will be on PIU/DSC and contractor based on the activities.

- Awareness programs related to community and workers, for reduction of sexual violence/rape cases.
- Awareness Program for physical/mental abuse in the project area.
- Awareness raising in Schools on child marriage.
- Establishment of Anti-Harassment Cell in Municipality and assign a focal person in the Municipality.
- Conduct an orientation program on code of conduct for contractors and subcontractors to manage GBV/SEA/SH risks during construction phase and Implementation of code of conduct on SEA/SH behavior and the provision of punishment for breaching of the code of the conduct.

3. DESCRIPTION OF THE PROJECT AND ITS ACTIVITIES

The upgrading of Different inter connected Road (9.929Km) Sections have seven roads which are interconnected each other through two highways. These roads are starts from highway (Sagarmatha and Madan Bhandari), also intersects by these two highways in different locations. The overall length of the proposed road is 9.929 Km. These roads pass through ward no 10,11, 12 and 13 of Triyuga Municipality. The settlement of this municipality lies in the plain Area of inner Terai. The road section requires pavement reconstruction to maintain acceptable levels of service.

The ROW was declared in 2055 BS by Municipality with the help of “Eastern area Directorate” under Ministry of Building and Physical planning. Government of Nepal. The Salient features of the road along with declared RoW, existing width, proposed construction width, proposed carriage way width of the road is presented in **Table 3-1**. The components of the proposed roads are improvement of pavement, Construction of footpath, side drains, road crossings, retaining wall/breast wall, zebra crossing, shifting of electric poles and improvement of the road furniture.

3.1 Salient Features of the Project

Salient features of the proposed Different inter connected Road Sections are shown in **Table 3-1**.

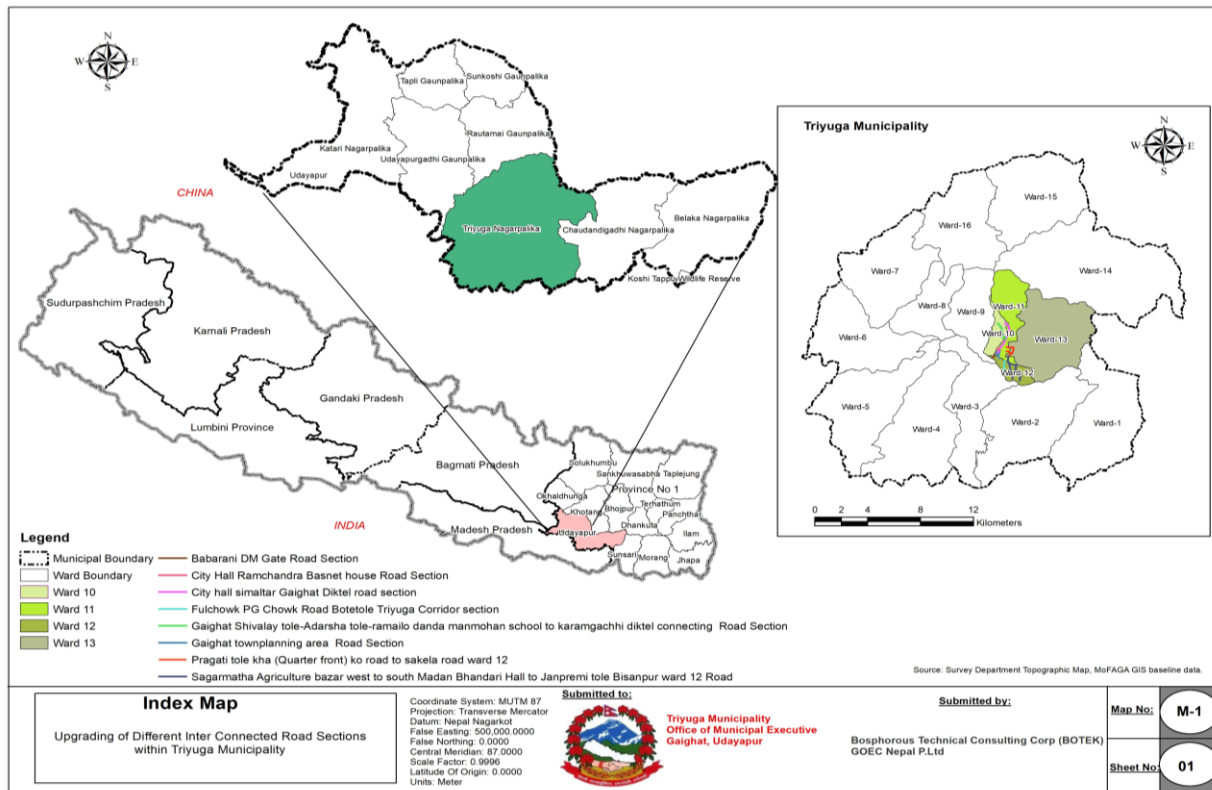


Figure 3-1: Index Map

Table 3-1: Salient features of interconnected road sections

SN	Description	Description				
1	Name of the Road	Different inter connected road (9.929 Km) sections				
2	Road Type	Urban/ Local Road (NURS 2076)/Collector Road				
3	Proposed road length	9.929 Km				
4	Number of Lane	Four and Two Lane				
5	Road Attributes					
	Name of the Road	Length (Km)	RoW (m)	Existing width (m)	Proposed construction width (m)	Proposed carriage way width (m)
a.	Gaighat town planning area road Section I	1.45	12 for 0+000 to 0+140, 18 for 0+140 to 1+010	12 for 0+000 to 0+140, 18 for 0+140 to 1+010	12 for 0+000 to 0+140, 17.5 for 0+140 to 1+010	7.5 13
	Section II	0.489	10 for 1+010 to 1+045 12 for 0+000 to 0+489	10 for 1+010 to 1+045 11.5 for 0+000 to 0+240 8 for 0+240 to 0+489	8 for 1+010 to 1+045 11.5 for 0+000 to 0+240 8 for 0+240 to 0+489	5.5 7 5.5
b.	City hall- Simaltar-Gaighat- Diktel road section	2.86	20	20 for 0+000 to 0+260 13 for 0+260 to 2+860	20 for 0+000 to 0+260 13 for 0+260 to 2+820 10.65 for 2+820 to 2+860	16 7.5 6.5
c.	Babarani DM Gate road section	0.622	8	8 for 0+000 to 0+260 6.3 for 0+260 to 0+420 8 for 0+420 to 0+520 6.75 for 0+520 to 0+610 8 for 0+610 to 0+622	8 for 0+000 to 0+260 6.3 for 0+260 to 0+420 8 for 0+420 to 0+520 6.75 for 0+520 to 0+610 8 for 0+610 to 0+622	5.5
d.	Fulchowk PG chowk Road Botetole Triyuga Corridor section	0.9	8	8	8	5.5
e.	City hall- Ramchandra Basnet house section	0.255	8	8 for 0+000 to 0+160 6.3 for 0+160 to 0+240 8 for 0+240 to 0+255	8 for 0+000 to 0+160 6.3 for 0+160 to 0+240 8 for 0+240 to 0+255	5.5
f.	Gaighat Shivalay tole-Adarsha tole-Ramailo danda Manmohan school to Karamgachhi Diktel connecting road section	1.6	8	8	8	5.5
g.	Pragati tole kha (In front of Quarter) ko road to Sakela road ward 12					

SN	Description	Description				
	Section I	0.788	10	8 for 0+000 to 0+560 6.3 for 0+560 to 0+600 8 for 0+600 to 0+740 6.3 for 0+740 to 0+760 8 for 0+760 to 0+788	8 for 0+000 to 0+560 6.3 for 0+560 to 0+600 8 for 0+600 to 0+740 6.3 for 0+740 to 0+760 8 for 0+760 to 0+788	5.5
	Section II	1.37	10	8	8	5.5
6	Camber of Carriage way	2.50%				
7	Pavement Surfacing	40 mm Asphalt 0.15 m Base 0.15 m Sub-base				
8	Terrain Type	Plain				
9	Climate	Tropical				
10	Geology	Sub Himalayan				
11	Wards & Major settlements	Triyuga Municipality –10, 11,12 and 13 Gaighat Bazar, Adarshanagar tole, BP chowk, Bidhut margha, Pipal chowk, Innar chowk, Janmukhi chowk, Simaltar, Babarani tole, Fulchowk, Bote tole, Sagarmatha tole, Sansari margha, City Hall, Adarsha tole, Ramailo danda, Karamgachhi, Pragati tole, Bishalnagar tole, Sakela tole.				
Design Parameters						
12	Design speed of Road	30 km/hr				
13	Design Life of Road	20 Years				
14	Maximum Gradient	4%				
15	Minimum Gradient	0.35%				
16	Total cost of EMP	NPR. 1700,000.00				
17	Total Project cost	NPR. 677,075,718.14 (including VAT and contingency)				
18	Cost per km	NPR. 68,191,733.12 (including VAT and contingency)				

Note: **Right of Way**- RoW defined by the municipality;

Carriageway Width- Black topped road width.

3.2 Proposed Campsite and Stockpile Area

During the field study, the campsite and stockpile area has been identified. The proposed campsite and stockpiling area are government land having an area of about 31436 Square meters which is situated near the north side of proposed Baruwa Bus Park (Sakela Area). It is 600 m far from the proposed site. It is an open ground area situated near Sakela Tole and can be used for contractor’s campsite and stockpiling of the construction materials. The existing land is barren land having some vegetation. Required facilities like separate male and female camps and toilets will be constructed during construction phase. Necessary consultation with municipality along with locals were conducted with presence of Mayor. The minutes related to consultation is attached in **Annex II**.

Table 3-2: Campsite and Stockpiling Location Details

SN	Description	Easting	Northing	Available Area (Sqm)	Remarks
1	Camp site	471259.33m E	2963533.47m N	2884	
2	Stockpile area	471252.25 m E	2963346.15 m N	7289	
3	Solid Waste management area	471255.50m E	2963501.14m N	50	Within camp area

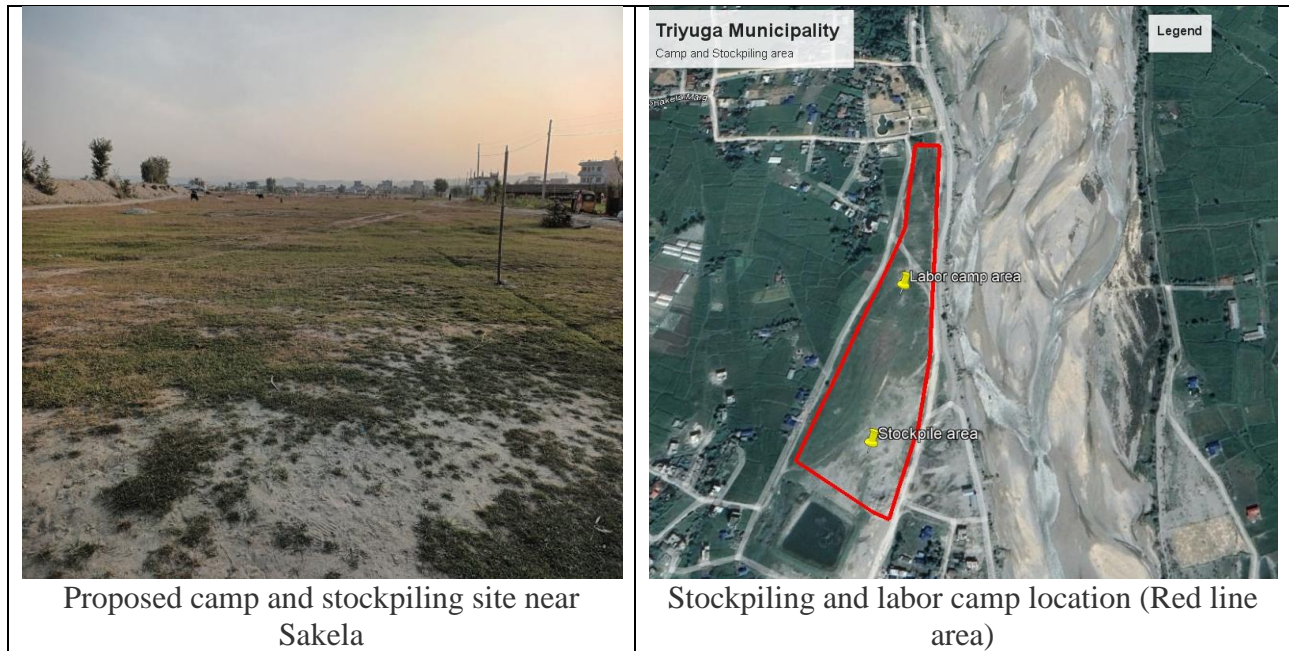


Figure 3-2: Proposed camp and stockpiling site

3.3 Solid Waste Management

Solid waste generated from the labor camps will be disposed within the proposed camp site. Soak pits or septic tanks will be established for the sanitation units/latrines at appropriate location within campsite. As the labor camp/stockpiling area is big, contractor will use one point (471255.50m E, 2963501.14m N), tentative area 50sqm) for solid waste management. Also, the waste generated during decommissioning of the temporary campsite, the reusable like cardboards, plastics, bins, etc. will be sold, the metal scrap will be sent or sold to scrap dealers, and any residue will be disposed off in coordination with the local ward/municipal authority

through existing solid waste management (collection & disposal) system of the municipality. The land will be cleared and restored to the satisfaction of the Municipality.



Picture: 1 : Solid waste management area

3.4 Spoil Management

As per the DPR of the road, there is earth cutting (26,251.83 m³) of earthwork is more than filling work (11,151.88 m³). After the backfilling, 15,099.95 m³ construction spoil will need to be managed in appropriate site. An open public space located around 500 m from Sagarmatha agri road section II end point (near Bisanpur Harid Park ward-12) with area of around 10,000 sq.m has been proposed as spoil disposal site which is the government land and far from the nearby settlement. Municipality letter related to spoil management site is attached in Annex II. Construction debris will be disposed at designated spoil site only and efforts will be made to minimize such waste through reuse, reduction, and recycling concepts. While hauling and storing spoil temporarily, spoil will be covered with plastic/tarpaulin cover. Construction trash should only be disposed of at approved spoil sites. The land will be cleared and restored to the satisfaction of the Municipality by following environmental obligations.

Table 3-3: Details of Spoil Generated by Subproject

S.N.	Name Of Road	Cut Quantity (m3)	Fill Quantity (m3)	Remarks
1	Gaighat Town planning road			
	Section-I	5542.37	345.76	

S.N.	Name Of Road	Cut Quantity (m3)	Fill Quantity (m3)	Remarks
	Section II	962.56	349.877	
2	City hall Simaltar Gaighat Diktel road section	8714.38	3944.275	
3	Babarani DM Gate road section	1581.113	175.747	
4	Fulchowk PG Chowk Road Botetole Triyuga Corridor section	1283.07	1269.24	
5	City Hall Ramchandra Basnet house road section	462.64	134.365	
6	Gaighat Shivalay tole-Adarsha tole-ramailo danda manmohan school to karamgachhi diktel connecting road section	4963.542	1753.892	
7	Pragati tole kha (Quarter front) ko road to sakela road ward 12			
	Section I	1222.72	484.076	
	Section II	1519.432	2694.65	
	Total	26251.827	11151.882	

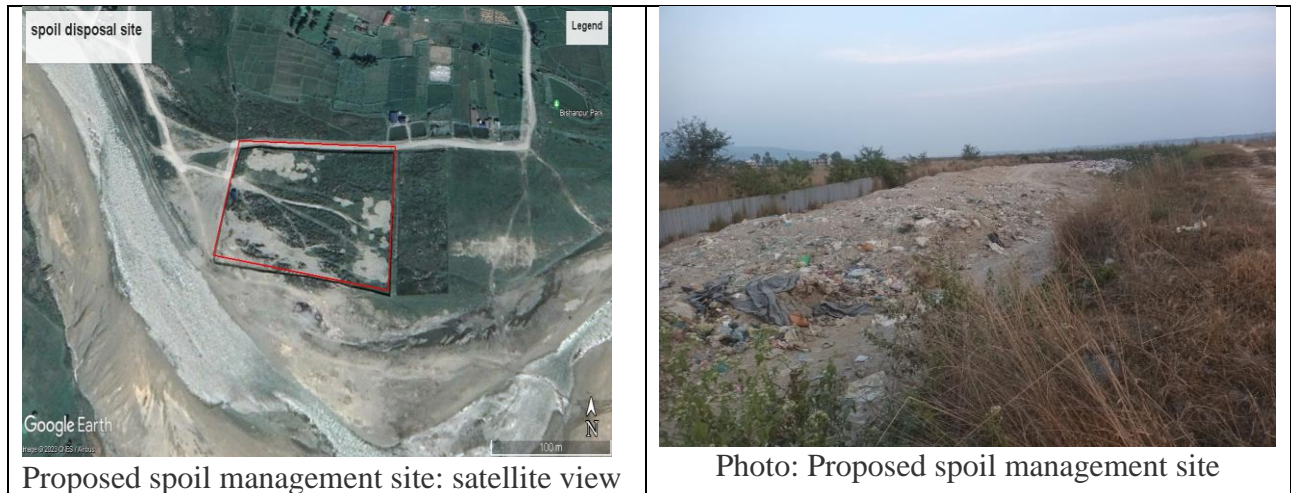


Figure 3-3: Proposed spoil management site

3.5 Quarry Sites

The upgrading of road will require boulders, sand and aggregate for activities like gravelling, construction of retaining walls, cross drainage, side drains, road furniture and other structures. These construction materials will be brought from the established quarry sites of municipality and crusher industry nearby the municipality. These construction materials will be brought from the IEE approved quarry sites at the Baruwa Khola, Triyuga Khola and Sunkoshi Khola (which have already received the environment clearance, IEE approval) within the Municipality. The details of quarry name and available quantity are presented in **Table 3-4** and the quarry site is shown in below **Map (Figure 3-4)**.

Table 3-4: Quantity of Sand, Aggregate and Stone required for the sub project construction

SN	Particulars	Quantity (m3)	Remarks
1	Sand	3764.38	
2	Stone	975.18	
3	Aggregate	43,584.72	

The Contractor may also obtain required construction materials from the legally operating crusher industries other than proposed quarry sites. So, the direct impact of quarries such as damage to cultivated land, high level of air pollution, noise pollution, cracking of houses nearby structures by heavy equipment movement, affect to the downstream aquatic life etc are not expected in this subproject. However, the quarry sites and amount of quarrying material will be included in Construction Environment and Social Management Plan (CESMP) within 45 days of commencement of works. PIU will check the site requirements and quality of quarrying material and approve it. DSC will also monitor whether the quarry sites has been legally operating or not.

Table 3-5: IEE Approved quarry site description

SN	Quarry Name	Ward No	Northing	Easting	Available quantity (m3)/year	Remarks
1	Babari (Rato Pahiro)	10	26°49'18.08'' to 26°49'8.37'' N	86°42'1.40''E to 86°42'7.47''E	9660	Approved IEE of these sites is not mentioned separate volume of the stone, aggregate and sand to be available
2	Bisanpur	12	26°46'36.17''N to 26°46'16.76''N	86°43'11.18''E to 86°43'26.15''E	25375	
3	Sakela	11, 13	26°48'5.09''N to 26°47'36.63''N	86°42'39.59'' E to 86°42'45.32''E	38080	
4	Dimki	11	26°48'25.65'' N to 26°48'6.42''N	86°42'38.34'' E to 86°42'42.59''E	14280	

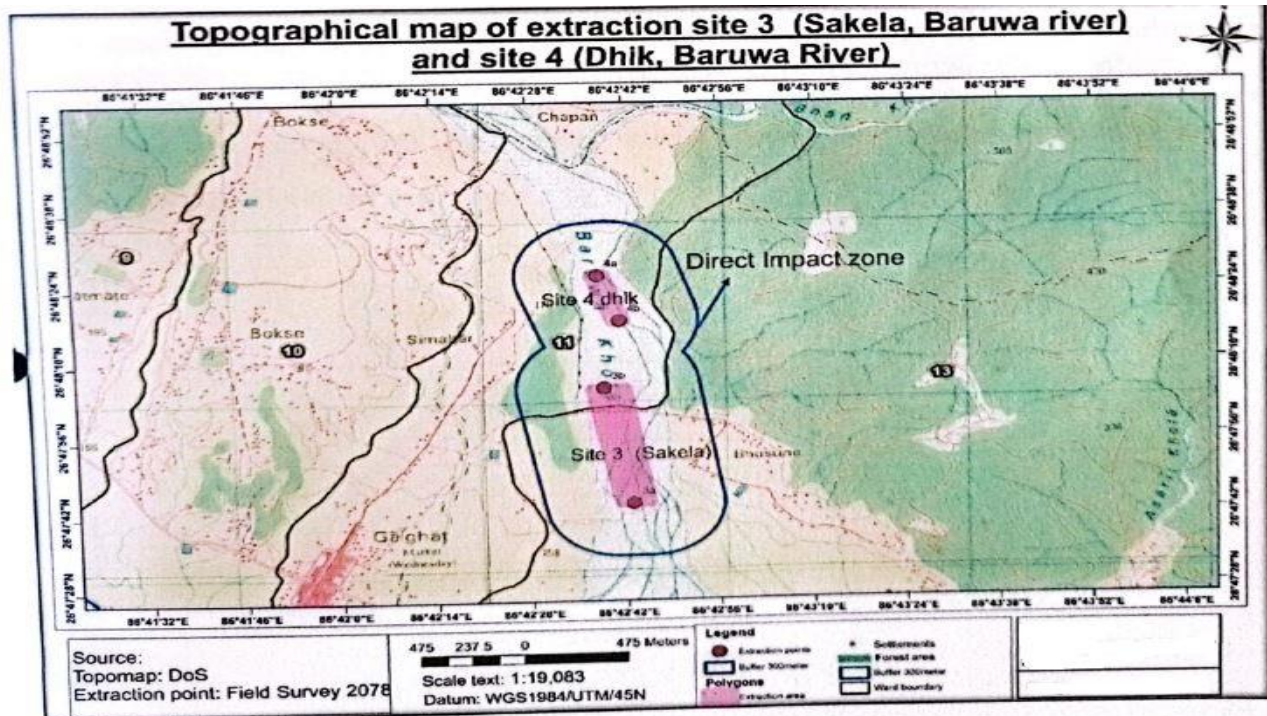


Figure 3-4: Topographic map of Quarry site

3.6 Borrow Pits

As per DPR, the quantity of earthwork cutting (26,251.83m³) is more than earth filling work (11,151.88m³), thus there is no need of borrow pits in this sub project however for ease and future requirement, borrow pits has been identified. The borrow pit is proposed at the Triyuga River which is 500m (Easting 471634.00m E, Northing 2960654.00m N) far from Sagarmatha Agri road, chainage 2+140 of section II (near Bisanpur Harid Park) and having IEE clearance by municipality. The letter of municipality about borrow pits is attached in **Annex II** of this report.

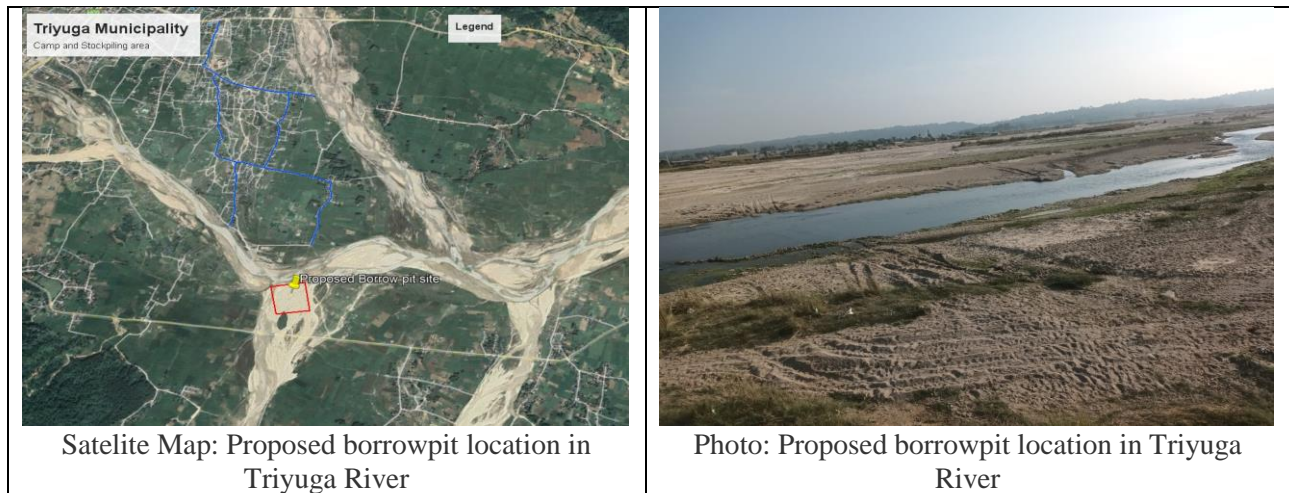


Figure 3-5: Proposed Borrow Pits site

3.7 Other facilities

The facilities like workshop area, concrete batch plant will be established during construction of road. The site is identified during filed study. It is proposed near the bank Baruwa Khola (Easting 472352.00 m E, Northing 2961269.00m N) which is 635m far from our site and is a government property. Necessary consultation with Municipality was conducted and minutes is attached in **Annex II**. Further, CESMP will be prepared by contractor, contracted within 45 days of contract. Thus, facilities like Workshop area, concrete batch plant establishment details will also be included in contractors CESMP.



Figure 3-6 Proposed other facilities establishment site

4. ENVIRONMENTAL AND SOCIAL MANAGEMENT PLAN (ESMP)

4.1 Social and Environmental Screening:

The Environment and Social Management Plan (ESMP) including monitoring plan provides a road map on how to address the anticipated negative environmental and social impacts associated with the implementation of the proposed development project throughout its project life cycle. The outline of the plan considers the anticipated impacts, the mitigation measures, the parties responsible for the action, time frame for the implementation and cost to address them. The monitoring process aims at generating useful information that would be useful in facilitation of the implementation of the mitigation measures.

The implementation of the ESMP shall be the responsibility of both the Municipality/PIU/DSC and contractor. Proponent will play both implementor and supervisory role for ensuring proposed measures are implemented as outlined in the plan. The frequency of implementation and monitoring of the proposed measures shall be dependent on the nature of the anticipated impact. The cost for the implementation of the ESMP during the construction phase will be factored in the bills of quantities (BOQ) while the cost after completion will be met by the PIU.

The proposed sub-project is an upgradation of an existing road. By using approved format, A social and environmental screening of the sub-project has been done to assess any potential risk associated with the road upgradation work. The checklists for social and environmental safeguards screening are provided in **Annex I**. The screening shows that the project area doesn't have major social or environmental risks. The land required for road construction is already available and is in public use. Documents regarding declaration of the RoW by the Triyuga Municipality is provided in **Annex II** of this report. As per people consultation during the field study, there are no any disputes regarding land ownership, existing road width and grievances. As per section B & C of Appendix C of Environmental and Social Management Framework (ESMF, August 2020) of NUGIP, it can be concluded that the proposed road upgradation project falls under 'Low to Medium impact project' under Category B. Hence an abbreviated ESMP is sufficient for the proposed project. This ESMP document has been prepared as per Appendix C of the ESMF.

4.2 Scope of Environmental and Social Management Plan:

The scope covers potential impacts, related mitigations and monitoring aspects related to activities under the proposed road upgradation works in Triyuga Municipality. These are summarized in **Table 4-2** below.

4.3 Implementation of Environmental and Social Management Plan

The sub-project ESMP implementation arrangements can be summarized as follows;

Table 4-1: Roles and responsibility for ESMP implementation

SN	Stakeholder	Role & Responsibilities for ESMP implementation
1	Triyuga Municipality/PIU	<ul style="list-style-type: none"> ✓ The overall project environmental and social safeguard management is the responsibility of PIU ✓ The regular monitoring will be carried out by the PIU
2	Contractor	<ul style="list-style-type: none"> ✓ The contractor is required to submit C-ESMP within 45 days of contract signing.

SN	Stakeholder	Role & Responsibilities for ESMP implementation
		<ul style="list-style-type: none"> ✓ Implement the mitigation measures and provisions as per ESMP and DPR of the Project
3	DSC	<ul style="list-style-type: none"> ✓ Preparation of ESIA/ESMP during DPR phase ✓ Supervision support to the Contractor to implement the ESMP ✓ Monitoring of implementation of ESMP and its compliance ✓ The E&S safeguards specialists of DSC will work closely with its technical staff to ensure project implementation in accordance to World Bank's safeguard standards.
4	PCO & PMST	<ul style="list-style-type: none"> ✓ The PCO will have overall responsibility to ensure compliance with pertaining laws, policies, regulation for all sub projects ✓ The PCO with support from PMST will review implementation support of environmental and social safeguard studies/ management plan prepared by PIU/DSC.

4.4 Objectives of ESMP:

The overall objective is to ensure that the environment and its surrounding areas are protected and developed to meet the needs of the local stakeholders and safeguard the requirements of the local people. The basic objectives of the ESMP are to:

- To ensure that all mitigation measures and monitoring requirements will actually be carried out at different stages of project implementation and operation - pre-construction, construction, and operation and maintenance;
- Establish the roles and responsibilities of all parties involved in the project's environmental and social management; and
- Ensure the budget required for implementation of recommended actions aimed at environmental and social management and its enhancement.

4.5 ESMP Matrix:

Following project interventions and related mitigation measures have been planned in relation to the existing status and potentials improvements for the proposed road alignments. The proposed interconnected roads nature and physical conditions are observed and found to be same. As the interconnected roads (7 nos.) having short length (255m) to long length(2860m) and almost same characteristics and lies in same plain terrain of Municipality. Thus, most of the impacts are same in nature and stated in respective headings. However, also, site specific impacts are listed in respective sections of below table.

Table 4-2: Environmental and Social Management Plan (ESMP) Matrix

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
Physical-Construction				
1.	Obstruction due to electric poles with in construction width Total 167Nos. (Gaighat town planning: 47, Cityhall Simaltar:47, Babarani: 4, Fulchowk:8, Cityhall Ramchandra: 5, Gaighat Shivalaya:29 and Pragati tole: 27)	<ul style="list-style-type: none"> • Obtain all necessary permits for dismantling and relocation of electric poles from NEA. • The cost of shifting should be included in project cost • Relocate the electric poles along the alignment in coordination with the Nepal Electricity Authority • The process should be completed prior the beginning of the road construction 	Municipality/DSC and Contractor in coordination with NEA.	Included in DPR BOQ (BOQ item no: A-2)
2.	Relocate and re-construction of Water Supply Pipe lines of the Triyuga small town water scheme (19.727 km pipelines likely to affected)	<ul style="list-style-type: none"> • Obtain permission (Obtained necessary estimation from relocation of pipes from water management committee) for relocate and reconstruction from Water Supply Management Committee. • The project must work in close coordination with the Water Supply Management Committee regarding disruption of water supply system in road section, • Alternative means of supply during the disruption and reconstruct the system will be arrange 	Municipality/DSC and contractor in coordination with water supply management committee	Included in DPR BOQ (BOQ item no: A-2)

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
3.	Quarrying Material and Operation	<ul style="list-style-type: none"> • Details in section 2.9. • Avoid and or minimize adverse environmental impacts arising out of quarry construction material exploitation in line with MOFE guidelines/ conditions / recommendations. • Include conditions for selecting borrow sites, timing and use of roads, maintenance of vehicles, selection of sites for material storage, rock blasting and aggregate production, handling hazardous or dangerous materials such as oil, explosives and toxic chemicals. • The construction materials will be brought from the established quarry sites located within or outside the municipality. So, the direct impact of quarries is not expected in this Subproject. • The municipality in support of DSC will monitor the quality of quarrying material and state of quarry sites. • The materials will be brought only from licensed vendors having environmental clearance. • Municipality may instruct the quarry operators to reinstate the established quarry sites as per agreed norms during environment clearance. • Prepare a CESMP and include the details of quarrying activities including required quantity, locations and required mitigation within 45 days of commencement of works and submit to the PIU for approval. 	Municipality/DSC, Municipality instructs the quarry operators to reinstate the established quarry sites as per agreed norms during environment clearance (follow IEE obligations)	Covers by municipality/PIU DSC monitoring cost
4.	Issues of stockpiling and construction material	<ul style="list-style-type: none"> • Locate and seek approval from the supervising consultant for the use of stockpile sites. The proposed campsite and stockpiling area are government land having an area of about 31436 Square meters which is situated near the north side of proposed Baruwa Bus Park (Sakela Area). 	Contractor	This will be the part of Contractor's Responsibility under Contractor Cost.

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
		<ul style="list-style-type: none"> • Stockpile should not be located on water courses; should not be within 50m of schools, hospitals or public standpipes; and should not affect locals and their properties. • Obtain written permission from landowners and local bodies for stockpiling on their land. • Only barren land will be used for stockpiling and proper insulator cover and proper drain will be managed to store the chemical to avoid the leakage of chemicals. • Stock of sand will be set wet to prevent it from blowing with the wind; water sprinkler will be used for this purpose. • The places used for the stockpiling of construction materials will be cleaned promptly after the completion of the project. • The area could be leased or rented based on price not lower than the prevailing market price 		This will be included in CESMP
5.	Borrow Pits	<ul style="list-style-type: none"> • Earth material Quantity required of the filling works, sub-grade, base and sub-base. • The construction materials will be obtained to supply and borrow pits must be legally operating with IEE approval from the concerned authority. • Implementation for the monitoring will be carried through contractor environment mitigation checklist which is prepared before the construction of structures • Reclamation of borrow pits, landscaping and tree plantation along the excavated borrow pits. • The borrow pit is proposed at the Triyuga River which is 500m (Easting 471634.00m E, Northing 2960654.00m N) far from Sagarmatha Agri road, chainage 2+140 of section 	Contractor	This will be the part of Contractor's Responsibility under Contract Cost. This will be included in CESMP

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
		II (near Bisanpur Harid Park) and having IEE clearance by municipality.		
6.	Spoil Disposal	<ul style="list-style-type: none"> Disposal of spoil into water bodies will be strictly prohibited and Generated spoil will be disposed only at designated spoil disposal sites. Open burning of solid waste from the Contractor's camps should be strictly banned The scarified spoil will be disposed within the RoW width along the alignment during the construction and required additional filling quantity is direct transport from the borrow pit site which is legally operating with IEE approval from the concerned authority. Implementation for the monitoring will be carried through contractor environment mitigation checklist which is prepared before the construction of structures. The identified spoil deposit site is an open public space located around 500 m from Sagarmatha agri road section II end point (near Bisanpur Harid park ward-12) with area of around 10,000 sq.m has been proposed as spoil disposal site which is the government land and far from the settlement. All the identified spoil disposal sites will be rehabilitated after disposal adopting natural drainage, and covering vegetation. 	Contractor	will be the part of Contractor's Responsibility under Contract Cost.
7.	Air/Dust Management	<ul style="list-style-type: none"> Road construction area shall be maintained damp by periodical spray of water. Gaighat town planning area, Simaltar, Babarani, Fulchowk and city hall area will be needed for water spray during construction. Delivery vehicles will be covered. 	Contractor/PIU/DSC	Included under Contractor's Cost The cost for the water sprinkling will be borne by the contract package.

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
		<ul style="list-style-type: none"> • Mixing equipment will be well sealed and equipped as per existing standards. • All construction vehicles should comply with Motor Vehicles and Transportation Management Act 2049 (Third Amendment) Rules, 2066 (2010). • Provide temporary hoardings where required to minimize dust impact on locations of temples (Shiva Mandir, Sakela park/Puspala Statue), primary health posts near Sakela park and schools (Manamohan Schol). • Provision of speed control measures in Gaighat town planning, Simaltar, City Hall, Fulchowk settlement and working areas to limit traffic speed. • Dust emission and air pollution due to construction activities and operation of heavy equipment and movement of transporting vehicles, to mitigate the impacts water will be sprinkled along the proposed road alignment and nearby dust prone area and repair and maintenance of equipment and vehicles regularly. • Air pollutant parameters (TSPM, PM10, Sox, NOx, Cox) will be monitored regularly during construction. Conforming NAAQS of Nepal. • Air quality monitoring/Sampling (at least 2 times during construction) 		(For Air quality test/sampling, Noise level monitoring and water quality test NRs 500,000)
8.	Noise, vibration due to the use of heavy machinery and other construction activities,	<ul style="list-style-type: none"> • Vehicles and equipment used will be fitted with silencer and maintained to keep noise at minimum levels and Select equipment and machinery with lower sound power levels for the use. • Workers will be provided with appropriate ear muffs/plugs specially at crusher site 	Contractor/DSC	Cost of noise level monitoring comes within the Air Quality monitoring cost above.

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
		<ul style="list-style-type: none"> • Sensitive locations i.e., Manmohan schools, , Triyuga Municipality office area etc. will avoided while placing the noise generating equipment. • Restrict activities with significant noise impacts to outside school (no horn sign). • Activities involving heavy machinery with significant noise impacts should be restricted to outside school hours. • Noise levels (1 hr Leq dB(A)) levels will be monitored regularly. Conforming WHO standards. 		Costs as a result of damage from vibrations will be borne by contractor.
9.	Water Pollution	<ul style="list-style-type: none"> • Hazardous materials shall not be stored near surface waters sources • Used lubricants and oils shall be collected and recycled or disposed off site. • Plastic sheeting shall be placed under hazardous material storage area to collect and retain leaks and spills. • Contaminated runoff from storage areas shall be captured in ditches or ponds with an oil trap at the outlet. • Contaminated and worn plastic sheeting shall be packed into drums and disposed off site. • Water Quality (EC, PH, DO, TSS, Oil and Grease). Conforming WHO standards. 	Contractor/DSC	cost of water pollution monitoring comes within the Air Quality monitoring cost included in above SN 7 of this table
10.	Labor Camp Location and Management	<ul style="list-style-type: none"> • Camps area should be barricades having proper maintained entrance gate and provided with a 24-hour guard having access to entry for authorized personnel only. Also, will maintain record of labor in proper way. • Camps shall not be located near settlements; near water supply intakes; or sites that affect the access by local people to drinking water. 	Contractor	Included in Contractor's Cost, It is the responsibility of Contractor.

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
		<ul style="list-style-type: none"> • Camp shall not be in the vicinity of landslide and flood plains. • Provide adequate health care facilities within construction sites and provide first aid facility round the clock • Install sanitary facilities for workers to avoid open defecation by construction of temporary toilet. • Separate Camps shall be constructed for Male and female workers with separate, safe and easily accessible facilities like separate toilets, Locker room etc. • Provide and maintain proper drinking water, sewerage and waste disposal facilities at the camps. • Camps shall be provided free of cost, with electricity and regulator & adequate fuel supplies of LPG or Kerosene. • After use, sites shall be cleared and restored to near natural or stable conditions with vegetative cover. 		
11.	Solid waste generation/ Waste from labor camp	<ul style="list-style-type: none"> • Waste minimization and waste segregation will be prioritized; 3R approach will be promoted. • Composting of organic waste generated from the camps will be disposed within the proposed camps. • Coordination with municipality for final disposal into the municipality's waste collection & disposal system. • Awareness raising event will be carried out. • Decommissioning waste will be re-used, sold to local scrap dealers. • Every week, it will be monitored for the management of waste from the camp site. • Further detailed plan will also be provided in C-ESMP with in 45days of contract agreement. 	Contractor	<p>Included in Contractor's Cost, It is the responsibility of Contractor.</p> <p>For awareness raising activities NRs 50,000</p>

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
12.	Road safety and Community safety	<ul style="list-style-type: none"> • Cross-Roads: Development Stretch of up to 15 m of each cross-road will be developed under this project. • Installation of Road markings at all major as well as minor intersections. Road Signs and Markings Road Markings has been provided as per Traffic Sign and Marking manual as per DPR. • The signage/hoarding board needed area are Gaight town planning, Simaltar, City Hall, Fulchowk, Sakela, DM Gate as well as Manmohan School area. • Retaining/ Breast wall: Stone Masonry Retaining wall has been provided along the alignment where embankment is required. • Installation of Street lights as per need. • Awareness activities (at least two event, minimum participants 30nos.) will be conducted to inform & aware locals regarding potentials risks and proposed safety measures related to the project activities 	DSC/Contractor	<p>Included in DPR BOQ (BOQ item no: B-7, H-41, H-42, H-43, H-44, H-46, H-47, H-48)</p> <p>For awareness activities NRs 50,000.00</p>
13.	Construction Safety	<ul style="list-style-type: none"> • Provide barricades and green net in sites for restriction of local people movement during construction in densely populated area such as Gaighat town planning area, Babarani, Simaltar area. • PPE (helmets, masks, boots, gloves, ear plugs and goggles) should be provided for workers safety. Storm water collected should be disposed through the nearest culvert sections through side drains. • The contractor will assign a safety officer and the PIU's safeguard specialist will monitor the implementation of the OHS measures. 	Contractor	<p>It will be the part of Contractor's Responsibility under Contract Cost</p> <p>NRs 50,000 for worker safety materials.</p>

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
		<ul style="list-style-type: none"> • Adequate lighting and safety signal devices be installed for work safety. • Adequate warning signs, safety barriers, traffic calming measures and persons with flags to control traffic will be provided for work safety. • Protective clothing including helmets, masks, boots, gloves, ear plugs and goggles should be provided for workers safety. • At every work place, a readily available first aid unit including an adequate supply of dressing materials will be provided. • Maintain health care system at construction camps including regular visits by trained medical staff for routine checkup of workers and avoidance of communicable disease. • Temporary diversions will be provided wherever necessary, with proper drainage facilities. • Electrical Equipment will be checked and certified regularly. • Provide and install all road signs as per design. • Impart road safety education to all community, schools, clubs and drivers of construction vehicles. • Hazards will be identified, and workers will correctly wear PPE, will properly use safety equipment, and will follow work safety arrangements. Safety signs and information will be provided and the work space will be barricaded to prevent unauthorized entry. Workers and people at the construction site will be provided with proper training, and 		

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
		to help ensure that workers are trained on what to do in the event that an accident occurs on site.		
14.	Traffic Management	<ul style="list-style-type: none"> Emergency traffic management plan should be included in CESMP by the contractor and approved by the PIU/DSC. This is required to cope up with the restriction on the vehicular movement due to closure of road for reasons including construction. The plan may include informing about the scheduled road closure and the alternative routes identified to divert the normal traffic flow, transport material during off-peak time, provide advance notice to stop vehicles by erecting indicator signs at a necessary distance in order to reduce congestion at the site of work, thus enabling making of proper security arrangements, or lane wise traffic management. Traffic management for the Manmohan school, Chainage 1+550, will be maintaining traffic diversion for the teachers/students within the sub-project up to construction period. Also, alternative routes will be identified. Traffic Safety such as street lights, traffic control devices and other features shall be covered through “Traffic Signs Manuals Vol-I and Vol II” and “Road safety manual” published by the DOR. Provision of alternative routes to ease the congestion and built up of traffic. 	PIU/ DSC and Contractor	Included in DPR BOQ (BOQ item no: B-7, H-41, H-42, H-48)
15.	Demobilization of contractor	<ul style="list-style-type: none"> construction sites are cleaned from residual waste and fully reinstated; Enabling environment is provided for natural regeneration of work base and construction sites to natural condition. 	Contractor and monitored by PIU/DSC	Contractor cost.

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
16.	Risk associated with others facilities like Crusher, asphalt plant	<ul style="list-style-type: none"> The Contractor will develop and maintain emergency response system in order to address any accidents or other emergency situation or disaster at site such as fall of workers from height, collapse of pier, flood, earthquake, accident, etc. The Contractor will dispose all the chemical wastes generated during the time of construction safely without interrupting the existing nearby settlements, water bodies, forests and wildlife. 	Contractor and monitored by PIU/DSC	Contractor cost.
Physical-Operation				
1.	Road Stability and Drainage Management	<ul style="list-style-type: none"> Road side tree plantation to mitigate possible degradation of land along the project alignment, Ensure proper compaction as per design. Proper drainage should be maintained and monitoring should be done for clogging. 	Contractor/Municipality	Municipality Regular program during Operation.
2.	Air and Noise pollution	<ul style="list-style-type: none"> Maintain signs and speed restrictions on the road section within settlements area to reduce vehicle speed, dust generation, and where horns will not be blown and traffic speed will be regulated Strict enforcement of vehicle emission standards. Maintain road side tree plantation Air pollutant parameters (TSPM, PM10, SOx, NOx, COx, Pb). Conforming NAAQS of Nepal. Water quality (EC, PH, DO, TSS, Oil and Grease). Conforming WHO Standards. Noise levels (1 hr Leq dB(A)). Conforming WHO standards 	Transportation entrepreneur, local people, Municipality	Municipality Regular program during Operation
3.	Water pollution	<ul style="list-style-type: none"> The operation of proposed work doesn't pose serious threat on water bodies; however, washing vehicles on fresh water streams will be avoided. 	Drivers, Ward, local people	Municipality will monitor

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
4.	Climate change and Disaster Risk	<ul style="list-style-type: none"> Trees must be planted along the alignment so as to enhance the greenery and beauty of the city and to reduce erosion, draught, flood etc. 	Contractor along with municipality	
Biological-Construction				
1.	Slopes stabilizations/ Landslide, Vegetation clearing.	<ul style="list-style-type: none"> Bio-engineering has been proposed as mitigation for the stabilization of slopes along the road. Civil structures like masonry wall, gabion walls are constructed during construction There is no any vegetation loss during construction. 	Contractor	Included in BOQ (BOQ item no G-38, G-39, G-40)
Biological-Operation Stage				
2.	Impact on Vegetation	<ul style="list-style-type: none"> Encouraging local people for protection of roadside plantation carried out during construction. 	Municipality	No additional cost
Social- Construction Stage				
1.	Land use change	<ul style="list-style-type: none"> Land will be required for the establishment of labor camps, storage of construction materials, workshops etc as rented basis which is the temporary in nature. All the temporarily rented land will be rehabilitated into previous state or better than the earlier state maintaining natural drainage and acceptable to the land owner/DSC. 	Contractor.	Contractor cost.
2.	Pedestrian and houseowner Safety: General people, children and elderly people.	<ul style="list-style-type: none"> Diversions should be safe for children, the elderly, differently-able people and other general pedestrians during construction to ensure their mobility. School areas i.e Manamohan School should be emphasized and adjacent transitions should be ensured. Appropriate signs and signage are used during project execution and construction to raise awareness of potential safety hazards of construction. 	Contractor	Included in BOQ (BOQ item no B-7, H-41, H-42)

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
		<ul style="list-style-type: none"> 72 nos. of Zebra crossing are provided throughout the alignment in certain interval and major crossings. 		
3.	Health and Sanitation	<ul style="list-style-type: none"> Proper awareness on use of sanitation facility for worker, A chlorine solution will be provided to workers to purify drinking water. Regular health checkups, records of illness of the workers, monitoring of drinking water. 	Contractor	NRs 50,000.00
4.	Child labor and forced labor	<ul style="list-style-type: none"> No child (below 16 years) and forced labor will be employed in project. Age verification document of workers will be documented and contractor will maintain the record of labor employed. 	Contractor, Monitoring by DSC every month	No additional cost
5.	Occupational Health and Safety	<ul style="list-style-type: none"> PPE including mask, gloves and First aid kit will be provided to the worker and also kept in Camp. Worker will be given clear instruction to follow safety rules. Orientation on use of PPE during construction will be provided to workers and in regular basis, Induction and refresher training to the workers will be provided. Safe Drinking water will be provided to the camp for worker use. Provision of insurance to cover physical damage to workers. Contractor will be responsible to maintain the records of each and every accident and incidence and will make available to DSC/PCO/PMST as and when required. Contractor's team, staff and laborers can also make use of the GRM to raise complaints / grievances if any 	Contractor	Included in BoQ (BOQ item no B-8)

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
6.	Community Health, Safety and Security	<ul style="list-style-type: none"> • Display appropriate signage for use during construction and implementation of the project to enhance awareness creation on the potential hazards of the project. <ul style="list-style-type: none"> • Carry out site management practice such as the fencing around work area and road signage. • Increase public awareness of safety, health and environmental issues by providing information directly and indirectly through campaign. 	Contractor/ Public awareness campaign by the municipality	Included in the BoQ (BOQ Item no A-3)
7.	Grievance Redress, GBV, SEA/SH risks and Stakeholder Consultation	<ul style="list-style-type: none"> • Use a grievance resolution process that includes a team or party for negotiation and/or mediation. • Clear and timely dissemination of project information, and safeguards aspects including anti-harassment cell; Coordination with the local stakeholders • Awareness raising, information and dissemination about GRM and GBV (meetings, monitoring and logistic costs@ 1 meeting every month) • SEA/SH awareness raising activities, trainings and stakeholder engagements such as: community based-awareness program, School based awareness program, Awareness program for women and against the gender-based violence (Domestic violence, Sexual violence) • GRM will include mechanism for referring SEA/SH-related grievances • Production of leaflets, and information dissemination through local media of ESMP, project provisions including GRM and SEA/SH and ESMP document in ward offices. • 11 awareness/ orientation/ trainings (awareness on domestic violence (3nos.), Rape/Sexual violence (3nos.), Child marriage (2ns.) CoC (3nos.)) with at least 30 	PIU/DSC	NRs 1,000,000.00 (Further breakdown presented in table 4-3 below)

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
		<p>participants in each activity and will conduct during first quarter of 1st year and 3rd quarter of first year).</p> <ul style="list-style-type: none"> • Skill enhancement training targeting the women, vulnerable, disadvantaged groups including IPs like Rai, Bote and Chaudhary @ 1 event. 		
8.	SEA/SH risks	<ul style="list-style-type: none"> • Separately Camps for man & women and maintain minimum distance from one camp to other camp. • Providing female labor-centric facilities such as separate female toilets, separate female camps, separate family camps and mother's rooms on the site. • GRM will include mechanism for referring SEA/SH-related grievances. • Formulating and adopting Code of conduct including sections on the safety of women and girls (CoC should be included in all contracts and training on CoC should be provided to all workers) • CoC are understood through orientations and signed by workers. 	Contractor,	This will be part of contractor's cost
Social Operation Stage				
	Encroachment of ROW	<ul style="list-style-type: none"> • The municipality will work with wards to discourage encroachment into the RoW. 	Municipality	Cost will be borne by municipality
	Air and Noise Pollution	<ul style="list-style-type: none"> • Maintain signs and speed restrictions on the road section within settlements area to reduce vehicle speed, dust generation, and where horns will not be blown and traffic speed will be regulated. • Strict enforcement of vehicle emission standards. • Maintain road side tree plantation • Air pollutant parameters (TSPM, PM10, SOx, NOx, COx, Pb). Conforming NAAQS of Nepal. 	Municipality	Municipality will be responsible during operation stage.

Stage	Impact	Mitigation Measure	Responsibility *	Cost (Remarks if any)
		<ul style="list-style-type: none"> Water quality (EC, PH, DO, TSS, Oil and Grease). Conforming WHO Standards. Noise levels (1hr Leq dB(A). Conforming WHO standards. 		
	Traffic accidents and associated risks	<ul style="list-style-type: none"> Raise awareness of traffic rules, pedestrian / cycle lanes and installation of speed bumps to control speed near pedestrian crossing areas Traffic management plan will be developed, especially along congested locations. Traffic control measures, including speed limits will be enforced strictly. Further encroachment and squatting within the ROW will be prevented. 	Municipality	Cost will be borne by municipality
	Limited access for elderly and differently-able people	<ul style="list-style-type: none"> Provide training on the use of facilities; maintain signboards, lights, instructions in strategic locations. 	Municipality	Cost will be borne by municipality

4.6 Summary of Costs of ESMP Activities

The summary of costs for the ESMP activities is outlined in **Table 4-3** below

Table 4-3: Summary of Cost of ESMP Implementation

SN	Items & Headings	Unit	Qty	Rate	Total, NPR	Remarks
1	Water quality test	Samples	4		500,000	
2	Air quality monitoring/Sampling	Samples	2			
3	Noise level monitoring	Samples	2			
4	Awareness/trainings on GBV, SEA/SH and will conduct during first quarter of 1st year and 3rd quarter of first year) with at least 30 participants in each event	Events	11	50000	550,000	
i	Awareness program on reduction of sexual violence/rape cases	Events	3			
ii	Awareness program regarding domestic violence (physical/ Mental abuse)	Events	3			
iii	Awareness for reduction of Child marriage	Events	2			
iv	Awareness/Training on CoC	Events	3			
5	Solid waste management	Events	1		50,000	
6	Awareness on Health and sanitation of workers and provision of chlorine solution	Events	1		50,000	
7	Awareness on Road safety and community safety	Events	2	25000	50,000	
8	Construction safety (PPE, Protective clothing including helmets, masks, boots, gloves, ear plugs, goggles etc)	LS	1	50000	50,000	
9	Skill enhancement training targeting the women, vulnerable, disadvantaged groups including IPs like rai, bote and chaudhary @ 1 event (NRs 100,000 per event)	Event	1	100000	100000	
10	Social safeguards (grievance meetings, site monitoring, etc)	Meetings/Events	18	12500	225,000	
11	Information dissemination materials and medium					
	(a) Leaflets and ESMP document printing				50,000	
	(b) PSA (local media)		3	25,000	75,000	
	Total				1,700,000	

Table 4-4: Implementation Schedule for Social Measure, GBV, SEA/SH

S N	Activities	Nos. of event	2024												2025						Remarks/ Targeted people	Responsi bility						
			11	12	1	2	3	4	5	6	7	8	9	10	11	12	1	2	3	4			5	6				
1	Awareness/ Orientation/Training on SEA/SH, GBV																											
i	Awareness on reduction of Physical and Mental Abuse.	3																									Local people	DSC/PIU
ii	Awareness on sexual violence/rape cases	3																									Youth of project area	DSC/PIU
iii	Awareness on reduction of Child Marriage	2																									Girls in Schools	DSC/PIU
2	Skill enhancement training targeting the women, vulnerable, disadvantaged groups including IPs like Rai, Bote and Chaudhary	1																									Women, vulnerable, disadvantaged groups (IPs)	DSC/PIU
3	Orientation on Code of conduct for Project staffs and contractors/subcontractors to manage GBV/SEA/SH risks	3																									Project staffs, staffs of contractors and sub-contractors	Contractor /DSC/PIU
4	Awareness on Health and sanitation for workers	1																									Workers	Contractor /DSC/PIU
5	Awareness on Road safety and community safety	2																									Locals/ Workers	Contractor /DSC/PIU
6	Awareness on Solid waste Management	1																									Workers	Contractor /DSC/PIU
7	Regular monitoring and reporting																											DSC/PIU

Table 4-5: Implementation schedule for Environmental measures

SN	Impact	Mitigation Measures	2024												2025					
			1	2	3	4	5	6	7	8	9	10	11	12	1	2	3	4	5	6
1	Air Pollution (Dust)	Water Spray																		
2	Other air pollution parameters	Instrumental analysis																		
3	Water pollution	Lab analysis																		
4	Noise pollution	Instrumental analysis																		
5	Spoil disposal	Proper Supervision																		
6	Borrow pits	Excavation as specified, Reclamation and rehabilitation																		
7	Road Traffic safety	Installation of safety sign boards, campaigning																		
8	Campsite monitoring	Supervision																		
9	Occupational Health & Safety	Safety awareness programme, PPE																		
10	Solid Waste Management	Disposal practice, 3R principal awareness for worker																		
11	Loss of vegetation; greenery promotion	Road side plantation																		
12	Quarry sites	Excavation as specified, Reclamation and rehabilitation																		

4.7 Environmental & Social Monitoring

Environmental and social monitoring is an essential tool to make ensure the implementation of mitigation measures and to know the effectiveness of those measures. ESMP monitoring is necessary for the following purposes;

- to track the impacts,
- to evaluate the effectiveness of proposed mitigation measures, and
- to suggest improvements, if any new circumstances arise.

The following table summarizes the plan for environmental and social monitoring for the proposed project;

Table 4-6: Environment and Social monitoring Plan

SN	Monitoring Aspects	Location	Parameters	Methodology	Frequency	Responsibility
A	Construction Phase					
1.	Air quality monitoring	Settlement areas near road alignment	At least TSP; PM ₁₀ /PM _{2.5}	Air Sampler / Detector	Quarterly	DSC
2.	Noise Levels	Settlement areas near road alignment	Average noise levels (L _{eq})	Noise Meter / Android Application	Every Month	DSC
3.	Water Quality	Near disposal site and Campsite	Parameters as per ESMF of the project	Laboratory Analysis	Quarterly	DSC
4.	Spoil Disposal	Along road alignment	Spoil tip sites; road sections where spoils are generated	Site verification	Every month	Contractor/ DSC
5.	Road Traffic safety	Along road alignment	Status of road for use; road accidents records, Management of diversion, traffic signboards, flow of existing traffic, accessibility to the locals, grievances etc.	Use of Logs; Records of complaints	Every Month	DSC, PIU
6.	Camp site monitoring	Campsite	Space for workers; Potable water; Sanitation facilities; waste management, First Aid, rehabilitation after closure of the camp sites etc.	Site verification; records of provisions of WASH materials;	Every Month	DSC, PIU
7.	Occupational Health & Safety	Active construction sites; camp site	Provision/Use of PPEs; First Aid/treatment; Awareness/	Site verification; records of supply of	Every Month	DSC, PIU

SN	Monitoring Aspects	Location	Parameters	Methodology	Frequency	Responsibility
			orientations conducted for workers	PPEs; records of events		
8.	Solid Waste Management	Camp site and along the alignment and other construction areas	Types of waste generation, quantity, disposal practice	Observation	Every Month	DSC, PIU
9.	Loss of vegetation; greenery promotion	Active construction sites; road sections passing through vegetated area	Site clearance at vegetated areas; plantation works, maintenance etc. as per EMP.	Site verification; records of trees cut; records of newly planted trees	Every Month	DSC, PIU
10.	Impact upon physical structures due to vibration of heavy machinery	Along the alignment	Public and private structures affected/cracks shown, loss of business, rehabilitation and relocation, etc.	Site verification, observation, documents in place.	Every Month	DSC, PIU
11.	Quarry sites	All the quarry sites	Land slide, water logging, impact upon public, impact upon water bodies, dust, impact upon physical structures, complain and grievances, rehabilitation of the sites etc.	Observation	Every Month	DSC, PIU
12.	Borrow pits	All the borrow pits excavated for the project	Land slide, water logging, dust, and impact upon private land, impact upon physical structures, complain and grievances, rehabilitation of the sites etc.	Observation	Every Month	DSC, PIU
13.	GBV and SEA/SH	Work sites; settlement areas near camp site	Laborers' records; Cases of GBV in relation to project works; any unrecorded cases	Community consultation; GRM records	Every Month	DSC, PIU
14.	Other facility sites established by the contractor	Project area		Observation	Every Month	DSC, PIU
B	Operation & Maintenance Phase					

SN	Monitoring Aspects	Location	Parameters	Methodology	Frequency	Responsibility
1	Road stability	Road alignment	Status of slopes along road alignment; status of road components	Site verification; ward records; traffic police office records	Every 3 months - 1 st year; Bi-annually after that	Municipality
2	Water pollution	Surface flows (natural drainages & irrigation canals)	Parameters as per standards (Annex IV)	Laboratory Analysis	Every 6 Months	Municipality
3	Vegetation/tree survival rate	Plantation sites	Number of surviving trees; status of planted trees	Site verification; interaction with road-side communities	Every 6 Months	Municipality
4	Road safety	Road alignment and adjacent settlements	Road accidents; status of road components	Site verification; interaction with road users	Every Month	Municipality

Monitoring activities during O&M Phase will need to be scheduled based on site requirements. Contractor will be responsible for monitoring during DLP period. After the DLP period, Triyuga Municipality will be responsible for the Operation and Maintenance phase of sub project.

5. STAKEHOLDER ENGAGEMENT AND INFORMATION DISSEMINATION

5.1 Stakeholder Engagement

The local community, ward offices, water supply user committees, School, labourers and farmers were consulted during the preparation of the ESMP. Consultations were also carried out with the women and indigenous communities living along the proposed road alignment. The main objectives of these consultations were to aware public on environment and social issues, find the issues/mitigations to be included in ESMP report. Focused group discussions (FGDs), formal consultation meetings, and Key informant interview (KII) were used to engage as many locals as possible during the public consultation process. The details of design aspects of the road, social & environmental safeguards, and GBV aspects were discussed during the public consultations. The list of people and institutions consulted are presented in **Table 5-1, 5-2**, minutes are attached in **Annex II** and issues raised during consultations are listed in **Annex VIII**.

Consultation summary/issues raised during consultations

Consultation with locals and stakeholders were carried out to gather information and interact local people to implementation process, procedure and community roles and responsibilities during and after construction as well as preparation phase. People in the project area have shown their commitment to supporting the project. There is no other objection except the completion of construction on time and maintaining specified quality.

During the consultation, peoples mainly emphasized on the quality of road and the material that will be used during the construction as well as proposed components of the roads, other concerns of locals are summarized below.

- Must be completed on time.
- Water must be sprayed during the construction phase
- Women empowerment must be taken into consideration.
- Women must be given priority for the work during the construction phase.
- Road safety must be taken seriously during construction phase.
- No any conflict/issue for the construction of the road.

Table 5-1 People consulted during field study

SN	Category	Male	Female	Total	%	Remarks
1	Brahman	63	22	85	19.91	
2	Kshetri	64	39	103	24.12	
3	Tharu	23	8	31	7.26	
4	Newa: (Newar)	43	9	52	12.18	
5	Rai	22	64	86	20.14	
6	Tamang	2	2	4	0.94	
7	Bishwokarma	5	10	15	3.51	
8	Sarki	1	5	6	1.41	
9	Lama	1	1	2	0.47	

SN	Category	Male	Female	Total	%	Remarks
10	Pariyar	3	0	3	0.70	
11	Giri	0	3	3	0.70	
12	Bhujel	1	0	1	0.23	
13	Majhi/Bote	14	5	19	4.45	
14	Thebe/Magar	2	11	13	3.04	
15	Other (Sada, Pamjiyar)	2	2	4	0.94	
Total		246	181	427	100	
%		57.6	42.4	100		

Source: field study 2023

Table 5-2: Details of participants during consultations

SN	Date	Name of the Road	Name	Organization	Consultation type	Remarks
1	2080-05-25	Mayor	Basanta Kumar Basnet	Triyuga Municipality	KII	
2	2080-05-24	Deputy Mayor	Maheshwori Rai	Triyuga Municipality	KII	
3	2080-05-20	Engineer	Nirmal Chaudhary	Triyuga Municipality	KII	
4	2080-05-24	W-11 Chair	Shumanta Koirala	Triyuga Municipality	KII	
5	2080-05-24	W-13 Chair	Nabin Kumar Chaudhary	Triyuga Municipality	KII	
6	2080-05-25	W-12 Chair (Acting)	Anita Thapa	Triyuga Municipality	KII	
7	2080-05-25	Ward Member, W12	Ramkali BK	Triyuga Municipality	KII	
8	2080-05-22	W-10 Chair	Kiran Panday	Triyuga Municipality	KII	
9	2080-05-24	Town Planning	Shahi kumar Tuladhar	Triyuga Municipality	KII	
10	2080-06-10	CAO, Triyuga Municipality	Bishnu Bhakta Sigdel	Triyuga Municipality	KII	
11	2080-05-24	Town planning	Shashi Kumar Tuladhar and other Himali tole members	Himali tole	FGD	
12	2080-06-11	Town planning	Ramesh Dahal and other tole members.	Jyotinagar tole bikas	FGD	
13	2080-05-11	City Hall Simaltar	Devi Prasad Khatiwoda and other tole members.	Shivalaya tole bikas Sastha, W-11	FGD	
14	2080-05-11	City Hall Simaltar	Nabin Karki and other tole members.	Indreshwor tole bikas Sastha, W-11	FGD	
15	2080-05-12	Babarani DM gate	Tek Bahadur Thapa and other tole members.	Babarani Tole bikas, Ward-11	FGD	
16	2080-05-07	Fulchowk	Tara Prasad Chaudhari and other tole members.	Fulchowk tole bikas, Ward-12	FGD	Chaudhary
17	2080-05-07	Fulchowk	Lawa Dhakal and other tole members.	Anamnagar tole bikas, Ward-12	FGD	

SN	Date	Name of the Road	Name	Organization	Consultation type	Remarks
18	2080-05-07	Fulchowk	Renu Kumar bote and other tole members.	Locals of Bote tole	FGD	Bote/Chaudhari
19	2080-05-09	Cityhall Ramchandra	Sabina Phuyal and other tole members.	Namuna Marga tole bikas sastha	FGD	
20	2080-06-08	Gaighat Shivalaya	Bhabindra Khatri and other tole members.	Ramailodanda tole bikas Sastha	FGD	
21	2080-06-08	Gaighat Shivalaya	Kalpna Khatri and other women members.	Ramailodanda tole (10 nos.)	FGD	Women Group
22	2080-05-24	Pragati tole	Basanta Kumar Basnet and other Municipality srepresentatives.	Triyuga Municipality	FGD	Municipality
23	2080-05-23	Pragati tole	Gita Rai and other tole members.	Chair, Sakela tole bikas Sastha, Ward-11	FGD	
24	2080-05-25	Pragati tole	Aasha Rai and other tole members.	Sakela Women Group, Ward-11	FGD	Women Group
25	2080-05-11	Pragati tole	Tilak Bahadur Katuwal and other tole members.	Janajyoti tole bikas Sastha, Ward-13	FGD	
26	2080-05-24	Town planning	Shashi Kumar Tuladhar	Himali tole/Locals	Meeting	
27	2080-06-11	Town planning	Bharat Prasad Chaudhary and other tole members.	Jyoti Nagar tole bikas	Meeting	
28	2080-05-11	City Hall Simaltar	Devi Prasad Khatiwoda and other tole members.	Shivalaya tole bikas Sastha, W-11	Meeting	
29	2080-05-12	City Hall Simaltar	Nabin Karki and other tole members.	Indreshwor tole bikas Sastha, W-11	Meeting	
30	2080-05-12	Babarani DM gate	Bishnu Shrestha with other tole members/ Locals.	Ward member/Locals, Ward-11	Meeting	

SN	Date	Name of the Road	Name	Organization	Consultation type	Remarks
31	2080-05-07	Fulchowk	Tara Prasad Chaudhari and other tole members.	Fulchowk tole bikas, Ward-12	Meeting	Chaudhary
32	2080-05-07	Fulchowk	Narman Bot and other tole members.	Ward 12 Chair and Locals	Meeting	Bote
33	2080-05-07	Fulchowk	Shankar Bahadur Kathayat and Locals	Locals, Ward 12	Meeting	
34	2080-05-12	Cityhall Ramchandra	Bishnu Shrestha and other local tole members	Locals, Ward-11	Meeting	
35	2080-06-08	Gaighat Shivalaya	Subindra BK and other tole members.	Ramailodanda tole bikas Sastha	Meeting	
36	2080-06-08	Gaighat Shivalaya	Kalpana Khatri	Ramailodanda tole	Meeting	Women Group
37	2080-05-23	Pragati tole	Gita Rai and other tole members.	Sakela tole bikas Sastha, Ward-11	Meeting	
38	2080-05-11	Pragati tole	Tilak Bahadur Katuwal and other tole members.	Pragati tole bikas Sastha	Meeting	
39	2080-06-12	Pragati tole	Kala Rai and other tole members.	Sakela tole	Meeting	Sakela tole Rai Community

5.2 Information Dissemination

All details regarding the suggested activities and their anticipated outcomes would be made available to the impacted people and other stakeholders. The project will make all necessary disclosures during each step of the project cycle in cooperation with the relevant municipal authorities, NGOs, and other community groups. In order to mutually identify relevant protective or corrective measures, agencies working for environmental and social elements will also be notified about the ongoing and planned activities. The following strategies will be used to ensure that information is available to all parties involved throughout the project cycle.

- Mass Media: Use local media like newspaper, radio and TV.
- Meeting/Workshops
- Distribution of project documents: Certain project documents will be disclosed in Nepali (or other relevant local language). Project-related information materials will be distributed prior to each construction work to local officials, local people, stakeholders and other concerned offices like municipality, Ward, Tole Committee etc.
- Publish ESIA/ESMP, information about grievance mechanism and other project related activities in the municipality's website.

An Information Centre will be established at the municipality office during implementation to disseminate all the documents related to the project activities. Based on the public information disclosure policy, PCO and the municipality will disclose the information through its website. The information dissemination plan for proposed Road sub project is presented in **Table 5-3**.

Table 5-3: Information Dissemination Plan

Means of Communication	Timeline & Frequency	Responsibility	Resources
Municipality Website (Project details, ESMP, GRM)	At the start of the project, & maintained throughout	PIU/ Information Officer	Information Officer
Newspaper and local Radio (project salient features, dates, GRM etc.)	Project implementation phase Weekly basis	PIU, municipality Information Officer	Radio-program, FM Radio Clip
Project leaflets and Fact Sheet,	Project details, Implementing agencies, project period - 2 times	PIU, Information Officer	Double-sided colour A4 (500 copies)
Face to face engagements - meetings, focus group discussion with relevant stakeholders	Project Main Activities, Financial Assistance, Implementing agencies, project period etc. 2 time in year	PIU, Information Officer	
Ward offices and Municipal social section/Library (Hard copy of ESMP)	Project period- 2 times	PIU/Municipality	Focal person

5.3 Public/Community Consultation Plan

During project implementation phase, all consultations on social and environmental issues will be conducted in a way that includes women and vulnerable social groups (such as low-income households,

members of certain castes, and people with disabilities, among others). Details of the Project Consultation Plan are presented in **Table 5-4**.

Table 5-4: Stakeholder consultation Plan

Objective and Target Goal	Method	Responsibility
I. Build Local Ownership		
Introduce Project DPR Report and its components	Group Meeting/Workshops	DSC/ Municipality
Maintain efforts for two-way communication with relevant stakeholders through the project	Face to face meeting with concerned stakeholders	DSC, Ward Level Representatives.
II. Start Consultation Process with Potentially Affected Communities by construction and operation of road		
Identify communities to be potential affected by project	Electronic and face to face communication with relevant stakeholders and implementing agencies	DSC, Municipality Ward Representatives
Consult with community representatives and ensure that their concerns with the proposed project are addressed	Face to face meeting with community representative Meeting will take place following protocol for meeting.	DSC, Municipality Ward Representatives
Ensure that the views and needs of vulnerable segment (if required) of communities, including but not limited to poor, women, elderly, and are addressed by the subproject	Face to face meeting with affected communities' representative (including social officer of Municipality, women's representative etc.)	DSC, Municipality Ward Representatives.
III. Implementation Phase		
Maintain effective communication with PIU	Electronic and face to face communication with representative of relevant agency /organization	PCO, Design and Supervision Consultant Municipality Ward Representatives
Raise awareness of project activities among potential beneficiaries	Media advertisements and targeted campaign	DSC/ Municipality
Maintain consultation process with a potential affected communities and beneficiaries	Face to face meeting with affected communities' representative (including social officer of Municipality, women's representative etc.)	Design and Supervision Consultant Municipality Ward Representatives
Monitoring and evaluation community involvement	Face to face meeting with affected communities' representative	Design and Supervision Consultant Municipality Ward Representatives
Reports outlining progress of activities related to engagement and communication	Collation of progress report, self-evaluation by PCO	PCO

Objective and Target Goal	Method	Responsibility
Agreement on operation and maintenance system	Electronic or face to face communication with relevant stakeholder Face to face meeting with local authority	Design and Supervision Consultant Municipality Ward Representatives
Implementation of ESMP	The contractor will prepare the various standalone plans to comply with ESMP requirements By including all the stand alone plans, the contractor will prepare Contractor’s Environmental and Social Management Plan (ESMP) and submit it to PIU. This requirement will be included in the contract BOQ	The contractor's bid document must include the requirements outlined in the ESMP. Before work starts, the contractor will draft the stand-alone plans, present them to the PIU, and get permission. The standalone plan comprises, among other things, a management plan for the environment, health, and safety; a strategy for traffic; a plan for handling complaints; a plan for managing spoils; a plan for disaster readiness; a plan for managing camps; a plan for managing labor; and a plan for managing air, water, and noise.

5.4 Grievance Redress Mechanism (GRM)

As part of the implementation stage the PIU, the project municipality, project engineers and environment and social staffs will directly interact and consult with the people of project alignment. The people of project alignment will be able to raise any grievances related to sub project with the subproject grievance redress mechanism (GRM), to help ensure the successful implementation of resettlement measures. At first instance, the project-affected grievant should raise their grievance with the information office of the project, and the information office will determine whether it can be resolved within the project, at the ward level, or whether another mechanism should be used. PAPs will be exempt from all administrative fees incurred, pursuant to the grievance redressed procedures except for cases filed in court. More details regarding the GRM are discussed in the following sections.

5.4.1 Structure of the GRC

Two-level Grievance Redress Committee (GRC) has been formed for the sub-project. The composition of the proposed GRC is given below. The GRC formation related municipality letter is attached in **Annex-II**.

a. The first level of GRC sits at the project site level. It is composed of the following team:

Chairperson of ward 11, Mr. Sumanta Koirala	– Coordinator
Ward Chairperson, ward-10, Mr. Kiran Pande	--Member
Acting Ward Chairperson, ward-12, MRs. Anita Thapa	--Member
Ward Chairperson, ward-13, Mr. Nabin Kumar Chaudhary	--Member
Ward member, ward-10, MRs Urmila BK	--Member
Ward member, ward-11, MRs Nila Kakri Dholi	--Member
Ward member, ward-12, MRs Ram kali Biswokarma	--Member

DSC (Supervision Engineer/Social/Environmental Safeguard Specialist)– Member Secretary

b. The second level GRC at the municipality level. It will comprise of the following team.

Mayor	– Coordinator
Deputy mayor	– Member
Chief, Administrative section	– Member
NUGIP focal person	– Member
Municipality’s Grievance Officer	– Member
Chief, Municipality’s Social Development section	– Member
DSC (Supervision Engineer/Social/Environmental Safeguard Specialist)–Member Secretary	

c. The third level GRC will be established at PCO Level.

5.4.2 Processes of the GRM

Grievances shall be submitted through various mediums, including in person, in written form to a noted address, through a toll-free phone line or through direct calls to concerned officials, and emails. The PCO will appoint a person (Operator) at PCO- Kathmandu to receive such calls and online messages. The person (Operator) based on nature of complaint, will forward the same to the information office or ward committee.

A ticket or a unique number will be generated for all such call, messages and letters. The complainant will follow up based that unique number with Operator at PCO-Kathmandu. All complaints will be responded within two weeks at any level. In case response is not received from 1st level within 15 days, the complaint will be escalated to next level. If complaint remains unaddressed at 1st and 2nd within maximum 30 days after registering the complaint, it will be elevated to 3rd level at PCO level. The PCO within 7 days of time should instruct the concerned person at Triyuga municipal level to arrange for a hearing within maximum 5 days of time. Effort will be given by all levels of GRCs to conduct hearing and resolve the concern at their level up to the satisfaction of complainant within the stipulated timeframe. In case 1st and 2nd level GRCs are unable to resolve the concern up to the satisfaction of complainant, these GRCs’ or Complainant may approach to 3rd level of GRC at PCO Level. After conducting hearing at any level of GRC, the decision will be communicated to complainant within maximum 30 Days of time.

All local contact information and options for complaint submission will be available on site, on Toles, Wards, municipality office, PCO on information boards and the project municipality websites. A half yearly report on Grievance Redress by the subproject project will be prepared and will be sent to the project municipality’s GRCs by Wards’ GRCs and ultimately to GRC of PCO. The PCO will forward the same to the World Bank.

The project GRM will include a process for addressing any SEA/SH-related grievances SEA/SH cases and to provide training/orientation on SEA/SH cases.

5.4.3 Further details of the GRM

The functions of grievance mechanism include redressing grievances of community / beneficiaries /project affected persons in all project respects, providing rehabilitation and resettlement assistance and related activities, and hearing grievances from workers involved in the project at any level or phase. The system

should be established to report back to the concerned community or persons regarding the decision on the complaint. The grievances related to women should be dealt by women officer. As required, the social mobilizers will be recruited. GRC will deal/hear the issues related to Environment, R&R and individual grievances and will give its decision/verdict within 30 days after hearing the aggrieved person. The final verdict of the GRC will be given by the Head of GRC in consultation with other members of the GRCs and will be binding to all other members. Potential grievances which may need to be addressed are listed below:

- Access to resource /utility/facility
- Ambient air and noise Quality
- Impact on water quality/resource
- Grievance from vulnerable community
- Gender related issues
- Grievances from workers
- Safety and risk repeated to project development

5.4.4 Other Mechanisms for Grievance Redress

All complainants have the option to approach court/judiciary or the World Bank's Grievance Redress Service in case he or she is not satisfied with the verdict provided.

6. LIST OF REFERENCES

- Environment Protection Act, Government of Nepal, 2019
- Environment Protection Regulations, Government of Nepal, 2020 (and amendments)
- Environmental and Social Management Framework, Nepal Urban Governance and Infrastructure Project, August 2020, the World Bank
- Final Detailed Project Report on Upgradation of Different inter connected Road (9.929Km) Sections, Triyuga Municipality Road, September 2023
- Nepal Human Rights Year Book 2023, Informal Sector Service Center (INSEC), Kathmandu.
- Project Implementation Manual, Nepal Urban Governance and Infrastructure Project, December 2022, the World Bank
- <https://censusnepal.cbs.gov.np>
- IEE Report of Triyuga, Baruwa River
- Municipality Profile of Triyuga Municipality 2019
- <https://Triyugamun.gov.np/>

7. LIST OF ANNEXES

Annex I: Environmental and Social Screening Checklists

Annex II: Minutes, Public Notice and Letters

Annex III: Proposed Typical Cross Sections

Annex IV: GoN Permissible Environmental limits/standards

Annex V: Air, Noise and Water Quality Test Report

Annex VI: List of Zebra crossings

Annex VII: Code of Conduct (CoC) on GBV

Annex VIII: Stakeholder Consultation Details

Annex IX: Existing electric pole details

Annex X: Photographs

Annex XI: Cadastral Maps of Alignment.

Annex XII: Alternative route.

Annex I: Environmental and Social Screening Checklists

Name of the Road: a) Gaighat town planning area Road Section

Table 7-1 Environment Screening Checklist for Gaighat town planning area Road

SN	Particulars	Yes	No	Can't Say	Remarks
1	Is the site vulnerable to major or induced hazards such as: Landslides, Flooding, Storm surge, Severe wind damage, Earthquakes, Fire, Explosion, Other (specify)	Yes			Possibility of Earthquake Natural Disaster, flooding
2	Is the project area adjacent to or within any of the following environmentally sensitive areas? <input type="checkbox"/> Cultural heritage site (historical, religious, traditional, or cultural significance) <input type="checkbox"/> Protected Area (National Parks, Wildlife Reserve, Hunting Reserve, Conservation Areas, and Buffer Zones etc.) <input type="checkbox"/> Wetland/Ramsar Site/Simsar <input type="checkbox"/> Forest <input type="checkbox"/> Special area for protecting biodiversity/interest <input type="checkbox"/> Breeding/nesting ground of wildlife/occurrence of migratory species <input type="checkbox"/> Migration route/Wildlife corridor <input type="checkbox"/> Any site of national or international importance		No		
3	Likely impact on trees (including Timber & fruit bearing) and vegetation cover		No		
4	Possibility of degradation of land and ecosystems of surrounding?				There is no any high slope having unstable surface and land slide prone areas. The road is lies in plain area. Thus, there is less chances to degradation of land and ecosystem.
5	Is the project area densely populated?	Yes			
6	Heavy with development activities/big industries nearby & type?		No		
7	Alteration of surface water hydrology of waterways due to the project resulting in increased sediment in streams affected by increased soil erosion at construction site?		No		
8	Chance of deterioration of surface water quality due to silt runoff and sanitary		No		

	wastes from worker-based camps and chemicals used in construction				
9	Does the sub project require significant extraction of surface or ground water?		No		
10	Increased risk of water pollution from oil, grease, fuel spills and other materials				Short term impact during Upgrading work.
11	Impact on water quality due to release of sewage/sludge?		No		Not connected to drainage system.
12	Possibility of flooding due to sewage		No		Not connected to drainage system.
13	Possibility of increased air pollution during preconstruction/ construction/ operation phase?				Short term impact during Upgrading work. ESMP measures applicable.
14	Other pollution concerns relating to inconveniences in living conditions that may trigger cases of upper respiratory problems?				Short term impact during Upgrading work.
15	Risks and vulnerabilities related to occupational health and safety due to physical, chemical, biological hazards during project construction and operation	Yes			Physical hazards like accidents and illness are likely
16	Noise and vibration due to blasting and/or other civil works?	Yes			Short term impact during Upgrading work.
17	Possibility of poor sanitation and solid waste disposal	Yes			Campsite management aspect, ESMP measures applicable
18	Creation of temporary breeding habitats for diseases such as those transmitted by mosquitoes and rodents?		No		
19	Accident risks associated with pre construction, construction & operation phases of project				Short term impact during Upgrading work.
20	Large population influx during project construction and operation that causes increased burden on social infrastructure and services (such as water supply and sanitation systems)				Suggested to use local worker during construction but Influx of people is anticipated during the time of Upgrading. Pressure upon existing natural resources such as water, existing food availability, existing educational facilities are likely to be affected with the increase in the number of outside workers during Upgrading.
21	Risks to community health and safety due to the transport, storage, and use and/or disposal of materials such as explosives, fuel and other chemicals during construction and operation?				Short term impact during Upgrading work.
22	interference with other utilities and blocking of access to resource/utility	Yes			Short term impact during Upgrading work.

23	Generation of solid waste and/or hazardous waste during construction/operation of project?				Short term impact during Upgrading work.
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Table 7-2 Checklist Social Screening of Gaighat town planning area Road

SN	Particulars		Remarks
1	Proposed Site Location		
	a	Land requirement for the project	It is an up-gradation of an existing road. There will be no additional land requirement.
	b	Landownership of the project area: Govt. / Private lands	Land within the proposed road width is already in use by the public. As per discussion with locals and municipality, there is no any outstanding issues related to grievance, compensation etc
	c	Is the project requiring acquisition of Govt. land/structures? If yes please mention the area of land, number of affected structures, Households	No
	d	Present use of Govt. Land that will be used for the project activities with Persons/Households using	No
	e	Is the project requiring acquisition of private land/structures? If yes please mention the area of land, number of affected structures, Households	No
	f	Present use of Govt. Land that will be used for the project activities with Persons/Households using <input type="checkbox"/> Agricultural purposes <input type="checkbox"/> Residential purposes <input type="checkbox"/> Commercial purposes <input type="checkbox"/> Other purposes (Indicate)	Public use
	g	Is the project requires relocation of encroachers/squatters If yes please elaborate number and nature	No
	h	Is the project requires relocation of community facilities/Govt. establishment or any object that are of religious, cultural and historical significance	No
	i	Proposed project located in an area where residents are- <input type="checkbox"/> All Mainstream <input type="checkbox"/> All Indigenous peoples <input type="checkbox"/> Majority Mainstream or Non-indigenous peoples <input type="checkbox"/> Majority Indigenous peoples	Majority are Mainstream
2	Potential Social Impacts- Will the Project cause		
	a	Involuntary resettlement of people? (Physical displacement and/or economic displacement)	No, resettlement is not required in this project.
	b	Impacts on the poor, women and children, Indigenous Peoples or other vulnerable groups?	There is no adverse impact to woman headed households in the project area as no land acquisition is anticipated for proposed project
	c	Will community facilities require relocation?	Yes,

SN	Particulars		Remarks
			Triyuga Small town water supply pipeline and electric poles likely to affected.
	d	Will the sub-project disturb any traditional activity on adjoining or nearby?	No
	e	poor sanitation and solid waste disposal in construction camps and work sites	Suggested to use local worker during construction but Influx of people is anticipated during the time of Upgrading. Pressure upon existing natural resources such as water, existing food availability, solid waste patterns are likely to be affected with the increase in the number of outside workers during Upgrading.
	f	Possible transmission of communicable diseases (such as STI's and HIV/AIDS) from workers to local populations?	May have chances to transmission of diseases.
	g	Large population influx during project construction and operation that causes increased burden on social infrastructure and services (such as water supply and sanitation systems)?	Yes
	h	Social conflicts relating to inconveniences in living conditions where construction interferes with preexisting roads	Yes, change in road morphology and disruption in the infrastructure like drinking water, electric lines will cause inconvenience. Traffic management plan will be prepared by the contractor and will be included in Contractor's Site specific ESMP.
	i	Describe any other impacts that have not been covered in this screening form	No permanent structure lies within the ROW of indigenous people, hence there is no adverse impact on indigenous group
	j	Describe alternatives, if any, to avoid or minimize displacement from private and public lands	No such concerns
	k	RAP/ARAP Requirement	Not required

Name of the Road: **b) City Hall Simaltar Gaighat Diktel Road Section**

Table 7-3: Environment Screening Checklist for City Hall Simaltar Gaighat Diktel road section

SN	Particulars	Yes	No	Can't Say	Remarks
1	Is the site vulnerable to major or induced hazards such as: Landslides, Flooding, Storm surge, Severe wind damage, Earthquakes, Fire, Explosion, Other (specify)	Yes			Possibility of Earthquake Natural Disaster, flooding.
2	Is the project area adjacent to or within any of the following environmentally sensitive areas? <input type="checkbox"/> <input type="checkbox"/> Cultural heritage site (historical, religious, traditional, or cultural significance) <input type="checkbox"/> <input type="checkbox"/> Protected Area (National Parks, Wildlife Reserve, Hunting Reserve, Conservation Areas, and Buffer Zones etc.) <input type="checkbox"/> <input type="checkbox"/> Wetland/Ramsar Site/Simsar <input type="checkbox"/> <input type="checkbox"/> Forest <input type="checkbox"/> <input type="checkbox"/> Special area for protecting biodiversity/interest <input type="checkbox"/> <input type="checkbox"/> Breeding/nesting ground of wildlife/occurrence of migratory species <input type="checkbox"/> <input type="checkbox"/> Migration route/Wildlife corridor <input type="checkbox"/> <input type="checkbox"/> Any site of national or international importance		No		The project RoW is devoid of such archaeologically and culturally significant areas and national parks.
3	Likely impact on trees (including Timber & fruit bearing) and vegetation cover		No		
4	Possibility of degradation of land and ecosystems of surrounding?				There is no high slope having unstable surface and land slide prone areas. The road lies in plain area. Thus, there is less chances to degradation of land and ecosystem.
5	Is the project area densely populated?			Partial	
6	Heavy with development activities/big industries nearby & type?		No		
7	Alteration of surface water hydrology of waterways due to the project resulting in increased sediment in streams affected by increased soil erosion at construction site?		No		
8	Chance of deterioration of surface water quality due to silt runoff and sanitary wastes from worker-based camps and chemicals used in construction		No		

SN	Particulars	Yes	No	Can't Say	Remarks
9	Does the sub project require significant extraction of surface or ground water?		No		
10	Increased risk of water pollution from oil, grease, fuel spills and other materials				Short term impact during Upgrading work.
11	Impact on water quality due to release of sewage/sludge?		No		Not connected to drainage system.
12	Possibility of flooding due to sewage		No		Not connected to drainage system.
13	Possibility of increased air pollution during preconstruction/ construction/operation phase?				Short term impact during Upgrading work.
14	Other pollution concerns relating to inconveniences in living conditions that may trigger cases of upper respiratory problems?				Short term impact during Upgrading work.
15	Risks and vulnerabilities related to occupational health and safety due to physical, chemical, biological hazards during project construction and operation				Short term impact during Upgrading work.
16	Noise and vibration due to blasting and/or other civil works?				Short term impact during Upgrading work.
17	Possibility of poor sanitation and solid waste disposal		No		
18	Creation of temporary breeding habitats for diseases such as those transmitted by mosquitoes and rodents?		No		
19	Accident risks associated with pre construction, construction & operation phases of project				Short term impact during Upgrading work.
20	Large population influx during project construction and operation that causes increased burden on social infrastructure and services (such as water supply and sanitation systems)			Partial	Suggested to use local worker during construction but Influx of people is anticipated during the time of Upgrading. Pressure upon existing natural resources such as water, existing food availability, existing educational facilities are likely to be affected with the increase in the number of outside workers during Upgrading.
21	Risks to community health and safety due to the transport, storage, and use and/or disposal of materials such as explosives, fuel and other chemicals during construction and operation?				Short term impact during Upgrading work.
22	interference with other utilities and blocking of access to resource/utility				Short term impact during Upgrading work.

SN	Particulars	Yes	No	Can't Say	Remarks
23	Generation of solid waste and/or hazardous waste during construction/operation of project?				Short term impact during Upgrading work.

Table 7-4: Social Screening Checklist for City Hall Simaltar Gaihat Diktel road section

SN	Particulars		Remarks
1	Proposed Site Location		
	a	Land requirement for the project	It is an up-gradation of an existing road. There will be no additional land requirement. Due to the narrow sections i.e from 2+820 to 2+860 the construction width of proposed road is 10.65 m.
	b	Landownership of the project area: Govt. / Private lands	Land within the proposed road width is already in use by the public. As per discussion with locals and municipality, there is no any outstanding issues related to grievance, compensation etc.
	c	Is the project requires acquisition of Govt. land/structures? If yes please mention the area of land, number of affected structures, Households	No
	d	Present use of Govt. Land that will be used for the project activities with Persons/Households using	No
	e	Is the project requires acquisition of private land/structures? If yes please mention the area of land, number of affected structures, Households	No
	f	Present use of Govt. Land that will be used for the project activities with Persons/Households using <input type="checkbox"/> Agricultural purposes <input type="checkbox"/> Residential purposes <input type="checkbox"/> Commercial purposes <input type="checkbox"/> Other purposes (Indicate)	Public use
	g	Is the project requires relocation of encroachers/squatters If yes please elaborate number and nature	No
	h	Is the project requires relocation of community facilities/Govt. establishment or any object that are of religious, cultural and historical significance	No
	i	Proposed project located in an area where residents are- <input type="checkbox"/> All Mainstream <input type="checkbox"/> All Indigenous peoples <input type="checkbox"/> Majority Mainstream or Non-indigenous peoples <input type="checkbox"/> Majority Indigenous peoples	Majority are Mainstream
2	Potential Social Impacts- Will the Project cause		
	a	Involuntary resettlement of people? (Physical displacement and/or economic displacement)	No, resettlement is not required in this project.

SN	Particulars		Remarks
	b	Impacts on the poor, women and children, Indigenous Peoples or other vulnerable groups?	No such impact on poor women and children, indigenous people, and/or economic displacement.
	c	Will community facilities require relocation?	Yes, Triyuga Small town water supply pipeline and electric poles are likely to affected.
	d	Will the sub-project disturb any traditional activity on adjoining or nearby?	No
	e	poor sanitation and solid waste disposal in construction camps and work sites	Yes, there will be concern of sanitation and solid waste disposal in construction camp and work sites.
	f	Possible transmission of communicable diseases (such as STI's and HIV/AIDS) from workers to local populations?	May have chances to transmission of diseases.
	g	Large population influx during project construction and operation that causes increased burden on social infrastructure and services (such as water supply and sanitation systems)?	Yes
	h	Social conflicts relating to inconveniences in living conditions where construction interferes with preexisting roads	The project area during the project construction works, other activities except commercial activities will be not affected due to excavation and construction activities. While, in the implementation phase the commercial activities will be enhanced and further increased in terms of numbers
	i	Describe any other impacts that have not been covered in this screening form	No permanent structure lies within the construction width of indigenous people, hence there is no adverse impact on indigenous group
	j	Describe alternatives, if any, to avoid or minimize displacement from private and public lands	No displacement of private and public land is required.
	k	RAP/ARAP Requirement	Not required

Name of the Road: c) **Babarani DM Gate Road Section**

Table 7-5: Environment Screening Checklist Babarani DM Gate Road Section

SN	Particulars	Yes	No	Can't Say	Remarks
1	Is the site vulnerable to major or induced hazards such as: Landslides, Flooding, Storm surge, Severe wind damage, Earthquakes, Fire, Explosion, Other (specify)		No		Possibility of Earthquake Natural Disaster, flooding.

SN	Particulars	Yes	No	Can't Say	Remarks
2	<p>Is the project area adjacent to or within any of the following environmentally sensitive areas?</p> <p><input type="checkbox"/> <input type="checkbox"/> Cultural heritage site (historical, religious, traditional, or cultural significance)</p> <p><input type="checkbox"/> <input type="checkbox"/> Protected Area (National Parks, Wildlife Reserve, Hunting Reserve, Conservation Areas, and Buffer Zones etc.)</p> <p><input type="checkbox"/> <input type="checkbox"/> Wetland/Ramsar Site/Simsar</p> <p><input type="checkbox"/> <input type="checkbox"/> Forest</p> <p><input type="checkbox"/> <input type="checkbox"/> Special area for protecting biodiversity/interest</p> <p><input type="checkbox"/> <input type="checkbox"/> Breeding/nesting ground of wildlife/occurrence of migratory species</p> <p><input type="checkbox"/> <input type="checkbox"/> Migration route/Wildlife corridor</p> <p><input type="checkbox"/> <input type="checkbox"/> Any site of national or international importance</p>		No		The project RoW is devoid of such archaeologically and culturally significant areas and national parks.
3	Likely impact on trees (including Timber & fruit bearing) and vegetation cover		No		
4	Possibility of degradation of land and ecosystems of surrounding?				There is no any high slope having unstable surface and land slide prone areas. The road is lies in plain area. Thus, there is less chances to degradation of land and ecosystem.
5	Is the project area densely populated?			Partial	
6	Heavy with development activities/big industries nearby & type?		No		
7	Alteration of surface water hydrology of waterways due to the project resulting in increased sediment in streams affected by increased soil erosion at construction site?		No		
8	Chance of deterioration of surface water quality due to silt runoff and sanitary wastes from worker-based camps and chemicals used in construction		No		
9	Does the sub project require significant extraction of surface or ground water?		No		
10	Increased risk of water pollution from oil, grease, fuel spills and other materials				Short term impact during Upgrading work.
11	Impact on water quality due to release of sewage/sludge?		No		Not connected to drainage system.
12	Possibility of flooding due to sewage		No		Not connected to drainage system.

SN	Particulars	Yes	No	Can't Say	Remarks
13	Possibility of increased air pollution during preconstruction/construction/operation phase?				Short term impact during Upgrading work.
14	Other pollution concerns relating to inconveniences in living conditions that may trigger cases of upper respiratory problems?				Short term impact during Upgrading work.
15	Risks and vulnerabilities related to occupational health and safety due to physical, chemical, biological hazards during project construction and operation				Short term impact during Upgrading work.
16	Noise and vibration due to blasting and/or other civil works?				Short term impact during Upgrading work.
17	Possibility of poor sanitation and solid waste disposal		No		
18	Creation of temporary breeding habitats for diseases such as those transmitted by mosquitoes and rodents?		No		
19	Accident risks associated with pre construction, construction & operation phases of project				Short term impact during Upgrading work.
20	Large population influx during project construction and operation that causes increased burden on social infrastructure and services (such as water supply and sanitation systems)				Suggested to use local worker during construction but Influx of people is anticipated during the time of Upgrading. Pressure upon existing natural resources such as water, existing food availability, existing educational facilities are likely to be affected with the increase in the number of outside workers during Upgrading.
21	Risks to community health and safety due to the transport, storage, and use and/or disposal of materials such as explosives, fuel and other chemicals during construction and operation?				Short term impact during Upgrading work.
22	interference with other utilities and blocking of access to resource/utility				Short term impact during Upgrading work.
23	Generation of solid waste and/or hazardous waste during construction/operation of project?				Short term impact during Upgrading work.

Table 7-6: Social Screening Checklist Babarani DM Gate Road Section

SN	Particulars	Remarks
1	Proposed Site Location	

SN	Particulars		Remarks
	a	Land requirement for the project	Land within the RoW is already in use by the public. It is the upgradation existing road. There will be no additional land requirement. Due to the narrow sections i.e from 0+260 to 0+420 and from 0+520 to 0+610 the construction width of the proposed road is 6.3 m and 6.75 m respectively.
	b	Landownership of the project area: Govt. / Private lands	Land within the proposed road width is already in use by the public. As per discussion with locals and municipality, there is no any outstanding issues related to grievance, compensation etc.
	c	Is the project requires acquisition of Govt. land/structures? If yes please mention the area of land, number of affected structures, Households	No
	d	Present use of Govt. Land that will be used for the project activities with Persons/Households using	No
	e	Is the project requires acquisition of private land/structures? If yes please mention the area of land, number of affected structures, Households	No
	f	Present use of Govt. Land that will be used for the project activities with Persons/Households using <input type="checkbox"/> <input type="checkbox"/> Agricultural purposes <input type="checkbox"/> <input type="checkbox"/> Residential purposes <input type="checkbox"/> <input type="checkbox"/> Commercial purposes <input type="checkbox"/> <input type="checkbox"/> Other purposes (Indicate)	Public use
	g	Is the project requires relocation of encroachers/squatters If yes please elaborate number and nature	No
	h	Is the project requires relocation of community facilities/Govt. establishment or any object that are of religious, cultural and historical significance	No
	i	Proposed project located in an area where residents are- <input type="checkbox"/> <input type="checkbox"/> All Mainstream <input type="checkbox"/> <input type="checkbox"/> All Indigenous peoples <input type="checkbox"/> <input type="checkbox"/> Majority Mainstream or Non-indigenous peoples <input type="checkbox"/> <input type="checkbox"/> Majority Indigenous peoples	Majority are Mainstream
2	Potential Social Impacts- Will the Project cause		
	a	Involuntary resettlement of people? (Physical displacement and/or economic displacement)	No, resettlement is not required in this project.
	b	Impacts on the poor, women and children, Indigenous Peoples or other vulnerable groups?	No such impact on poor women and children, indigenous people, and/or economic displacement.
	c	Will community facilities require relocation?	No
	d	Will the sub-project disturb any traditional activity on adjoining or nearby?	Yes,

SN	Particulars		Remarks
			Triyuga Small town water supply pipeline and electric poles likely to affected
	e	poor sanitation and solid waste disposal in construction camps and work sites	Suggested to use local worker during construction but Influx of people is anticipated during the time of Upgrading. Pressure upon existing natural resources such as water, existing food availability, solid waste patterns are likely to be affected with the increase in the number of outside workers during Upgrading.
	f	Possible transmission of communicable diseases (such as STI's and HIV/AIDS) from workers to local populations?	May have chances to transmission of diseases.
	g	Population influx during project construction and operation that causes increased burden on social infrastructure and services (such as water supply and sanitation systems)?	Yes
	h	Social conflicts relating to inconveniences in living conditions where construction interferes with preexisting roads	The project area during the project construction works, other activities except commercial activities will be not affected due to excavation and construction activities. While, in the implementation phase the commercial activities will be enhanced and further increased in terms of numbers
	i	Describe any other impacts that have not been covered in this screening form	No permanent structure lies within the ROW of indigenous people, hence there is no adverse impact on indigenous group
	j	Describe alternatives, if any, to avoid or minimize displacement from private and public lands	No displacement of private and public land is required.
	k	RAP/ARAP Requirement	Not required

Name of the Road: **d) Fulchowk PG Chowk Road Botetole Triyuga Corridor section**

Table 7-7: Environmental Screening Checklist for Fulchowk PG Chowk Road Botetole Triyuga Corridor Section

SN	Particulars	Yes	No	Can't Say	Remarks
1	Is the site vulnerable to major or induced hazards such as: Landslides, Flooding, Storm surge, Severe wind damage, Earthquakes, Fire, Explosion, Other (specify)		No		Possibility of Earthquake Natural Disaster, flooding

SN	Particulars	Yes	No	Can't Say	Remarks
2	Is the project area adjacent to or within any of the following environmentally sensitive areas? <input type="checkbox"/> <input type="checkbox"/> Cultural heritage site (historical, religious, traditional, or cultural significance) <input type="checkbox"/> <input type="checkbox"/> Protected Area (National Parks, Wildlife Reserve, Hunting Reserve, Conservation Areas, and Buffer Zones etc.) <input type="checkbox"/> <input type="checkbox"/> Wetland/Ramsar Site/Simsar <input type="checkbox"/> <input type="checkbox"/> Forest <input type="checkbox"/> <input type="checkbox"/> Special area for protecting biodiversity/interest <input type="checkbox"/> <input type="checkbox"/> Breeding/nesting ground of wildlife/occurrence of migratory species <input type="checkbox"/> <input type="checkbox"/> Migration route/Wildlife corridor <input type="checkbox"/> <input type="checkbox"/> Any site of national or international importance		No		
3	Likely impact on trees (including Timber & fruit bearing) and vegetation cover		No		
4	Possibility of degradation of land and ecosystems of surrounding?				There is no any high slope having unstable surface and land slide prone areas. The road is lies in plain area. Thus, there is less chances to degradation of land and ecosystem. However, during DPR stage it will be considered.
5	Is the project area densely populated?			Partial	
6	Heavy with development activities/big industries nearby & type?		No		
7	Alteration of surface water hydrology of waterways due to the project resulting in increased sediment in streams affected by increased soil erosion at construction site?		No		
8	Chance of deterioration of surface water quality due to silt runoff and sanitary wastes from worker-based camps and chemicals used in construction	Yes			ESMP measures applicable.
9	Does the sub project require significant extraction of surface or ground water?		No		
10	Increased risk of water pollution from oil, grease, fuel spills and other materials				Short term impact during Upgrading work.
11	Impact on water quality due to release of sewage/sludge?		No		Not connected to drainage system.
12	Possibility of flooding due to sewage		No		Not connected to drainage system.

SN	Particulars	Yes	No	Can't Say	Remarks
13	Possibility of increased air pollution during preconstruction/construction/operation phase?	Yes			Short term impact during Upgrading work.
14	Other pollution concerns relating to inconveniences in living conditions that may trigger cases of upper respiratory problems?				Short term impact during Upgrading work.
15	Risks and vulnerabilities related to occupational health and safety due to physical, chemical, biological hazards during project construction and operation				Short term impact during Upgrading work.
16	Noise and vibration due to blasting and/or other civil works?				Short term impact during Upgrading work.
17	Possibility of poor sanitation and solid waste disposal		No		
18	Creation of temporary breeding habitats for diseases such as those transmitted by mosquitoes and rodents?		No		
19	Accident risks associated with pre construction, construction & operation phases of project				Short term impact during Upgrading work.
20	Large population influx during project construction and operation that causes increased burden on social infrastructure and services (such as water supply and sanitation systems)				Suggested to use local worker during construction but Influx of people is anticipated during the time of Upgrading. Pressure upon existing natural resources such as water, existing food availability, existing educational facilities are likely to be affected with the increase in the number of outside workers during Upgrading.
21	Risks to community health and safety due to the transport, storage, and use and/or disposal of materials such as explosives, fuel and other chemicals during construction and operation?				Short term impact during Upgrading work.
22	interference with other utilities and blocking of access to resource/utility				Short term impact during Upgrading work.
23	Generation of solid waste and/or hazardous waste during construction/operation of project?				Short term impact during Upgrading work.

Table 7-8: Social Screening Checklist for Fulchowk PG Chowk Road Botetole Triyuga Corridor Section

SN	Particulars	Remarks
1	Proposed Site Location	

SN	Particulars		Remarks
	a	Land requirement for the project	Land within the RoW is already in use by the public. It is an upgradation of an existing road. There will be no additional land requirement.
	b	Landownership of the project area: Govt. / Private lands	Land within the proposed road width is already in use by the public. As per discussion with locals and municipality, there is no any outstanding issues related to grievance, compensation etc
	c	Is the project requires acquisition of Govt. land/structures? If yes please mention the area of land, number of affected structures, Households	No
	d	Present use of Govt. Land that will be used for the project activities with Persons/Households using	No
	e	Is the project requires acquisition of private land/structures? If yes please mention the area of land, number of affected structures, Households	No
	f	Present use of Govt. Land that will be used for the project activities with Persons/Households using <input type="checkbox"/> Agricultural purposes <input type="checkbox"/> Residential purposes <input type="checkbox"/> Commercial purposes <input type="checkbox"/> Other purposes (Indicate)	Public use
	g	Is the project requires relocation of encroachers/squatters If yes please elaborate number and nature	No
	h	Is the project requires relocation of community facilities/Govt. establishment or any object that are of religious, cultural and historical significance	No
	i	Proposed project located in an area where residents are- <input type="checkbox"/> All Mainstream <input type="checkbox"/> All Indigenous peoples <input type="checkbox"/> Majority Mainstream or Non-indigenous peoples <input type="checkbox"/> Majority Indigenous peoples	Majority Indigenous peoples
2	Potential Social Impacts- Will the Project cause		
	a	Involuntary resettlement of people? (Physical displacement and/or economic displacement)	No, resettlement is not required in this project
	b	Impacts on the poor, women and children, Indigenous Peoples or other vulnerable groups?	No such impact on poor women and children, indigenous people, and/or economic displacement.
	c	Will community facilities require relocation?	Yes, Triyuga Small town water supply pipeline and electric poles likely to affected
	d	Will the sub-project disturb any traditional activity on adjoining or nearby?	No
	e	poor sanitation and solid waste disposal in construction camps and work sites	Suggested to use local worker during construction but Influx of

SN	Particulars		Remarks
			people is anticipated during the time of Upgrading. Pressure upon existing natural resources such as water, existing food availability, solid waste patterns are likely to be affected with the increase in the number of outside workers during Upgrading.
	f	Possible transmission of communicable diseases (such as STI's and HIV/AIDS) from workers to local populations?	May have chances to transmission of diseases.
	g	Large population influx during project construction and operation that causes increased burden on social infrastructure and services (such as water supply and sanitation systems)?	Pressure upon existing water supply system, food availability, are likely to be affected with the increase in the number of outside workers
	h	Social conflicts relating to inconveniences in living conditions where construction interferes with preexisting roads	The project area during the project construction works, other activities except commercial activities will be not affected due to excavation and construction activities. While, in the implementation phase the commercial activities will be enhanced and further increased in terms of numbers
	i	Describe any other impacts that have not been covered in this screening form	No permanent structure lies within the ROW of indigenous people, hence there is no adverse impact on indigenous group
	j	Describe alternatives, if any, to avoid or minimize displacement from private and public lands	No displacement of private and public land is required.
	k	RAP/ARAP Requirement	Not required

Name of the Road: e) City Hall Ramchandra Basnet house Road Section

Table 7-9: Environmental Screening Checklist for City Hall Ramchandra Basnet house Road Section

SN	Particulars	Yes	No	Can't Say	Remarks
1	Is the site vulnerable to major or induced hazards such as: Landslides, Flooding, Storm surge, Severe wind damage, Earthquakes, Fire, Explosion, Other (specify)	Yes			Possibility of Earthquake Natural Disaster, flooding
2	Is the project area adjacent to or within any of the following environmentally sensitive areas? <input type="checkbox"/> <input type="checkbox"/> Cultural heritage site (historical, religious, traditional, or cultural significance)		No		The project RoW is devoid of such archaeologically and culturally significant areas and national parks

SN	Particulars	Yes	No	Can't Say	Remarks
	<input type="checkbox"/> Protected Area (National Parks, Wildlife Reserve, Hunting Reserve, Conservation Areas, and Buffer Zones etc.) <input type="checkbox"/> Wetland/Ramsar Site/Simsar <input type="checkbox"/> Forest <input type="checkbox"/> Special area for protecting biodiversity/interest <input type="checkbox"/> Breeding/nesting ground of wildlife/occurrence of migratory species <input type="checkbox"/> Migration route/Wildlife corridor <input type="checkbox"/> Any site of national or international importance				
3	Likely impact on trees (including Timber & fruit bearing) and vegetation cover		No		
4	Possibility of degradation of land and ecosystems of surrounding?				There is no any high slope having unstable surface and land slide prone areas. The road lies in plain area. Thus, there is less chances to degradation of land and ecosystem. However, during DPR stage it will be considered.
5	Is the project area densely populated?				
6	Heavy with development activities/big industries nearby & type?		No		
7	Alteration of surface water hydrology of waterways due to the project resulting in increased sediment in streams affected by increased soil erosion at construction site?		No		
8	Chance of deterioration of surface water quality due to silt runoff and sanitary wastes from worker-based camps and chemicals used in construction	Yes			ESMP measures applicable.
9	Does the sub project require significant extraction of surface or ground water?		No		
10	Increased risk of water pollution from oil, grease, fuel spills and other materials				Short term impact during Upgrading work.
11	Impact on water quality due to release of sewage/sludge?		No		Not connected to drainage system.
12	Possibility of flooding due to sewage		No		Not connected to drainage system.
13	Possibility of increased air pollution during preconstruction/construction/operation phase?				Short term impact during Upgrading work.
14	Other pollution concerns relating to inconveniences in living conditions that		No		

SN	Particulars	Yes	No	Can't Say	Remarks
	may trigger cases of upper respiratory problems?				
15	Risks and vulnerabilities related to occupational health and safety due to physical, chemical, biological hazards during project construction and operation	Yes			Physical hazards like accidents and illness are likely
16	Noise and vibration due to blasting and/or other civil works?	Yes			ESMP measures applicable
17	Possibility of poor sanitation and solid waste disposal	Yes			Campsite management aspect, ESMP measures applicable
18	Creation of temporary breeding habitats for diseases such as those transmitted by mosquitoes and rodents?		No		
19	Accident risks associated with pre construction, construction & operation phases of project	Yes			Injuries during construction phase, and traffic accidents during operation phase are potential risks
20	Population influx during project construction and operation that causes increased burden on social infrastructure and services (such as water supply and sanitation systems)				Suggested to use local worker during construction but Influx of people is anticipated during the time of Upgrading. Pressure upon existing natural resources such as water, existing food availability, existing educational facilities are likely to be affected with the increase in the number of outside workers during Upgrading.
21	Risks to community health and safety due to the transport, storage, and use and/or disposal of materials such as explosives, fuel and other chemicals during construction and operation?				Short term impact during Upgrading work.
22	interference with other utilities and blocking of access to resource/utility				Short term impact during Upgrading work.
23	Generation of solid waste and/or hazardous waste during construction/operation of project?				Short term impact during Upgrading work.

Table 7-10: Social Screening Checklist for City Hall Ramchandra Basnet house Road Section

SN	Particulars	Remarks
1	Proposed Site Location	
a	Land requirement for the project	Land within the RoW is already in use by the public. It is an upgradation of an existing road. There will be no additional land requirement. Due to the narrow sections, from 0+160 to 0+240 the

SN	Particulars		Remarks
			construction width of the proposed road is 6.3 m.
	b	Landownership of the project area: Govt. / Private lands	Land within the proposed road width is already in use by the public. As per discussion with locals and municipality, there is no any outstanding issues related to grievance, compensation etc
	c	Is the project requires acquisition of Govt. land/structures? If yes please mention the area of land, number of affected structures, Households	No
	d	Present use of Govt. Land that will be used for the project activities with Persons/Households using	No
	e	Is the project requires acquisition of private land/structures? If yes please mention the area of land, number of affected structures, Households	No.
	f	Present use of Govt. Land that will be used for the project activities with Persons/Households using <input type="checkbox"/> <input type="checkbox"/> Agricultural purposes <input type="checkbox"/> <input type="checkbox"/> Residential purposes <input type="checkbox"/> <input type="checkbox"/> Commercial purposes <input type="checkbox"/> <input type="checkbox"/> Other purposes (Indicate)	Public use.
	g	Is the project requires relocation of encroachers/squatters If yes please elaborate number and nature	No
	h	Is the project requires relocation of community facilities/Govt. establishment or any object that are of religious, cultural and historical significance	No
	i	Proposed project located in an area where residents are- <input type="checkbox"/> <input type="checkbox"/> All Mainstream <input type="checkbox"/> <input type="checkbox"/> All Indigenous peoples <input type="checkbox"/> <input type="checkbox"/> Majority Mainstream or Non-indigenous peoples <input type="checkbox"/> <input type="checkbox"/> Majority Indigenous peoples	Majority are Mainstream
2	Potential Social Impacts- Will the Project cause		
	a	Involuntary resettlement of people? (Physical displacement and/or economic displacement)	No, resettlement is not required in this project
	b	Impacts on the poor, women and children, Indigenous Peoples or other vulnerable groups?	No such impact on poor women and children, indigenous people, and/or economic displacement.
	c	Will community facilities require relocation?	Yes, Triyuga small town water supply pipeline and electric poles likely to affected.
	d	Will the sub-project disturb any traditional activity on adjoining or nearby?	No
	e	poor sanitation and solid waste disposal in construction camps and work sites	Suggested to use local worker during construction but Influx of people is anticipated during the time of Upgrading. Pressure upon existing natural resources such as

SN	Particulars		Remarks
			water, existing food availability, solid waste patterns are likely to be affected with the increase in the number of outside workers during Upgrading.
f	Possible transmission of communicable diseases (such as STI's and HIV/AIDS) from workers to local populations?		May have chances to transmission of diseases.
g	Large population influx during project construction and operation that causes increased burden on social infrastructure and services (such as water supply and sanitation systems)?		Pressure upon existing water supply system, food availability, are likely to be affected with the increase in the number of outside workers
h	Social conflicts relating to inconveniences in living conditions where construction interferes with preexisting roads		The project area during the project construction works, other activities except commercial activities will be not affected due to excavation and construction activities. While, in the implementation phase the commercial activities will be enhanced and further increased in terms of numbers
i	Describe any other impacts that have not been covered in this screening form		No permanent structure lies within the ROW of indigenous people, hence there is no adverse impact on indigenous group
j	Describe alternatives, if any, to avoid or minimize displacement from private and public lands		No displacement of private and public land is required.
k	RAP/ARAP Requirement		Not required

Name of the Road: f) **Gaighat Shivalay tole-Adarsha tole-Ramailo danda Manmohan school to Karamgachhi Diktel connecting Road Section**

Table 7-11: Environmental Screening Checklist for Gaighat Shivalay tole-Adarsha tole-ramailo danda manmohan school to Karamgachhi diktel connecting Road

SN	Particulars	Yes	No	Can't Say	Remarks
1	Is the site vulnerable to major or induced hazards such as: Landslides, Flooding, Storm surge, Severe wind damage, Earthquakes, Fire, Explosion, Other (specify)	Yes			Possibility of Earthquake Natural Disaster, flooding
2	Is the project area adjacent to or within any of the following environmentally sensitive areas? <input type="checkbox"/> Cultural heritage site (historical, religious, traditional, or cultural significance)		No		The project RoW is devoid of such archaeologically and culturally significant areas and national parks

SN	Particulars	Yes	No	Can't Say	Remarks
	<input type="checkbox"/> Protected Area (National Parks, Wildlife Reserve, Hunting Reserve, Conservation Areas, and Buffer Zones etc.) <input type="checkbox"/> Wetland/Ramsar Site/Simsar <input type="checkbox"/> Forest <input type="checkbox"/> Special area for protecting biodiversity/interest <input type="checkbox"/> Breeding/nesting ground of wildlife/occurrence of migratory species <input type="checkbox"/> Migration route/Wildlife corridor <input type="checkbox"/> Any site of national or international importance				
3	Likely impact on trees (including Timber & fruit bearing) and vegetation cover		No		
4	Possibility of degradation of land and ecosystems of surrounding?				There is no any high slope having unstable surface and land slide prone areas. The road is lies in plain area. Thus, there is less chances to degradation of land and ecosystem. However, during DPR stage it will be considered.
5	Is the project area densely populated?		No		
6	Heavy with development activities/big industries nearby & type?		No		
7	Alteration of surface water hydrology of waterways due to the project resulting in increased sediment in streams affected by increased soil erosion at construction site?		No		
8	Chance of deterioration of surface water quality due to silt runoff and sanitary wastes from worker-based camps and chemicals used in construction		No		
9	Does the sub project require significant extraction of surface or ground water?		No		
10	Increased risk of water pollution from oil, grease, fuel spills and other materials				Short term impact during Upgrading work.
11	Impact on water quality due to release of sewage/sludge?		No		Not connected to drainage system.
12	Possibility of flooding due to sewage		No		Not connected to drainage system.
13	Possibility of increased air pollution during preconstruction/construction/operation phase?				Short term impact during Upgrading work.
14	Other pollution concerns relating to inconveniences in living conditions that				Short term impact during Upgrading work.

SN	Particulars	Yes	No	Can't Say	Remarks
	may trigger cases of upper respiratory problems?				
15	Risks and vulnerabilities related to occupational health and safety due to physical, chemical, biological hazards during project construction and operation				Short term impact during Upgrading work.
16	Noise and vibration due to blasting and/or other civil works?				Short term impact during Upgrading work.
17	Possibility of poor sanitation and solid waste disposal		No		
18	Creation of temporary breeding habitats for diseases such as those transmitted by mosquitoes and rodents?		No		
19	Accident risks associated with pre construction, construction & operation phases of project				Short term impact during Upgrading work.
20	Population influx during project construction and operation that causes increased burden on social infrastructure and services (such as water supply and sanitation systems)				The population influx will be in small scale.
21	Risks to community health and safety due to the transport, storage, and use and/or disposal of materials such as explosives, fuel and other chemicals during construction and operation?				Short term impact during Upgrading work.
22	interference with other utilities and blocking of access to resource/utility				Short term impact during Upgrading work.
23	Generation of solid waste and/or hazardous waste during construction/operation of project?				Short term impact during Upgrading work.

Table 7-12: Social Screening Checklist for Gaighat Shivalay tole-Adarsha tole-Ramailo danda Manmohan school to Karamgachhi Diktel connecting Road

SN	Particulars		Remarks
1	Proposed Site Location		
	a	Land requirement for the project	Land within the RoW is already in use by the public. It is an up-gradation of an existing road. There will be no additional land requirement.
	b	Landownership of the project area: Govt. / Private lands	Land within the proposed road width is already in use by the public. As per discussion with locals and municipality, there is no

SN	Particulars	Remarks
		any outstanding issues related to grievance, compensation etc.
c	Is the project requires acquisition of Govt. land/structures? If yes please mention the area of land, number of affected structures, Households	No
d	Present use of Govt. Land that will be used for the project activities with Persons/Households using	No
e	Is the project requires acquisition of private land/structures? If yes please mention the area of land, number of affected structures, Households	No
f	Present use of Govt. Land that will be used for the project activities with Persons/Households using <input type="checkbox"/> Agricultural purposes <input type="checkbox"/> Residential purposes <input type="checkbox"/> Commercial purposes <input type="checkbox"/> Other purposes (Indicate)	Public
g	Is the project requires relocation of encroachers/squatters If yes please elaborate number and nature	No
h	Is the project requires relocation of community facilities/Govt. establishment or any object that are of religious, cultural and historical significance	No
i	Proposed project located in an area where residents are- <input type="checkbox"/> All Mainstream <input type="checkbox"/> All Indigenous peoples <input type="checkbox"/> Majority Mainstream or Non-indigenous peoples <input type="checkbox"/> Majority Indigenous peoples	Majority are Mainstream
2	Potential Social Impacts- Will the Project cause	
a	Involuntary resettlement of people? (Physical displacement and/or economic displacement)	No, resettlement is not required in this project
b	Impacts on the poor, women and children, Indigenous Peoples or other vulnerable groups?	No such impact on poor women and children, indigenous people, and/or economic displacement.
c	Will community facilities require relocation?	Yes, Triyuga small town water supply pipeline and electric poles likely to affected
d	Will the sub-project disturb any traditional activity on adjoining or nearby?	No
e	poor sanitation and solid waste disposal in construction camps and work sites	Yes, there will be concern of sanitation and solid waste disposal in construction camp and work sites.
f	Possible transmission of communicable diseases (such as STI's and HIV/AIDS) from workers to local populations?	Local peoples have knowledge on such communicable diseases but labourers' understanding may be low about possible transmission of communicable diseases
g	Large population influx during project construction and operation that causes increased burden on social	Yes

SN	Particulars		Remarks
		infrastructure and services (such as water supply and sanitation systems)?	
	h	Social conflicts relating to inconveniences in living conditions where construction interferes with preexisting roads	Yes, change in road morphology and disruption in the infrastructure like drinking water, sewer system will cause inconvenience. Traffic management plan will be prepared by the contractor and will be included in Contractor's Site specific ESMP.
	i	Describe any other impacts that have not been covered in this screening form	No permanent structure lies within the ROW of indigenous people, hence there is no adverse impact on indigenous group
	j	Describe alternatives, if any, to avoid or minimize displacement from private and public lands	No displacement of private and public land is required.
	k	RAP/ARAP Requirement	Not required

Name of the Road: **g) Pragati tole kha (Quarter front) ko road to sakela road ward 12**

Table 7-13: Environmental Screening Checklist for Pragati tole kha (Quarter front) ko road to sakela road ward 12

SN	Particulars	Yes	No	Can't Say	Remarks
1	Is the site vulnerable to major or induced hazards such as: Landslides, Flooding, Storm surge, Severe wind damage, Earthquakes, Fire, Explosion, Other (specify)		No		Possibility of Earthquake Natural Disaster, flooding.
2	Is the project area adjacent to or within any of the following environmentally sensitive areas? <input type="checkbox"/> <input type="checkbox"/> Cultural heritage site (historical, religious, traditional, or cultural significance) <input type="checkbox"/> <input type="checkbox"/> Protected Area (National Parks, Wildlife Reserve, Hunting Reserve, Conservation Areas, and Buffer Zones etc.) <input type="checkbox"/> <input type="checkbox"/> Wetland/Ramsar Site/Simsar <input type="checkbox"/> <input type="checkbox"/> Forest <input type="checkbox"/> <input type="checkbox"/> Special area for protecting biodiversity/interest <input type="checkbox"/> <input type="checkbox"/> Breeding/nesting ground of wildlife/occurrence of migratory species <input type="checkbox"/> <input type="checkbox"/> Migration route/Wildlife corridor <input type="checkbox"/> <input type="checkbox"/> Any site of national or international importance		No		The project RoW is devoid of such archaeologically and culturally significant areas and national parks

SN	Particulars	Yes	No	Can't Say	Remarks
3	Likely impact on trees (including Timber & fruit bearing) and vegetation cover		No		
4	Possibility of degradation of land and ecosystems of surrounding?				There is no any high slope having unstable surface and land slide prone areas. The road is lies in plain area. Thus, there is less chances to degradation of land and ecosystem. However, during DPR stage it will be considered.
5	Is the project area densely populated?				
6	Heavy with development activities/big industries nearby & type?		No		
7	Alteration of surface water hydrology of waterways due to the project resulting in increased sediment in streams affected by increased soil erosion at construction site?		No		
8	Chance of deterioration of surface water quality due to silt runoff and sanitary wastes from worker-based camps and chemicals used in construction		No		
9	Does the sub project require significant extraction of surface or ground water?		No		
10	Increased risk of water pollution from oil, grease, fuel spills and other materials				Short term impact during Upgrading work.
11	Impact on water quality due to release of sewage/sludge?		No		Not connected to drainage system.
12	Possibility of flooding due to sewage		No		Not connected to drainage system.
13	Possibility of increased air pollution during preconstruction/ construction/operation phase?				Short term impact during Upgrading work.
14	Other pollution concerns relating to inconveniences in living conditions that may trigger cases of upper respiratory problems?				Short term impact during Upgrading work.
15	Risks and vulnerabilities related to occupational health and safety due to physical, chemical, biological hazards during project construction and operation				Short term impact during Upgrading work.
16	Noise and vibration due to blasting and/or other civil works?				Short term impact during Upgrading work.
17	Possibility of poor sanitation and solid waste disposal		No		
18	Creation of temporary breeding habitats for diseases such as those transmitted by mosquitoes and rodents?		No		

SN	Particulars	Yes	No	Can't Say	Remarks
19	Accident risks associated with pre construction, construction & operation phases of project				Short term impact during Upgrading work.
20	Large population influx during project construction and operation that causes increased burden on social infrastructure and services (such as water supply and sanitation systems)				Suggested to use local worker during construction but Influx of people is anticipated during the time of Upgrading. Pressure upon existing natural resources such as water, existing food availability, existing educational facilities are likely to be affected with the increase in the number of outside workers during Upgrading.
21	Risks to community health and safety due to the transport, storage, and use and/or disposal of materials such as explosives, fuel and other chemicals during construction and operation?				Short term impact during Upgrading work.
22	interference with other utilities and blocking of access to resource/utility				Short term impact during Upgrading work.
23	Generation of solid waste and/or hazardous waste during construction/operation of project?				Short term impact during Upgrading work.

Table 7-14: Social Screening Checklist for Pragati tole kha (Quarter front) ko road to sakela road ward 12

SN	Particulars	Remarks
1	Proposed Site Location	
	a Land requirement for the project	Land within the RoW is already in use by the public. It is an up-gradation of an existing road. There will be no additional land requirement. Due to the narrow sections i.e from 0+560 to 0+600 and from 0+740 to 0+760 the construction width of the both proposed road section is 6.3 m.
	b Landownership of the project area: Govt. / Private lands	Land within the proposed road width is already in use by the public. As per discussion with locals and municipality, there is no any outstanding issues related to grievance, compensation etc.
	c Is the project requires acquisition of Govt. land/structures? If yes please mention the area of land, number of affected structures, Households	No

SN	Particulars	Remarks
	d Present use of Govt. Land that will be used for the project activities with Persons/Households using	No
	e Is the project requires acquisition of private land/structures? If yes please mention the area of land, number of affected structures, Households	No
	f Present use of Govt. Land that will be used for the project activities with Persons/Households using <input type="checkbox"/> <input type="checkbox"/> Agricultural purposes <input type="checkbox"/> <input type="checkbox"/> Residential purposes <input type="checkbox"/> <input type="checkbox"/> Commercial purposes <input type="checkbox"/> <input type="checkbox"/> Other purposes (Indicate)	Public use
	g Is the project requires relocation of encroachers/squatters If yes please elaborate number and nature	No
	h Is the project requires relocation of community facilities/Govt. establishment or any object that are of religious, cultural and historical significance	No
	i Proposed project located in an area where residents are- <input type="checkbox"/> <input type="checkbox"/> All Mainstream <input type="checkbox"/> <input type="checkbox"/> All Indigenous peoples <input type="checkbox"/> <input type="checkbox"/> Majority Mainstream or Non-indigenous peoples <input type="checkbox"/> <input type="checkbox"/> Majority Indigenous peoples	Majority are Mainstream
2	Potential Social Impacts- Will the Project cause	
	a Involuntary resettlement of people? (Physical displacement and/or economic displacement)	No, resettlement is not required in this project
	b Impacts on the poor, women and children, Indigenous Peoples or other vulnerable groups?	No such impact on poor women and children, indigenous people, and/or economic displacement.
	c Will community facilities require relocation?	Yes, Triyuga small town water supply pipeline and electric poles likely to affected.
	d Will the sub-project disturb any traditional activity on adjoining or nearby?	No
	e poor sanitation and solid waste disposal in construction camps and work sites	Suggested to use local worker during construction but Influx of people is anticipated during the time of Upgrading. Pressure upon existing natural resources such as water, existing food availability, solid waste patterns are likely to be affected with the increase in the number of outside workers during Upgrading.
	f Possible transmission of communicable diseases (such as STI's and HIV/AIDS) from workers to local populations?	May have chances to transmission of diseases.
	g Large population influx during project construction and operation that causes increased burden on social infrastructure and services (such as water supply and sanitation systems)?	Pressure upon existing water supply system, food availability, are likely to be affected with the

SN	Particulars		Remarks
			increase in the number of outside workers
	h	Social conflicts relating to inconveniences in living conditions where construction interferes with preexisting roads	The project area during the project construction works, other activities except commercial activities will be not affected due to excavation and construction activities. While, in the implementation phase the commercial activities will be enhanced and further increased in terms of numbers
	i	Describe any other impacts that have not been covered in this screening form	No permanent structure lies within the ROW of indigenous people, hence there is no adverse impact on indigenous group
	j	Describe alternatives, if any, to avoid or minimize displacement from private and public lands	No displacement of private and public land is required.
	k	RAP/ARAP Requirement	Not required

**Annex II: Minutes, Public Notice and Letters
ROW Declaration Letter**



त्रियुगा नगरपालिका

नगर कार्यपालिकाको कार्यालय

०२५-५२०१०८
चौमाल ५२०१८५
triyuga.mun@gmail.com

पत्र संख्या : ०७९/०८०
चलानी नम्बर : २६९०

गाईघाट, जैज्यपुर
कोशी प्रदेश, नेपाल

मिति :- २०८०/०२/०७

विषय:- सडकको ROW सम्बन्धमा।

श्री शहरी विकास तथा भवन निर्माण विभाग,

नेपाल शहरी शासकीय तथा पूर्वाधार आयोजना समन्वय कार्यालय, चक्रमहल, काठमाण्डौ।

प्रस्तुत विषयमा NUGIP कार्यक्रम अन्तर्गत यस नगरपालिकाबाट योजनाहरूको प्रथम चरणमा मिति २०७९/०६/०९ गते यसको नगर स्तरीय समन्वय समितिको निर्णयानुसार तपसिल सडकहरूको सम्भाव्यता अध्ययनका लागि प्राथमिकतामा छुनोट भएको तपसिल सडकहरूको लम्वाई र सडक क्षेत्राधिकार (राईट अफ बे) को विवरण तपसिल वर्गोजिम रहेको व्यहोरा हुन अनुरोध छ।

तपसिल

पहिलो प्राथमिकता (९,१९५० किमी)

सडकको नाम	त्रियुगा नगर क्षेत्र भित्र शहरी निर्माण सम्बन्धि मापदण्ड	प्राथमिकता न.
गाईघाट टाउन पब्लिक क्षेत्र भित्र भएको सडकहरू त्रियुगा स्कूल पूर्व ज्योतिनगर सडक विनपा ११ समेत (ROW= 12m, 18 m and 25m) लम्वाई १/५०० कि.मी.	जिल्ला विकास समितिले गाईघाट बजारमा प्लानिङ गरी ८० फिट, ६० फिट र ४० फिट चौडाई कायम सडकहरूलाई गभावत कायम गर्नुपर्ने पैज नं. ११	१
सिटिहल देवि विमानटार गाईघाट दिक्तेल सडक Access road सहित (ROW = 20 m) लम्वाई २/८८० कि.मी.	शिव मन्दिर बोक्स सतमाटे सडक २० मिटर पैज नं. ७	१
बाघारानी देवि डि एम गेट सम्म सडक (ROW = 8 m) लम्वाई ०/६५० कि.मी.	डि. एम. गेट देवि पश्चिम उदयपुर डिपिटलरीसम्म जाने बाटो ८ मिटर पैज नं. १०	१
फूलचोक देवि पि जी चोक सडक बाटैटोल त्रियुगा कोरीडर (ROW = 8m) लम्वाई ०/६६० कि.मी.	फूलचोक देवि दक्षिण मुसहरी टोल हुदै बाटैसम्म जाने सडक ८ मिटर सहित पैज नं. १०	१
सिटिहल देवि राम चन्द्र बस्नेतको घरसम्म सडक (ROW = 8 m) लम्वाई ०/२८० कि.मी.	प्रवेश माध्यमको चौडाई ८ मिटर र लम्वाई १००० मिटर पैज नं. १२	१
गाईघाट शिवालय टोल आदेश टोल रमाईलो डाडा मनमोहन स्कूल देवि करमगाडी गाईघाट दिक्तेल सडकसम्म (ROW = 8 m) लम्वाई १/६१० कि.मी.	प्रवेश माध्यमको चौडाई ८ मिटर र लम्वाई १००० मिटर पैज नं. १२	१
प्रगति टोल स (क्वाटर अगाडी) को सडक हुदै सार्केला जाने सडक चडा नं. ११ (ROW = 10 m) लम्वाई २/१७० कि.मी.	प्रवेश माध्यमको चौडाई ११ मिटर र लम्वाई १००० मिटर भन्दा माथि पैज नं. १२	१
सगरमाथा कृषि बजार पश्चिमबाट दक्षिण मदन भण्डारी सभाहल हुदै जनप्रेमी टोल बिसनपुर चडा नं. १२ (ROW = 8 m and 14 m) लम्वाई ३/७५० कि.मी.	डि.एम.गेट देवि दक्षिणबाटै पुन सुम्निमा चोक पुगने गाईघाट जनप्रेमी प्रा.वि. लक्ष्मीपुर मा.वि. हुदै बेहडवासम्म जाने बाटो सडक चौडाई १२ मिटर	२
राजाजी मन्दिर चडा नं १२ बाट उत्तरको सडक हुदै मदन भण्डारी राजमार्ग जोडने सडक (ROW = 8 m) लम्वाई ०/६६० कि.मी.	प्रवेश माध्यमको चौडाई ८ मिटर र लम्वाई १००० मिटर पैज नं. १२	३

योधार्थ:

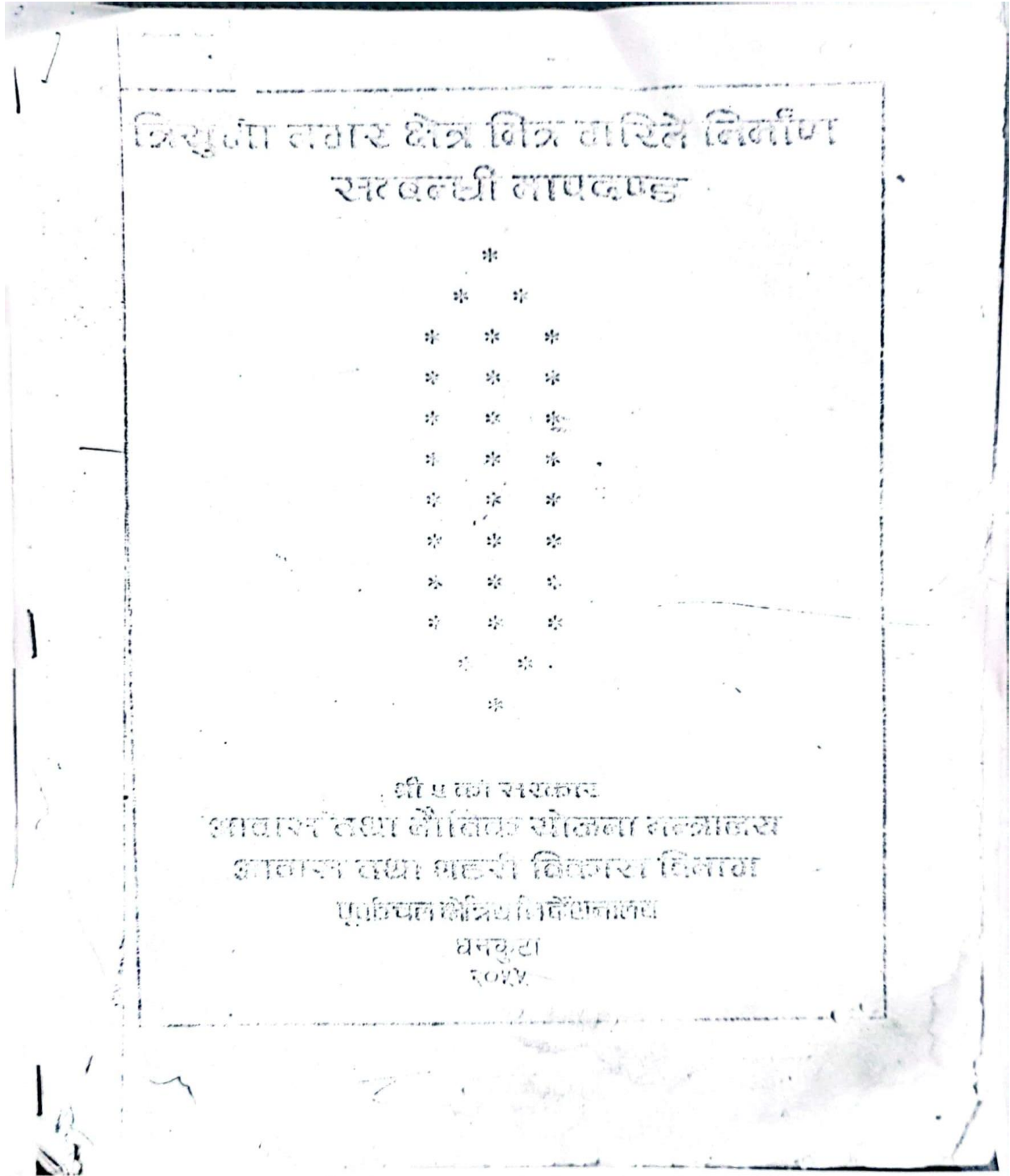
श्री BOTEK-GOEC JV

Wimal

किरण कुमार झाकी

नि. प्रमुख प्रशासकीय अधिकृत

नि. प्रमुख प्रशासकीय अधिकृत



त्रियुगा नगर क्षेत्र भित्र गरिने निर्माण
सम्बन्धी मापदण्ड



श्री ए क्म सशक्त
आवास तथा लौकिक सेवा सन्त्रास्त
आवास तथा ग्रामी विद्युत विभाग
एनपिचल क्षेत्र विकास न्याय
धनकुटा
२०१९

त्रियुगा नगर क्षेत्र गरिने निर्माण सम्बन्धी मापदण्ड तयार गर्ने टोली:

१. श्री कविन्द्र बहादुर बिष्ट
क्षेत्रिय निर्देशक, आवास तथा श.वि.वि.
पूर्वाञ्चल क्षेत्रिय निर्देशनालय
धनकुटा
२. श्री महेश चन्द्र न्यौपाने
इन्जिनियर, जिल्ला विकास समितिको कार्यालय
उदयपुर
३. श्री कोपनाथ अधिकारी
इन्जिनियर, जिल्ला आवास तथा शहरी विकास कार्यालय
उदयपुर
४. श्री ब्रम्ह देव चौधरी
शोभर सियर, त्रियुगा नगर पालिकाको कार्यालय
उदयपुर
५. श्री केशव नाथ कट्टेल
सुर्भेक्षक, आवास तथा शहरी विकास विभाग
पूर्वाञ्चल क्षेत्रिय निर्देशनालय
धनकुटा

भूमिका (Preface)

श्री ५ को सरकारबाट वि.स. २०५२ साल चैत्र १६ गते गठित गाईघाट नगर विकास समिति क्षेत्रभित्र दिन प्रति दिन बढ्दो घना जनआवादी, उद्योग-वाणिज्य र व्यापारमा दीप्तता, यातायात तथा संचारका साधनहरूको विकास, आधुनिक वैज्ञानिक साधनहरूको बढ्दो उपयोग, शिक्षा, स्वास्थ्य जस्ता सामाजिक पक्षको उन्नती जस्ता कुराहरूले यस क्षेत्रको आर्थिक तथा शैक्षणिक गतिविधिलाई प्रत्यक्ष प्रभाव पार्ने सक्ने कुरा सर्व विदित छ । यसरी मानव सभ्यता र सन्दर्भित लगायत सामाजिक, आर्थिक सम्पूर्ण क्षेत्रमा नै आउने परिवर्तनले हाथा गाई घर एव वजार केन्द्रहरू क्रमशः शहरीकरण तर्फ उन्मुख हुदै जाने क्रम बढ्दै गएको वर्तमान परिप्रेक्ष्य भित्र नै यो नगर विकास समिति पनि पर्दछ ।

बढ्दो शहरीकरणले कतिपय कुराहरूमा जनआवासीलाई हित पुग्ने सक्ने भएता पनि अव्यवस्थित बसोबासका कारण फोहरमैला बहने, विजुली, दुरसंचार, ढल विकास, सडक निर्माण आदि जस्ता भौतिक पूर्वाधारहरू तयार गर्ने सन्दर्भमा विविध समस्याहरूले समष्टिमा शहरको संरचनालाई नै विगान्न सक्ने हुदा त्यस्ता विद्गीहरूलाई समग्रमा नै निराकरण गर्नु अपरिहार्य भएकोले सम्बन्धित क्षेत्रभित्रका नगर विकास समिति, नगर पालिका, पेशागत संघ संस्था, कुटुंबिकी, गन्दमान्य व्यक्तिहरू लगायत सर्वसाधारणहरू समेतबाट संकलित राय सुझावहरूलाई आधार मानि विस्तृत छलफल र गहन अध्ययन पश्चात प्रस्तावित भू-उपयोग क्षेत्र र सडक तथा भवन निर्माण सम्बन्धी मापदण्ड तयार पारिएको छ ।

अन्तमा यस कार्यमा सहयोग गर्ने सम्पूर्ण व्यक्तिहरू धन्यवादको पात्र हुनुहुन्छ ।

भन््याङ (Staircase):

भन््याङको न्युनतम चौडाईहरू निम्नानुसार हुनुपर्नेछ:

क) आवासिय भवन (घरहरू)	.....	०.९० मी.
ख) आवासिय (होटल भवनहरू)	.....	१.५० मी.
ग) मानिसहरू जम्मा हुने (अडिटोरियम, नाचघर, सिनेमा घर आदी) भवन	.....	१.५० मी.
घ) शैक्षिक भवन	.....	१.५० मी.
ङ) संस्थागत भवन	.....	२.०० मी.
च) अन्य भवनहरू	.....	१.५० मी.

सुटकिला (Step) को चौडाई (Tread):

आवासिय भवनहरूको लागि सुटकिलाको चौडाई २५ से.मी. र अन्य भवनहरूका लागि ३०.० से.मी. भन्दा कम हुनुहुदैन । एउटा फ्ल्याटमा बढीमा (Step) संख्या १२ वटा र कमिमा ३ वटा हुन सक्नेछ ।

सुटकिला (Step) को उचाई (Riser):

(Riser) को उचाई १५-१९ से.मी. सम्म हुन सक्नेछ ।

यसको सख्या एउटा फ्ल्याट (Flight) मा बढीमा १३ वटासम्म हुन सक्नेछ । बेस्ट स्लाब (Waist Slab) को स्लोप २५° देखि ४२° सम्म हुनु पर्नेछ ।

लिफ्ट (Lift):

भवनको उचाई ४ (चार) तला भन्दा धेरै बढाउनु पर्ने भएमा लिफ्टको व्यवस्था गर्नुपर्नेछ ।

६. सडकको लागि छोडनु पर्ने जग्गा (राईट अफ वे):
सररमाथा राजमार्गको लागि सडक एनले तोके बमोजिम हुनेछ ।

६.१ बाहिरी चक्रपथ (Outer Ring Road):

भद्राचारी - चुहाडे - जलजले - देउरी - श्रीपुर - बगाहा - पाप - मुल्के - छैजपुरको - मिलनचोक - पोताहा - विजयपुर - बेहडवा - अचारी - भिन्न मन्दिर - चोकले - रास्ताडे - राजावास - साईखोला - सारखी हुँदै भद्राचारीसम्मको चक्रपथको लागि:

राईट अफ वे (Right of Way):

सडक केन्द्र रेखा देखि हुँदै तर्फ	.....	१०/१० मी.
सडक कारीघारका धरहरू बिचको	.....	२० मी.
दुई (Between building lines)	.....	२० मी.

City hall Simlitar Gaighat Diktel road

पानी ट्यांक, पारावेट बाल, भन््याङ छोपनका लागि बनाईने उचाई बाहेक निर्माण गर्न सकिने अधिकतम उचाई-१२.२० मीटर ।

६.२ भित्री चक्रपथ (Inner Ring Road):

खति बजार वीरन - भिक्तकेटील - दिपु चौक - राजावास - साईखोला - सरस्वती - नया भोहा - चुहाडे चौक - जलजले - कुमारी चौक - देउरी - छैरजोडी खोला - मुल्के - बगाहा चौक - मंगी गहा - दियुगा कजदेवाट गाईघाट बजारको मुख्य चौक हुँदै - खती बजारसम्मको चक्रपथको लागि:

सडक केन्द्र रेखा देखि हुँदै तर्फ	.....	१०/१० मीटर
सेट ब्याक	.....	३/३ मीटर हुँदै तर्फ

पानी ट्यांक पारावेट बाल, भन््याङ छोपनका लागि बनाईने उचाई बाहेक निर्माण गर्न सकिने अधिकतम उचाई-१२.२० मीटर

६.३ रेडियल रोड्स (Radial Roads):

- जिल्ला सदरमुकामवाट वाहिरी चक्रपथसम्म जोड्ने सडक ।
- मेन चौकवाट रजत जयन्ती चौकहुँदै उत्तर शिव मन्दिरसम्म जाने सडक ।
- गाईघाट बजार मुख्य चौक देखि पूर्व अचारी जाने सडक ।
- गोतीगडा चौक देखि पूर्व मुल्केसम्म जाने सडक ।
- नगर क्षेत्र बिचको सररमाथा राजमार्ग ।
- गाईघाट बजार देखि बगाहासम्म ।
- गाईघाट बजार चोकले हुँदै बस्पा खोलासम्म
- सडकको केन्द्र रेखावाट हुँदै तर्फ - ८/८ मीटर
- सेट ब्याक २/२ मीटर हुँदै तर्फ
- पानी ट्यांक, पारावेट बाल, भन््याङ छोपनका लागि बनाईने उचाई बाहेक निर्माण गर्न सकिने अधिकतम उचाई-१२.२० मीटर ।

६.४ शाखा सडक (आर्टेरियल रोड्स):

- चक्रपथ वा रेडियल सडकवाट विभिन्न भू-उपयोग क्षेत्रहरू जोड्ने सडक ।
- सडकको केन्द्र रेखावाट हुँदै तर्फ ६/६ मीटर
- सेट ब्याक १/१ मीटर हुँदै तर्फ

विभिन्न भू-उपयोग क्षेत्र तथा स्थान:

औद्योगिक क्षेत्र	- बगाहा र श्रीपुर
बजार क्षेत्र	- छैजपुरको मिलन चौक/गाईघाट/देउरी/जलजले/वीरेनी १९ न./चुहाडे/बगाहा/चोकले/परपापुर ।

आवास क्षेत्र - चौकते बास मन्दिर भएको ठाउँबाट उत्तर पूर्व वरिपरि क्षेत्र/
रास्ताटे/पुरानो गाईघाट
बसपार्क क्षेत्र - बस्वा बाईको उत्तर/शिव मन्दिरको छेउ ।
हाट बजार क्षेत्र - हवाई फिल्डको जग्गा ।
मुन्डी/मण्डी बजार - हासको छति बजार क्षेत्र ।
देउरीमा पुरानो गा.वि.स. भवनको दक्षिणपट्टि पोखरी जस्तो ठाउँ

खुला क्षेत्र (Open Space): मिलनचोक देखि दक्षिण फर्सेट खोला देखि पूर्व क्याम्पस देखि
पश्चिम जंगल भन्दा उत्तरको क्षेत्र ।

स.साना खुला क्षेत्र: जिल्ला विक्स. समितिको दक्षिणपट्टि मन्दिरलाई पुग्ने गरि
छुट्याई बाँकी रहेको जग्गामा ।

मोतिगढा पन्चलाल खोरिया
जलजले बजार देखि माथिको डाडो (रतनपुर डाडो)

पर्यटन क्षेत्र - ककनी (त्रि.न.पा. १२ र ४ असार/धार्मिक क्षेत्र)

मूढ, अशक्त र अपाङ्गहरुका लागि आवास क्षेत्र: बगाहाको सालघारी (परमानन्दको घर
अगाडी बाटो देखि तलतिर)

मुर्दाघाटको स्वस्थ - त्रियुगा कञ्चै भन्दा एक कि.मी. तल/चपनमा डा. रनेश्वर
खोरिया

रंगशाला - त्रि.न.पा. ७ मुल्के

हमिपट्ट साईट - जलजले भन्दा दक्षिण पश्चिम जंगलमा भन्दा जस्तो ठाउँ
चूल क्याम्पस - चुहाडे देखि जलजले साईडको विचमा/भण्डावारी/बोस्ते छैजपुर/
गाईघाट बजार, देउरी/भुभरहुवा ।

कृषि क्षेत्र - बांसवारी एरिया/असार/परिख/सकिपुर/चुहाडे १४ नं. बाटो
दायाँ बायाँ बाहेकको
क्षेत्र/धैरुडवा/होस्ते/इपौती/छैजपुर/पास्तेवाम ।

५.४ प्रशाखा सडक (Connector Roads):

वार्डरी चक्रपथ, मित्रि चक्रपथ, रेडियाल सडकबाट र आर्टेरियल सडकहरु मध्ये
कुनैबाट सम्बन्धित जोड्ने सडक ।

सडकको चौडाई १० मीटर
मेट व्याक १/१ मीटर सडकको दुवै तर्फ

Sagarmatha
Agriculture
bazar west to
south Madan
Bhandari Hall
to Janpremi
tole Bisanpur
ward 12 Road

पानी ट्याक, कारभेट बाल, भन्दा छेपका लागि बनाउने उचाई बाहेक निर्माण गर्न
नसकिने अधिकतम उचाई १.२० मीटर

- परपपुर चोक देखि सती खोला हुँदै रामाटे जाने सडक ।
- परपपुर चोक देखि रोता वौडिहा खुस हुँदै नयाँ बसि सम्म जाने बाटो ।
- डि.एम.गेट देखि दक्षिणपट्टिबाट पूर्व मुनिमा चोक - पुरानो गाईघाट - बरपेनी
प्रा.वि. - सकिपुर मा.वि. हुँदै बेहडवा सम्म जाने बाटो ।
- चुहाडेबाट भण्डावारी हुँदै मुकुण्डीसम्म जाने बाटो ।

६.१ गाहरी सडक (Urban Road):

एक आवासीय तथा व्यापारिक क्षेत्रबाट अर्को आवासीय क्षेत्र वा व्यापारिक क्षेत्रसम्म
सोड्ने सडक ।

सडकको चौडाई १ मीटर
मेट व्याक १/१ मीटर सडकको दुवै तर्फ

पानी ट्याक, कारभेट बाल, भन्दा छेपका लागि बनाउने उचाई बाहेक निर्माण गर्न
नसकिने अधिकतम उचाई १.२० मीटर ।

- बावरी देखि खरवार टोल - दुम्ने खोला हुँदै बासवारीसम्म जाने बाटो ।
- आदी खारे - रामाटे प्रा.वि. हुँदै परिसम्म ।
- आदी खारे कुवेट हुँदै खटारी गाईघाटमा मिल्ने सडक ।
- गाईघाट रिक्लेन सडकबाट परपपुरको गोरे विद मन्दिरसम्म
- सकिपुर गाउँ विक्स समिति गाईघाटबाट उत्तर सती मुनास धौली सडक जाने
बाटो सम्म ।
- फेन पार्किङको धरवाट मुनिमा चोकसम्म ।
- पुल चोक देखि दक्षिण मुकुण्डी टोल हुँदै गोटे टोलसम्म जाने सडक ।
- सिटी स्क्वाट पूर्व संगम टोल हुँदै धरवा खोलातिर जाने बाटो ।
- डि.एम.गेट देखि पश्चिम खरवार विदिसम्म जाने बाटो ।
- मोतिगढाबाट चापसम्म जाने सडक ।
- मोतिगढाबाट वि.मा.वि. सङ्घबाट पश्चिम पौवरी टोलबाट पुगी पञ्चवाम
रामगार्नको क्याम्पस चोकमा मिल्ने बाटो ।
- बसपार्क चोकबाट क्याम्पससम्म जाने बाटो ।
- फर्सेट खोला भन्दा पूर्व खरवारबाट पहाड जाने सडक ।

Fulchowk PG Chowk Road Botetole
Triyuga Corridor section

Babaran DM Gate Road Section

६.२ गाँस सडक (Service Road):

मार्गका दुवैतिर सडकबाट फेरि घरसम्म जोड्ने सडक ।

सडकको चौडाई ४ मीटर
सडकको तर्फाई गर्दिमा २० मीटर

Town Planning Road

निम्नलिखित विद्यमान समितिले साईंघाट बजारमा प्लानिङ गरि २०', १०' र ४०' चौडाई हरम बरेका साइडरस्ताई यथासकत बनाव गनुनेपर्नेछ ।

७. नदी किनारा, सार्वजनिक जग्गा बाधि नभिक निर्माण गर्दा छोडनु पर्ने जग्गा:
नदीमा वर्षाको समयमा आउन सक्ने बाढी (High Flood) ले अक्षर गर्न नसक्ने क्षेत्रमा समायचित निस्सकले स्वल्पत निरिक्षण गरि धर बनाउन उपयुक्त हुने भएमा स्वीकृत दिन सक्नेछ तर उक्त निर्माणले हरित क्षेत्रलाई कुनै निरिक्षणको अक्षर गपर्ने हुनुपर्दछ । बाटोको दायाँ बायाँ २०/१० मिटर (नदीको Course मातेका) हरित क्षेत्रको लागि छोडनु पर्नेछ । सार्वजनिक जग्गा बाटो घाटो आदिमा भवन निर्माणका लागि आवश्यक पर्ने निर्माण सामग्री पुर्‍याउने तथा बेच विचन गर्ने कार्य गर्न पाईने छैन ।

८. पैटी, बलेसी, हल आदिको लागि छोडनु पर्ने जग्गा:

- छत्र तथा बलेसीको पानी आपत्ती जग्गामा मात्र सञ्चालन पाईनेछ । तथा सार्वजनिक सडक क्षेत्रमा पानी सञ्चालन पर्दा अनिवार्य रूपमा जमिनसम्म पाईपको व्यवस्था गर्नुपर्नेछ ।
- आपत्ती पर्जालसम्बन्धी पनि दिनेदीसाईं यथा हुने गरि सो पर्जालमा बलेसीको पानी सञ्चालन पाईने छैन ।
- घरको पैटीको माथ दिनेको घरका पैटीसंग मिलाउनु हुनुपर्दछ ।
- पैटीको म्युनसिम उपार्द १ मिटर (०.२० मिटर) हुनु पर्दछ । म्युनसिम चौडाई २ मिटर (०.६० मिटर) हुनु पर्दछ । घरको पैटी सार्वजनिक जग्गा सडकमा बनाउन हुदैन ।
- पैटी हल्लाको छुवाबाट आपत्ती अस्थायी पानी सक्ने गरि बढीमा १ मिटरसम्म ब्याण्टीसिपर स्थापन विचालन पाईनेछ ।

९. प्रत्येक घर/घरेरीमा आवश्यक पर्ने पर्जाल प्राकृतिक प्रकाश र हावाको व्यवस्था:

- अन्तर्गत प्रयोगको हिसाबले बर्जानका कने कोठाहरू बाहिरी वा भित्री खुला भाग वा खुला बरन्दासंग जोडिएको हुनु पर्नेछ ।
- बाहिरी वा भित्री खुला भागका कोठामा आवश्यक हुने प्रकाश र भेन्टिलेसनको लागि बरन्दा र खुले बदन वा निर्माणको अगाडीको भाग, पछाडीको भाग वा बाईंछाट बायाँछाट बाईंछाटको भाग त्यस्तो ठाउँमा जोडिने खुला भाग थपियमा बाटोसाईं फर्किको पार्न समेत पर्जाल हुने गरी छोडनु पर्नेछ ।
- प्रकाश र भेन्टिलेसनको लागि भवनको प्रदेको क्षेत्रफलको प्रतिशतमा १/१० भाग क्षेत्रफल माथिक भूभाग तथा भेन्टिलेसनको क्षेत्रको हुनु पर्दछ । बर्जानका बरन्दाहरू अन्ततः ३'०" भन्दा बढी सार्जन्दी हुनु हुदैन । अन्ततः बरन्दाहरू सवभय ३'०" चौडाई र २'०" उचाईको हुनु पर्नेछ । चौडाई र उचाईको अनुपात १:२ भन्दा बढी हुनु हुदैन ।

१०. घरेरी विकास भई सो उपयोग गर्न आउने जनसमुदायका लागि आवश्यक पर्ने सामुदायिक सेवा र आधारभूत पूर्वाधारहरूको व्यवस्था तथा निजी क्षेत्रका जग्गा विकासकर्ताले पावना गर्नुपर्ने मापदण्ड तथा शर्तहरू:
 दुर्नैपनि वृत्तिक, फर्म, सप, संस्थाले श्री ५ को सरदारघाट या छरिद गरि श्रवणिय पट्टाहरू विचनस (Planning) गरि जनसमुदायसाईं बेच विचन गर्नका लागि निम्नानुसार पूर्वाधार कार्यक्रमको व्यवस्था गरि नगर विकास समितिले स्वीकृती लिई बेच विचन गर्न सकिनेछ ।

प्रवेश माध्यमको चौडाई र सम्बाई:

प्रवेश माध्यमको चौडाई (मी.)	प्रवेश माध्यमको सम्बाई (मी.)
४.०	४०.०
६.०	२००.०
८.०	१०००.०
११.०	१०००.० भन्दा माथि

- City Hall Ramchandra Basnet house Road
- Gaighat Shivalay tole-Adarsha tole-ramailo danda mamlohan school to karangachhi diktel connecting Road

निजी क्षेत्रका जग्गा विकासकर्ताले नगर विकास समितिले बर्गिकरण गरेको भू-उपयोग क्षेत्र माथि आवश्यक क्षेत्र विचन मात्र जग्गा विकास कार्यक्रम नगर विकास समितिले स्वीकृती लिई गर्नु पर्नेछ ।

- प्रत्येक घरेरीको क्षेत्रफल ४,७५' (२ आना २ बीसा) घुर् भन्दा कम हुने गरि जग्गा टुक्र्याउन पाईने छैन ।
- घरेरीको चौडाई कमिमा ६ मिटर हुनु पर्नेछ ।
- म्युनसिम बौतिक पूर्वाधारहरू अन्तः प्रत्येक घरेरीमा घुर्ने छप्पलिनस (Gravel) बाटो बर्जालको पानी निस्कानको लागि परिक्रम नाला (Strom drain) को व्यवस्था गर्नु पर्दछ ।
- सडकमा जानेपानीको लागि नि.आ.ई. पाईप जोड्याउने कार्य गर्नुपर्ने छ ।
- यदि श्रवणिय भवनहरू निर्माण गरि चर्द साधारणसाईं बेच विचन गर्ने भएमा प्रत्येक पट्टको क्षेत्रफलको अतिरिक्त ७०% घाटण्ड कमर हुने गरि भवन निर्माण गर्न पाईने छ ।

१०.१ निजी क्षेत्रका जग्गा विकासकर्ताले जग्गा विकास कार्यक्रम संचालन गर्दा विभिन्न प्रयोगका लागि छुट्याउनु पर्ने जग्गा निम्नानुसार हुनु पर्नेछ ।

- सार्वजनिक खुला क्षेत्र (पार्क, खेलकुद, मैदान, हाँचाली क्षेत्र, सार्वजनिक गोपरा) जग्गा जग्गाको १० - १२%
- सार्वजनिक क्षेत्र (आवाकका लागि माथ) जग्गा जग्गाको १५ - २०%
- मिश्रित उपयोग क्षेत्र (समावेश तथा व्यापारिक प्रयोगको क्षेत्र) जग्गा जग्गाको ४ - ५%

Formation of GRM (1st Level)



पत्र संख्या :- ०८००८१
पलाणी नठपर :-

त्रियुगा नगरपालिका

गाईघाट, बुटवल
कोशी प्रदेश, नेपाल
२००३

०१५-५२०७०८
फ्याक्स ५२०७०५
triyuga.mun@gmail.com

मिति: २०८०/०६/१७

विषय:- प्रथम तहको गुनासो सुनुवाई समिति गठन गरिएको बारे

मिति २०८०/०६/१७ गतेका दिन यस त्रियुगा नगरपालिकाका नगर प्रमुख श्री बसन्त कुमार बस्नेत ज्यूको अध्यक्षतामा बसेको बैठकले विश्व बैंकको आर्थिक सहयोगमा नेपाल शहरी शासकीय तथा पूर्वाधार आयोजना (NUGIP) मार्फत स्तरोन्नति हुन लागेको यस उदयपुर जिल्ला त्रियुगा नगरपालिका वडा नं. १०, ११, १२, १३ मा पर्ने पहिलो र दोस्रो प्रथमिकतामा परेका बाटोहरूको कुल लम्बाई १४.५ किलोमिटर सडक खण्डको निर्माण अघि र निर्माणका क्रममा कुनैपनि प्रकारका समस्याहरू, गुनासाहरू आई सोको सामाधानको लागि आयोजनाको मापडण्ड अनुसार तपसिलमा उल्लेखित प्रथम तहको गुनासो सुनुवाई समिति गठन गर्ने निर्णय गरियो।

तपसिल

१. संयोजक: श्री सुमन्त कोइराला, वडाध्यक्ष वडा नं. ११
२. सदस्य: श्री किरन पाण्डे, वडाध्यक्ष वडा नं. १०
३. सदस्य: श्री अनिता थापा, का.बा अध्यक्ष वडा नं. १२
४. सदस्य: श्री नविन कुमार चौधरी, वडाध्यक्ष वडा नं. १३
५. सदस्य: श्री उर्मिला बि, क वडासदस्य वडा नं. १०
६. सदस्य: श्री निला काफ्री ढोली वडासदस्य वडा नं. ११
७. सदस्य: श्री राम कली विशकर्मा वडासदस्य वडा नं. १२
८. सदस्य: सामाजिक सुरक्षण/वतावरण विशेषज्ञ/सुपरभिजन ईन्जिनियर (DSC)

बसन्त कुमार बस्नेत

नगर प्रमुख

बसन्त कुमार बस्नेत

नगर प्रमुख

Formation of GRM (2nd Level)



पप संख्या :- ०८००८१
चलानी बजार :-

त्रियुगा नगरपालिका

गाइयाड, उदयपुर
कोशी प्रदेश नेपाल
जयपुर

०१५-४२०१०८
फ्याक्स ४२०१८५
triyuga.mun@gmail.com

सूचना प्रकाशन मिति: २०८०/०७/१५

बिषय:- द्वितीय तहको सुनुवाई समिति गठन गरिएको बारे

मिति २०८०/०७/१५ गतेका दिन यस त्रियुगा नगरपालिकाका नगर प्रमुख श्री बसन्त कुमार बस्नेत ज्यूको अध्यक्षतामा बसेको बैठकले विश्व बैंकको आर्थिक सहयोगमा नेपाल शहरी शासकीय तथा पूर्वाधार आयोजना (NUGIP) मार्फत स्तरोन्नति हुन लागेको यस उदयपुर जिल्ला त्रियुगा नगरपालिका वडा नं. १०, ११, १२, १३ मा पर्ने पहिलो र दोस्रो प्रथमिकतामा परेका बाटोहरूको कुल लम्बाई १४.५ किलोमिटर सडक खण्डको निर्माणका अबधिमा आउने गुनासाहरूको अध्ययन गरी स्थानिय स्तरमार्फत समाधान गर्न निम्न उल्लेखित ब्यक्तिहरू रहनेगरी एक गुनासो सुनुवाई समिति (GRC) गठन गरीएको बारे सम्बन्धित सबैलाई जानकारी गराइन्छ । साथै आयोजना सम्बन्धि कोहिकसैलाई कुनै गुनासो भए उल्लेखित गुनासो सुनुवाई समितिमा मौखिक वा लिखित रूपमा जानकारी गराउनहुन अनुरोध छ ।

तपसिल

१. संयोजक: श्री बसन्त कुमार बस्नेत त्रियुगा नगरपालिकाका नगर प्रमुख
२. सदस्य: श्री महेश्वरी राई, नगरपालिका उप- प्रमुख
३. सदस्य: नगरपालिका प्रशासन शाखा प्रमुख
४. सदस्य: नगरपालिका आयोजनाका सम्पर्क ब्यक्ति(Engineer)
५. सदस्य: नगरपालिका सामाजिक अधिकृत/ सामाजिक विकाश शाखा प्रमुख
६. सदस्य: सामाजिक सुरक्षण, वतावरण विशेषज्ञ, सुपरभिजन, ईन्जिनियर (DSC)


बसन्त कुमार बस्नेत

नगर प्रमुख

बसन्त कुमार बस्नेत
नगर प्रमुख

Letter regarding Anti-Harassment Cell



पत्र संख्या: २०७९/०८०
चलानी नं.: २०५२

त्रियुगा नगरपालिका
नगर कार्यपालिकाको कार्यालय
गाईघाट, उदयपुर
कोशी प्रदेश, नेपाल




☎ ०३५-४२०१०८
www.triyugamun.gov.np

मिति: २०८०/०३/१७

विषय : सम्पर्क व्यक्ति (Focal Person) तोकिएको ।

अधिकृतस्तर सातौं, श्री राजकुमार खड्का (९८५२८३१६००)
सामाजिक विकास शाखा प्रमुख,
त्रियुगा नगरपालिका, गाईघाट, उदयपुर ।

प्रस्तुत विषयमा तपाईंलाई यस अघि तोकिएको जिम्मेवारीको अतिरिक्त यस नगरपालिकाको Anti Harassment Cell को सम्पर्क व्यक्ति (Focal Person) तोकिएको व्यहोरा अनुरोध छ ।


बिष्णु भक्त सिग्देल
प्रमुख प्रशासकीय अधिकृत

बोधार्थ

श्री शहरी विकास तथा भवन निर्माण विभाग,
नेपाल शहरी शासकीय तथा पूर्वाधार आयोजना, आयोजना समन्वय कार्यालय,
बदरमहल, काठमाण्डौ ।

प्रमुख प्रशासकीय अधिकृत

Email: Info@triyugamun.gov.np
Triyuga.mun@gmail.com

Public Notice for ESMP Preparation



पंच संख्या :- ०८०१०८१
पलासी नगर :-

त्रियुगा नगरपालिका
नगर कार्यपालिकाको कार्यालय
गाईघाट, उदयपुर
कोशी प्रदेश नेपाल

☎ ०३५-४२०१०८
फ्याक्स ४२०१८५
triyuga.mun@gmail.com

मिति :- २०८०/०६/१५

सूचना !

सूचना !!

सूचना !!!

विश्व बैंकको आर्थिक सहयोगमा शहरी विकास तथा भवन निर्माण विभाग (DUDBC), नेपाल शहरी शासकीय तथा पूर्वाधार आयोजना (NUGIP) द्वारा स्तरोन्नति हुन लागेको यस उदयपुर जिल्ला त्रियुगा नगरपालिका वडा नं. १०, ११, १२ र १३ स्थित पर्ने पहिलो प्राथमिकतामा परेको सडक खण्डको स्तरोन्नति गर्ने भएको छ। यसै शिलशिलामा उक्त सडक खण्डको विस्तृत परियोजना प्रतिवेदन तयारीका चरणमा गरिने वातावरणीय तथा सामाजिक व्यवस्थापन योजना (ESMP) तयारीका लागि परामर्शदाताका DSC Team, नगरपालिका आयोजनाबाट प्रभावित हुने बासिन्दा / जग्गाधनीहरु विच निम्न लिखित स्थान र मितिमा हुने विस्तृत छलफल तथा अन्तरकृया कार्यक्रममा उपस्थित हुनका लागि सम्बन्धित सबै सरोकारवालालाई सूचित गरिन्छ।

अन्तरकृया हुने मिति र स्थान :

मिति : २०८०/०६/२५

मय : विहान ११:०० बजे

स्थान : त्रियुगा नगरपालिका सभाहल

ई.शैलेन्द्र कुमार मण्डल

शहरी पूर्वाधार विकास शाखा

ई. शैलेन्द्र कुमार मण्डल
इन्जिनियर

Notice related to GRM



त्रियुगा नगरपालिका

नगर कार्यपालिकाको कार्यालय

०२५- ४२०७०८
फ्याक्स ४२०७०५
triyuga.mun@gmail.com

पत्र संख्या :- ०८०१०८१
चलानी नगर :-

गाईघाट, उदयपुर
कोशी प्रदेश नेपाल

सूचना प्रकाशन मिति: २०८०/०६/१८

सूचना !

सूचना !!

सूचना !!!

मिति २०८०/०६/१८ गतेका दिन यस त्रियुगा नगरपालिकाका नगर प्रमुख श्री बसन्त कुमार बस्नेत ज्यूको अध्यक्षतामा बसेको बैठकले विश्व बैंकको आर्थिक सहयोगमा नेपाल शहरी शासकीय तथा पूर्वाधार आयोजना (NUGIP) मार्फत स्तरोन्नति हुन लागेको यस उदयपुर जिल्ला त्रियुगा नगरपालिका वडा नं. १०, ११, १२, १३ मा पर्ने पहिलो प्राथमिकतामा गाईघाट टाउन प्लानिङ क्षेत्र भित्र भएको सडकहरु, सिटिहल देखि सिमलटार गाईघाट दित्केल सडक, बाबारानी देखि डि एम गेट सडक, फुलचोक देखि पि जी चौक सडक वोटेटोल त्रियुगा कोरीडोर, सिटिहल रामचन्द्र बस्नेतको घरसम्म सडक, गाईघाट शिवालय टोल आर्दश टोल रमाईलो डाडा मनमोहन स्कूल देखि करमगाछी गाईघाट दित्केल सडक, प्रगति टोल क्वाटर अगाडीको सडक हुदै साकेला जाने सडक र दोस्रो प्राथमिकतामा सगरमाथा कृषि बजार पश्चिमबाट दक्षिण मदन भण्डारी सभाहल हुदै जनप्रेमी टोल विसनपुर परेका बाटोहरुको कुल लम्बाई १४.५ किलोमिटर भएको सडक खण्डको निर्माणका अबधिमा आउने गुनासाहरुको अध्ययण गरी स्थानिय स्तरमानै समाधान गर्न आयोजना स्तरमा वडा ११ का अध्यक्ष श्री सुमन्त कोइराला ज्यूको संयोजकत्वमा निम्न उल्लेखित व्यक्तिहरु रहनेगरी एक गुनासो सुनुवाइ समिति (GRC) गठन गरीएको बारे सम्बन्धित सबैलाई जानकारी गराईन्छ । साथै आयोजना सम्बन्धित कोहिकसैलाई कुनै गुनासो भए उल्लेखित गुनासो सुनुवाइ समितिमा मौखिक वा लिखित रूपमा जानकारी गराउनहुन अनुरोध छ ।

तपसिल

१. संयोजक: श्री सुमन्त कोइराला , वडाध्यक्ष वडा नं. ११
२. सदस्य: श्री किरन पाण्डे , वडाध्यक्ष वडा नं. १०
३. सदस्य: श्री अनिता थापा, का.बा अध्यक्ष वडा नं. १२
४. सदस्य: श्री नविन कुमार चौधरी , वडाध्यक्ष वडा नं. १३
५. सदस्य: श्री उरमिला बि,क वडासदस्य वडा नं. १०
६. सदस्य: श्री निला काक्री ढोली वडासदस्य वडा नं. ११
७. सदस्य: श्री राम कली विश्कर्मा वडासदस्य वडा नं. १२
८. सदस्य: सामाजिक सुरक्षण/वतावरण विशेषज्ञ/सुपरभिजन ईन्जिनियर (DSC)

ई. शैलेन्द्र कुमार मण्डल
शहरी पूर्वाधार विकास शाखा
ई. शैलेन्द्र कुमार मण्डल
ईन्जिनियर

Minute related to Labor camp, stockpiling area, spoil site location

आज मिति २०८०।०८।१५ गतेका दिन त्रियुगा नगरपालिका मार्दत कार्यन्वयन हुने कामले खासतया तथा र्कनकार क्रामोन्ग (NUGIP) इकापुर जिल्लाका त्रियुगा नगरपालिका पहिलो दोस्रो पथविभागा परीका नाराइका, गार्डवाल, गडव प्लाकिङ क्षेत्राडिका इकाइका सिटिल देरी सिमलरा गाईकाट दिक्कल सड, नावागरी देरी डि. एगुटा, फुलचोरे देरी वि. वि. चौद चोरेगोल त्रियुगाकोरि-डोर, सिटिल देरी रामचन्द्र वरुनेतका बासमा, गार्डवाल सिमल गोल आकरी गले रामडिलो डाडा प्रमोत्तन स्कूल देरी बरमागदी गार्डवाल दिक्कल सड, पुत्रिगले (ख) कुवाट अगादीको सड हुने साइला जोते सड सगरमाथा कृषि कला (पञ्चनीकनर दासले) मदतगन्डाकी शाखल ईडे जातपनीगले त्रियुगा कडा ई १२ का स्वकलेसुत इन्जनी -गेले सिनेसिलामा शायोकाका लागि लेबर कर्मचारी न त्रियुगा सामग्रीक नखन कजा त्रियुगा शरको इताने प्रत नगरपालिकाका नगरप्रमुख श्री अणुनाइका उक्त सुकलेसुतमा पर्ने कजा शाने समजसेवी शक्तिनी न खासतया दल लगाएत सुरकारवालाकले सडिका उपर-छातिमा गरिएको दालफल तथा फासका तणशिले उपर-छाति गालीकडा सडगातिमा से डलवपुवी र्व इलोलाशका साथ राम्रत भयो।

उपस्थिति

१.	श्री	नसल कृष्ण बरुनेत	(नगरप्रमुख)	श्री
२.	श्री	महेन्द्रजी काई	(नगरप्रमुख)	
३.	श्री	त्रियुगा शक सिटिल	(प्रमुख पशासविपक्ष)	
४.	श्री	व्युत्त कोडरेला	(पडाकाका - ११)	
५.	श्री	त्रियुगा कृष्ण चौधरी	(वडाअध्यक्ष - १३)	श्री
६.	श्री	आत्रेना थापा	(का.का. २५५५५६-१२)	श्री
७.	श्री	त्रियुगा पाठे	(का.का. २५५५५६ - १०)	श्री
८.	श्री	त्रियुगा सुन्दर	(ई)	श्री
९.	श्री	अमरादेव थापा	(ई) DSC	
१०.	श्री	सिका लाल गुरागी	(अ.क.ई)	(३)

(Signature)

प्रस्ताव
१. लेबर क्याम्प सारक्यमा ।

विषय

प्रस्ताव नं: १ मागी बलपान गर्दा यस त्रियुगा नगरपालिका भित्र पर्ने पडिलो र दोस्रो प्राथमिकतामा पर्ने ब्याड आयोजनाको कार्यान्वयनको समयमा लेबर क्याम्पको आवश्यक पर्ने भरडोले डकु लेबर क्याम्प सारक्यको लागी आवश्यक पर्ने जग्गा भूमि त्रियुगा नगरपालिका वडा नं १३ स्थित बडेको बरवा बसपार्क भन्ने सरकारी स्थान जग्गा रहेको हुदा डकु स्थानी तहकाम लेबर क्याम्प र विमोचन सारक्यी गठाराइ गर्न दिने विषय गादिमी ।

(Signature) २०७२/१९६ *(Signature)* *(Signature)* *(Signature)* *(Signature)* *(Signature)*

Municipal letter related to campsite, stockpile, Solid waste and other facilities establishment area



पत्र संख्या :- ०८०१०८१
चलानी नम्बर :-

त्रियुगा नगरपालिका

गाईघाट, उदयपुर
कोशी प्रदेश नेपाल
२०७३

☎ ०२५- ४२०१०८
फ्याक्स ४२०१८५
triyuga.mun@gmail.com

मिति:- २०८०/०८/१८

विषय:- निर्माण को क्रममा आवश्यक जग्गा उपलब्ध गरिएको बारे।

यस त्रियुगा नगरपालिकामा विश्व बैंकको आर्थिक सहयोगमा नेपाल शहरी शासकिय तथा पूर्वाधार आयोजना (NUGIP) मार्फत स्तरोन्नति हुन लागेको यस उदयपुर जिल्ला त्रियुगा नगरपालिका वडा नं. १०, ११, १२, १३ मा पर्ने पहिलो र दोस्रो प्रथमिकतामा परेका १४.५ किलोमिटर सडक खण्डको निर्माणको लागि तपशिल बमोजिमको ठाँउ उपलब्ध गराईने व्याहोरा जानकारी गराईन्छ। तपशिल

१. Camp Site , Stockpile, Solid waste Management, Workshop, Concrete Batch Plant - बरुवा बस पार्कको उतर तर्फको नगरपालिकाको खली जग्गा।
२. Quarry Site Borrow Pits -त्रियुगा र बरुवा खोलामा IEE Approved भएको ठाँउ।
३. Spoil Disposal Site- बिसनपुरमा रहेको Dumping site।
४. Crusher- त्रियुगा- बरुवा दोभानको खाली रहेको जग्गा।


बसन्त कुमार बस्नेत

नगर प्रमुख

बसन्त कुमार बस्नेत
नगर प्रमुख

Municipal executive decision about IEE of Quarry site Approval

बैठक संख्या नं. : १९
आज मिति २०८०।०५।०९ गते सनिबारका दिन त्रियुगा नगरपालिकाका नगर प्रमुख श्री वसन्त कुमार वसन्तज्यूको अध्यक्षतामा तपसिल बमोजिमका वडा अध्यक्ष तथा कार्यपालिका सदस्यज्यूहरुको उपस्थितिमा नगर कार्यपालिकाको आकस्मिक बैठक बस्यो ।

उपस्थिति

सि.नं.	नामथर	पद	दस्तखत
१	श्री वसन्त कुमार वसन्त	नगर प्रमुख	
२	श्री महेश्वरी राई	नगर उपप्रमुख	
३	श्री मनोज कुमार चौधरी	वडा अध्यक्ष वडा नं. १	
४	श्री घमान सिंह खत्री	वडा अध्यक्ष वडा नं. २	
५	श्री लिलक बहादुर कटुवाल	वडा अध्यक्ष वडा नं. ३	
६	श्री यद्वी वसन्त	वडा अध्यक्ष वडा नं. ४	
७	श्री उद्वज कुमार थापा	वडा अध्यक्ष वडा नं. ५	
८	श्री योगेन्द्र बहादुर दाहाल	वडा अध्यक्ष वडा नं. ६	
९	श्री अमर बहादुर वनुवार	वडा अध्यक्ष वडा नं. ७	
१०	श्री राज कुमार राउत	वडा अध्यक्ष वडा नं. ८	
११	श्री राज कुमार वनुवार	वडा अध्यक्ष वडा नं. ९	
१२	श्री किरण पाण्डे	वडा अध्यक्ष वडा नं. १०	
१३	श्री सुमन्त कोईराला	वडा अध्यक्ष वडा नं. ११	
१४	श्री अनिता थापा	का.भा.वडा अध्यक्ष.वडा नं. १२	
१५	श्री नविन कुमार चौधरी	वडा अध्यक्ष वडा नं. १३	
१६	श्री हिम राज मगर	वडा अध्यक्ष वडा नं. १४	
१७	श्री गणेश प्रसाद तिमसीना	वडा अध्यक्ष वडा नं. १५	
१८	श्री मान बहादुर राई	वडा अध्यक्ष वडा नं. १६	
१९	श्री विमल राई	कार्यपालिका सदस्य	
२०	श्री डिल्ली सेर राई	कार्यपालिका सदस्य	
२१	श्री बलदेव विष्टकर्मा	कार्यपालिका सदस्य	
२२	श्री गीता कुमारी उप्रेती	कार्यपालिका सदस्य	
२३	श्री रेखा थापा मगर	कार्यपालिका सदस्य	
२४	श्री रेणुका श्रेष्ठ	कार्यपालिका सदस्य	
२५	श्री सुनिता गजमेर	कार्यपालिका सदस्य	
२६	श्री कुन्ती परियार	कार्यपालिका सदस्य	

सचिव

श्री विष्णु भक्त सिग्देल

प्रमुख प्रशासकीय अधिकृत

१४	अनिता वनुवार	का.स.	आ.स्वा.से.के.शाहीखोला	६ महिना
१५	इन्दिरा बि.क.	का.स.	आ.स्वा.से.के.मैरुन	४ वर्ष+
१६	कमला आचार्य	का.स.	आ.स्वा.से.के.साकेला	४ वर्ष+
१७	कमला भट्टराइ	का.स.	आ.स्वा.से.के.बुढाचौक	१ वर्ष+
१८	शुभ कुमारी राइ	का.स.	आ.स्वा.से.के.बहेडवा	५ वर्ष+
१९	उमा कुमारी बुढाथोकी	का.स.	आ.स्वा.से.के.करमगाछी	२ वर्ष +
२०	सरणमति राई	का.स.	स्वास्थ्य चौकी साउने	५ वर्ष+
२१	तारा तामाङ माझी	का.स.	आ.स्वा.से.के.वाजगरा	३ महिना
२२	कुमारी राइ	का.स.	स्वास्थ्य चौकी चिलाउने	४ वर्ष+
२३	चित्र बहादुर मगर	का.स.	आ.स्वा.से.के.जोते	६ वर्ष +
२४	छलीमाया प्रजा	का.स.	आ.स्वा.से.के.सुकौरा	४ वर्ष+
२५	कमला सुपिजारी	का.स.	आ.स्वा.से.के.सुकौरा	३ वर्ष+

स्वीपर:

सि.नं.	नाम थर	पद	कार्यरत संस्था	कैफियत
१	राजेश मरिक्	स्वीपर	स्वास्थ्य शाखा	४ वर्ष+
२	नितु मलिक	स्वीपर	स्वास्थ्य चौकी जोगीदह	३वर्ष+
३	ललिता मरिक्	स्वीपर	स्वास्थ्य चौकी देउरी	४ वर्ष+
४	इधरी सवा	स्वीपर	स्वास्थ्य चौकी भुमरसुवा	४ वर्ष+

निर्णय नं.४८३

त्रियुगा नगरपालिकाको मुख्य आन्तरिक स्रोतको रूपमा रहेको त्रियुगा, बरुवा र सुनकोशी नदीको नजिकस्थ पदार्थको बिक्रि कर संकलनको लागि प्रारम्भिक वातावरणीय परिक्षण (IEE) स्वीकृत गर्नु पर्ने प्रावधान रहेकोमा पत्राचार जियो सोलुसन प्रा.लि., विराटनगरसँग IEE का लागि कार्यालयबाट सम्झौता भएकोमा सो फर्मबाट पत्र भएको कार्यसूची तथा IEE को तपसिल बमोजिमको परिमाण स्वीकृत गरी नियमानुसार ढेका प्रकृत्यामा जानको लागि आवश्यक व्यवस्था गर्न कार्यालयलाई निर्देशन दिने निर्णय गरियो ।

तपसिल

प्याकेज १: बरुवा खोला र चुनकोशी नदी

उत्खनन् क्षेत्र / प्लट नं.	क्षेत्र	वार्षिक निकालन सकिने		वार्षिक जम्मा हुने घ.मि.	वार्षिक निकालन सकिने घ.मि.	दैनिक निकालन सकिने घ.मि.
		क्षेत्रफल वर्ग मि.	गहिराई मि.			
१	मुस्ताग छहरा (बाँसगरा)	३१७८२	०.६	२८६०३.८	९४३४.६	३४,३१३३३
२	दिम्की	४१६४४	०.६	४१६४३.२	-	७६,२०४४४
३	साकिला	४३३०८	०.६	४११७७.३	-	६४,४१३३३
	जम्मा	११६७३४		११६९०४.३	९४३४.६	१६७,९३०००

प्याकेज १: बरुवा खोला / चुनकोशी नदी

क्र.सं.	उत्खनन् गरिने स्थान	ड्रिङा	सावेल	आलुवा	माटो	अनुमानित राजस्व(रु.)
१	मुस्ताग छहरा (बाँसगरा)	३१७८२	६३४७	-	-	३३४,९३०,३४४
२	दिम्की	४१६४४	२०२७४	-	-	४१०,७०३,६३३
३	साकिला	४३३०८	२२३१९	-	-	४३०,७४६,३४६
	जम्मा	११६७३४	४८९४०	-	-	१,१७६,३८०,३२३

Handwritten signatures and initials below the first table.

प्याकेज २: त्रियुगा खोला

उत्खनन् क्षेत्र / प्लट नं.	क्षेत्र	वार्षिक निकालन सकिने		वार्षिक जम्मा हुने घ.मि.	दिगो रुपमा वार्षिक निकालन र.किने आपर्जाब घ.मि.	दिगो रुपमा दैनिक निकालन सकिने आपर्जाब घ.मि.
		क्षेत्रफल वर्ग मि.	गहिराई मि.			
१	बोडे	३८१९०	०.६	२२९१४	११४४७	४२,४३३
२	उत्तराई टोल	४७३७६	०.४	२३६३८	९४४४.२	३४,०११
३	दिम्की	१९७९७	०.६	१९७९७.२	४७४१.२८	१७,४९९
४	बसपाके	२२४४६	०.६	१३४७३.६	-	१९,९६६
५	त्रियुगा चुनकोशी दोभान	४२९६९	०.४	२६४८४.४	४३६९.४४	३९,२३३
६	सावेलिनि (त्रियुगा-दिम्की खोला दोभान)	६४६४९	०.४	४२९२९.४	१७१७.६	६३,४६६
७	जोगिहरा (त्रियुगा-केग दोभान)	६६४०७	०.४	४३४३४.४	१७३०१.४	६४,०७७
	जम्मा	३४३०४४		१८४४४.३	७६११९.९२	३८१,९६३

Handwritten signatures and initials below the second table.

प्याकेज २: त्रियुगा खोला

प्लट नं.	उत्खनन् क्षेत्र	वार्षिक जम्मा हुने घ.मि.	ड्रिङा	सावेल घ.मि.	आलुवा घ.मि.	अनुमानित राजस्व ड्रिङा/सावेलबाट रु.	अनुमानित राजस्व आलुवाबाट रु.	कुल राजस्व प्लट अनुसार रु.
१	बोडे	११४४७	-	११४४७	-	६६३२२३८	-	६६३२२३८
२	उत्तराई टोल	९४४४.२	१३१४	६१४१.२	-	२३७३७७	-	२३७३७७
३	दिम्की	४७४१.२८	-	४७४१.२८	-	११७४४४०	-	११७४४४०
४	त्रियुगा बसपाके	४३६९.४४	-	४३६९.४४	-	१३३२३७	-	१३३२३७
५	दोभान	१०४६३.६	-	१०४६३.६	-	२२४४७३०	-	२२४४७३०
६	सावेलिनि (त्रियुगा-दिम्की खोला दोभान)	१७१७.६	-	१७१७.६	-	३६३६४३३	-	३६३६४३३
७	जोगिहरा (त्रियुगा-केग दोभान)	१७३०१.४	-	१७३०१.४	-	३६४३६४	-	३६४३६४
	जम्मा	७६११९.९२	१३१४	४४०६३.२	-	७६,७६,४३७	-	७६,७६,४३७

Handwritten signatures and initials below the third table.

Public Consultation meeting minutes

आज मिति २०८०/०६/११ गतेका दिन यल त्रियुगा नगरपालिकाका
स्वचालन भन्नेको नेपाल शासकीय शासकिय प्रबन्धन आयोगका
(NUGIP) अन्तर्गत नगरपालिका द्वारा खगेर गडिबो गाउँपालिका
गाउँ पञ्चायत, मित्रको वडा नं. ११ वडा कार्यालय आगाडीको सडक
नाबोको सामाजिक, आर्थिक, भौतिक तथा वातावरणीय जानकारी
नयाँ अध्ययन गर्न आएका (Contact. Name - V. (Municipality)
त्रियुगा नगरपालिका, ११ नं वडा कार्यालय उपतिथि र त्रियुगा
जिल्ला विकास समिति यल नगरपालिका कार्यालय गरियो।

क्र.सं	नाम	पद	हस्ताक्षर
१)	सुब्बाजोशी	सहायक	
२)	जगत प्रसाद नेम्कि	सहायक	
३)	शेरा बहाल		
४)	कृष्ण शर्मा		
५)	गिताहा देवी चौधरी		
६)	सुष्मा कुमारी		
७)	सुब्बाजोशी		
८)	गिताहा देवी चौधरी		
९)	अभिषेक शर्मा		
१०)	सुब्बाजोशी		
११)	निर्मला कुमारी		
१२)	विष्णु कुमारी	१५१-२५६७	
१३)	विष्णु		
१४)	देव शर्मा चौधरी		
१५)	अमरेश साह	DSC Engineer	
१६)	विष्णु शर्मा		
१७)	विष्णु शर्मा	(अ.स. इ.स.स.)	
१८)			

प्रस्ताव रक
१) गाउँपालिका गाउँ पञ्चायत वडा ११ नं वडा कार्यालय
आगाडीको सडकको विस्तृत परियोजना (PPR)
अध्ययन सम्पन्नमा।
२) सडक अख्तियार क्षेत्र (ROW) सम्पन्नमा।

निर्णय रक
१) प्रस्ताव नं. १ माथी बलपत्र गर्दा यस सडकको विस्तृत
परियोजना परियोजना (PPR) अध्ययन गर्दा भए सडकको
वातावरणीय तथा सामाजिक आन्वेषण योजना बारे बलपत्र
गर्ने र अख्तियार क्षेत्र (ROW) रकम गर्ने यस क्षेत्रका स्थानीय
वाषिण्डाकले सहयोग गर्ने गरी निर्णय गरियो।

२) प्रस्ताव नं. २ माथी बलपत्र गर्दा यस सडकको सडक
अख्तियार क्षेत्र (ROW) लाई त्रियुगा नगरपालिका ले २०८५मा
वनापको सडक मापकन २०८५ क्रममा सम्पन्न गर्ने निर्णय
गरियो साथै बलपत्रको क्रममा कुनै डिजाइनको आवश्यकता
तथा वातावरणीय प्रभाव तहको निर्णय गरियो।

सिंह
शर्मा
शर्मा

आज मिति २०२०-४-६ शुक्रवार दिन यत्र त्रियुगा नगरपालिकाका संचालन गर्नेहरूको नेपाल ग्राहरी शासकिय पुर्तकार आयोगना (NUGAP) अन्तर्गत नगरपालिका द्वारा इन्जिनियर गारिबको फुलचोक देविब नेप जी थक सडक वेदोषका त्रियुगा कौरीडा बाटोको सामाजिक आर्थिक तथा उचित योजनाकारीको लागि अध्ययन गर्न आएका वेदोषका र यस नगरपालिकाको सडक कार्यना प्रतिनिधि र स्थानीय बासिन्दा विद्य इनाफस गभिया

अध्ययन उपस्थिति

दिन	नाम	ठेगाना	पद	सम्पर्क नं	हस्ताक्षर
१	तारा प्रसाद चौबारी	चमचौड	रेकर्डिस्त		
२	इरे बाल चौबारी	"	"		
३	श्याम चौबारी	"	श्याम चौबारी		
४	गफ बडाडु इमार्त	"	"		
५	नेवनाशपाल चौबारी	"	"		
६	शंकर जोडा	"	"		
७	कोषराज खडका	"	"		
८	मित बडाडु खडका	"	"		
९	महेन्द्र चौबारी	"	"		
१०	बहादुर चौबारी	"	"		
११	अनिता थापा	"	"		
१२	मोना राय	PSC environmental expert			
१४	अमरसि राण माडव	DSC Engineer			
१६	त्रिनाद पौडेल	सामाजिक सेवार्ता विरा			

सल्लाहकार विषयहरू

१) यस सडक उप-आयोजनाको अन्तर्गतको स्थानिय कारिबिहरूको सडक उप-आयोजना न NUGAP को सामाजिक तथा आर्थिक वातावरणीय उपस्था पन आयोजना, लोडिङ हिस्ता आदि विषयमा जानकारी गराइयो।

१) यस सडक उप-आयोजनाको काम कारबाही स्थिति सुस्त भएको हुँदा सम्बन्धीत निकायहरूमा फिरो लागू गतको लागि पहल गर्ने भन्ने निर्णय गरियो।

२) यस सडक उप-आयोजनाको सडक अधिकार क्षेत्र (R.O.W) आदि सितर कायमागी नगरपालिकाको काम गर्दै आएको हुँदा यस सडक उप-आयोजना सितर कुनै पनि चलायन, तयती भन्ने निर्णय गरियो।

३) यस सडक उप-आयोजना अन्तर्गतको होल प्लानि हलगा प्रविष्ठा हिंसा, नौल हिंसा, कुरणी पूर्णेशन आदि जस्ता सडकहरू स्वामी तयारको र कलिले काही धेरै लु लाग्दा, स्थिति फिरो लाग्दा सम्बन्धि निवेदनहरू, होल विकाश संस्था मा आउने गरेको पाइयो।

अनुमोदित
 (Signature)
 (Signature)
 (Signature)

आवृत्ति दिने २०२०/०९/१६ गतेका दिन १२४ डिग्रिया नगरपालिकाका संचालन प्रहरेको नेपाल शहरी आवागमन सर्वोत्तर आयोजना (NUGAP) अन्तर्गत नगरपालिका द्वारा खनौट गरिएको फलजोड रोड सि.सि. चोड हाड कोर्रोल डिग्रिया नोडिअर कोडो यामजोड कुपुडि तथा भोजिड जातकारीको लागि अड्डन गर्न शारदा गेली र अरु नगरपालिका १२ र वडा समालेभ प्रतिदिदी र सञ्चार प्रवाहकार रोल बासिन्दा किय खण्डन गरियो।

क्र.सं.	नाममा	ठेगाना	पद	हस्ताक्षर
१.	श.क. बहादुर कुटापत	वि. १५१९२		श.क.
२.	गोहरे सुभा	-		गोहरे
३.	शुभेरा सुभा	-		शुभेरा
४.	गोपना कुमारी डाई	-		गोपना
५.	सुम कुमारी कुमल	-		सुम
६.	कविता साठ	-		कविता
७.	गिता राण	-		गिता
८.	गिता राण	-		गिता
९.	उत्तर वन्दि प्रजापार	-		उत्तर
१०.	पानु राण	-		पानु
११.	मने कोट	-		मने
१२.	माया कोट	-		माया
१३.	गोबिन्द सुभा	-		गोबिन्द
१४.	लव ठकाल	-		लव
१५.	सुम कुमारी कुमल	-		सुम
१६.	कविता राण	-		कविता
१७.	अमरदिप नाथ गदव DSC Engineer	-		अमरदिप
१८.	गोपना राय DSC environmental expert	-		गोपना
१९.	विनायक पौडेल का मागिने कर्मचारी विना DSC	-		विनायक

निर्वाह तथा खनजोडका विषयहरु:

१. यस खण्ड उप आयोजना अन्तर्गतको स्थानीय बासिन्दा कोडो यस (NUGAP) आयोजनाको तारेमा जातकारी गरियो।

१) यस खण्ड उप आयोजना अन्तर्गत यस क्षेत्रको स्थानीय बासिन्दाको लागि अधिकतम सम्बन्धी काम गर्ने खर्च सेइया हरको प्राप्ति विहसुमाको लक्षिको सम्बन्ध तथा - खासापिको खासावैसीकरण खण्डको जा तकारी गरियो। साथै यस क्षेत्रमा लक्षिको खिमा, लागुपदवी दुर्भावान - खण्डको काम होला विकास संस्था र लागुपालिकाको पाले गरेको हुंदा यस क्षेत्रमा खरेखु कागाजा खाँचु सिमान्त सम्बन्धी सुझाव मात्र लागुपालिका, होला विकास संस्थागा फल हुने गरेको पाइयो।

२) यस खण्ड उप आयोजना खण्ड अधिकार क्षेत्र (२०३) ट मिटर काममा लागुपालिकाले २०२६ सालमान काममा गरेर खेचालतमा खाएको हुंदा यत खण्डमा कुनै पले घर खरचना खरव विरुवा नपने हुंदा निर्माण को काममा स्थानीय बासिन्दाको कुनै पले वाधा विरोध गर्ने हुंदा अने खर्च खर्चमा त निर्णय गरियो।

गोपना राय

आपड प्रिति २०२०/०५/१२ गतेका दिन चमर शिमुगा
 हाड पाकिमा/काकाका अरुको तेपाक सड है मालकि-
 य पुका चार आका जका (NUCZP) अरुगत चमर मिलिहा-
 देको रापचरु अरुवेतको चरसफा सडको सामाजिक
 तथा वातावरणिय व्यवस्थापन घोषणा अलाउतको बागी
 यत सेत्रको स्थापिपु हउ नगा तय्यागु नरुणकवा तथा
 राप सुमाय बेकलन गतेकापु स्मपदा गरियो ।

क्र।न विषयको

१.	विठ्ठल भण्ड	चमरसडय शिमुगा -११	
२.	रमेश चन्द	हुंगाता स्थापिपु वाली	
३.	सतिला कुमाल	" " "	सतिला
४.	सुरकुष देवी देवकोटा	" " "	सुरकुष
५.	सुमिश्रा भण्ड	" " "	सुमिश्रा भण्ड
६.	तारा सिरी	" " "	
७.	पुर्जिता शाहि	" " "	पुर्जिता
८.	मोना शाहि	" " "	मोना
९.	नरेश पुष्पाचर	" " "	नरेश
१०.	सुन्दरभाया शर्मा	" " "	
११.	उदय चण्ड	" " "	
१२.	सिद्धा सिवाजी	" " "	सिद्धा
१३.	सिला मगर	" " "	सिला
१४.	रापचरु करतल	" " "	रापचरु
१५.	मेनुका राई	" " "	मेनुका
१६.	विजा मीर	" " "	विजा
१७.	सुखभाया राई	" " "	सुखभाया
१८.	सतिला कुमाल	" " "	सतिला
१९.	सिला सुन्दर	" " "	सिला
२०.	भावना राई	" " "	भावना
२१.	सुर्जिता कुमाल	" " "	सुर्जिता
२२.	सिला राई	" " "	सिला
२३.	अमरपिपु वणा माहेश	DSC Engineer	Famer
२४.	सिला Rega environmental engineer		
२५.	सिला पोखरा सामाजिक विकासिका	DSC	सिला

प्रस्तावहरु:

१. सिटिहालेदेसिपे रापचरु अरुवेतको चरसफाको, सडको विसृत परिशोधना प्रानिदेन (DPR) अद्ययतन अरुअरु चाना।
२. सडको आधिकार क्षेत्र (COJ) अरुअरु चाना।
३. विविध

निर्णयहरु:

१. उपरोक्त प्रस्ताव नं. १ माथि अरुअरु गरुन गरुन सडको विसृत परिशोधना प्रानिदेन (DPR) अद्ययतन गरुन यस सडको वातावरणिय तथा सामाजिक व्यवस्थापन घोषणा वारे अरुअरु गरुन र आपडको तय्यागु अरुअरु गरुन पदसु भेसका स्थापिपु वासिहाले अरुअरु गरुन अरु निर्णय गरियो।
२. उपरोक्त प्रस्ताव नं. २ माथि अरुअरु गरुन यस सडको सडको आधिकार क्षेत्र (COJ) अरुअरु गरुन शिमुगा न.पा.ने २०६५ मा नै शिमुगा न.पा.को सडको अरुअरु मापको २०५५ वनाडी र सिरकापुमा गडेको हुंदा र अरु सडको २०६५ देसिने रावेक open रहेको कुनै पनि चरसफा अरुअरु सडको मा तपने हुनाले यस सेत्रको स्थापिपु वासिहाले अरुअरु निर्माणको अरुअरु कुनै पनि वादा अरुअरु अरुअरु अरुअरु अरुअरु निर्णय गरियो।

सिला

Consultation meeting with IP and Dalit community

आज मिति २०२०।०६।०८ गतेका दिन मध्य त्रियुगा नगरपालिकामा संचालन भईरहेको नेपाल सडक शासकिय एकेण्ड प्रायोजन (NUGPPP) अन्तर्गत नगरपालिका द्वारा धनीय गरिएको गण्डक सिवलिय होल आदर्श होल समाईले डाँडा मसमोहन स्कूल देवी इरमगाकी गण्डक विकेल सडक लि.न.पा.१० कसोको सापडीब आर्षेडि, पोर्निडे तथा वातावरणीय जानकारीको लगी अहम गरि आरडा (७०९५-९०९६-११०७) त्रियुगा नगरपालिका र १० र्का का प्रतिनिधी र स्थानीय समाजसेवीको सहकार्य सरेरका होल कासोकी धनजल गरियो।

अस्वीति

क्र.सं.	नामधर	संस्था	पद	हस्ताक्षर
	सुनिल नि.कु.	लि.न.पा.१०	डा.अध्य	[Signature]
	सुनिल अरुण			[Signature]
	उमेश क. नि.कु.			[Signature]
	इरम गौरी			[Signature]
	अनुरा ग. सुवेदी			[Signature]
	कल्पना अरुण			[Signature]
	विना प्रसा			[Signature]
	प्रविषा वि.कु.			[Signature]
	सन्ध्या नि.कु.			[Signature]
	विमला नि.कु.			[Signature]
	राम क. नि.कु.			[Signature]
	पुष्पिता वि.कु.			[Signature]
	सुवेदी राई			[Signature]
	विना प्रसा - समाजिक विकास DSC			[Signature]
	धनजल			[Signature]

१। प्रत्येक समाजसेवीको अन्तर्गत गण्डक सिवलिय होल आदर्श होल मसमोहन स्कूल देवी इरमगाकी सडक अन्तर्गतको सिवलिय होल परियोजना प्रतिवेदन तयारीको लगी आगम्य सम सुझाव संचालन गर्ने काम सम्पन्न गरियो।

२०२०

१) मध्य सडक प्रायोजनमा हुने डिभिजनको विचार कसोको र हुने पाते का सरेरचना सडक फिज गर्ने।

२) प्रत्येक सडक प्रायोजन करिप र गुणवत्ता बन्नुपर्ने र यस्ता सिवलिय होल काम हुनु हुने।

[Signatures]

आज मिति २०७०/०५/१२ उतिका दिन मध्य त्रियुगा नगरपालिका
संचालन भेडेटो नेपाल राजी शासकिय प्रशासन प्रशासन
(NUGTP) अन्तर्गत नगरपालिका द्वारा छत्रोड त्रियुगा प्रशासन
सुद) (अगाडी) को सडक दुई साइला जाने कालो
सावनीड, आषाढि, सैमि र वातावरणीय जातकारीको लागि अलग
जर्न आएका (Botek-Gore JV-Consultant) त्रियुगा नगरपालिका
बाट नं ११ का जनप्रशासकीय र साइला गेलका सडक अखिली
समुदाय किच बलफल गरियो।

क्र.सं.	नामगा	हलफ
१	गिता बाई	१
२	कुला बाई	२
३	दिल कु. बाई	३
४	कदपती धापा	कदपती
५	विष्णु प्राप्ता रुकुल	बापु
६	कदपती बस्नेत	कदपती
७	बसन्त प्राप्ता	बसन्त
८	विष्णु प्राप्ता	विष्णु
९	मुना बाई	मुना
१०	मेनुका बाई	मेनुका
११	दिल कु. बाई	दिल
१२	जाजुका बाई	जाजुका
१३	उमा बाई	उमा
१४	खिन बाई	खिन
१५	सोमना बाई	सोमना
१६	सामिला बाई	सामिला
१७	मेख बाई	मेख
१८	सोमना बाई	सोमना
१९	लोक नारायण श्रेष्ठ	लोक
२०	दिल बाई	दिल
२१	नारायण श्रेष्ठ	नारायण

२२	दुर्गा देउजा	दुर्गा
२३	रेखा राता मगर	रेखा
२४	दिनेश श्रेष्ठ	दिनेश
२५	सोमना बाई	सोमना
२६	दिलमाया श्रेष्ठ	दिल
२७	गोबिन्दा श्रेष्ठ	गोबिन्दा
२८	सोमना बाई	सोमना
२९	शोभापति श्रेष्ठ	शोभा
३०	पुन पर्सिया	पुन
३१	विष्णु श्रेष्ठ	विष्णु
३२	सुमना बाई	सुमना
३३	नारायण श्रेष्ठ	नारायण
३४	नारायण श्रेष्ठ	नारायण
३५	सुमना बाई	सुमना
३६	सोमना बाई	सोमना
३७	राज कु. बाई	राज
३८	रेखा श्रेष्ठ	रेखा
३९	बालकृष्ण	बालकृष्ण
४०	विष्णु श्रेष्ठ	विष्णु
४१	दिनेश श्रेष्ठ (क. वि. प्र. न.)	दिनेश

प्रशासनिक बलफलका विषय

- १) आयोजनाबाट जातकारी सञ्चयमा। यस प्रगती गेलको (सुद) (अगाडी) सडक दुई साइला जाने कालो कालो मध्य साइला गेलको बाई आदिगेली सावनीड जातकारी गरियो।
- २) सावनीड तथा वातावरणीय मापक्यापन तथा जातकारी लागी सुझाव सङ्कलन सञ्चयमा। यस सडकको सामाजिक र वातावरणीय मापक्यापनको तथा जातकारीको लागि सावनीड तथा सुझाव सङ्कलन लिने काम सञ्चयमा गरियो। साथै दुई विधिको सामाजिक तथा वातावरणीय अलग गरे भन्ने निर्णय गरियो।
- ३) विधिको सञ्चयमा सङ्कलन गर्ने सञ्चयमा। यस सडक आयोजनाको फल होल कालोमा सामाजिक सुझावको लागि सामाजिक समागतो सञ्चयमा सङ्कलन गरियो।

नेपाल नगरपालिका (२) नारायण श्रेष्ठ

Stakeholder Consultation
Nepal Urban Governance and Infrastructure Project (NUGIP)
Triyuga Municipality
Gaigat, Udayapur

ESIA and ESMP of गाईघाट गाउँपालिका क्षेत्रको सडकहरू.....Road

Attendance Sheet of FGD

Date: २०२०/५/२४ Venue: त्रियुगा नगरपालिका Ward No. ११

SN	Name	Organization/Title	Contact no.	Signature
१	बसन्त कुमार खड्गे	गाईघाट प्रमुख त्रियुगा		
२	महेन्द्र शर्मा	गाईघाट प्रमुख		
३	विष्णु शर्मा	गाईघाट प्रमुख		
४	निर्मल शर्मा	गाईघाट प्रमुख		
५	सुमन शर्मा	गाईघाट प्रमुख		
६	विष्णु शर्मा	गाईघाट प्रमुख		
७	विष्णु शर्मा	गाईघाट प्रमुख		
८	विष्णु शर्मा	गाईघाट प्रमुख		
९	विष्णु शर्मा	गाईघाट प्रमुख		
१०	विष्णु शर्मा	गाईघाट प्रमुख		
११	विष्णु शर्मा	गाईघाट प्रमुख		

Suggestions/Recommendations:

- सडकको आवश्यकता प्रतिक्रिया दिना भएकोले यथासंभव चाँडो काम सुरु गर्न पर्ने।
- त्रियुगा नगरपालिका क्षेत्रमा नै त्रियुगाको सडकहरूको मापदण्डको अभावमा नभएर केही अतिरिक्त मापदण्डको आवश्यकता भएको हुँदा कुनै औसतमा खर्चनामा रूपमा हेर्ने।
- कारणले प्रतिक्रिया दिनु भएको हुँदा consultant र सिडको बीचमा टाटो छिटो गरिनु पर्ने।

Stakeholder Consultation
Nepal Urban Governance and Infrastructure Project (NUGIP)
Triyuga Municipality
Gaigat, Udayapur

ESIA and ESMP of गाईघाट गाउँपालिका क्षेत्रको सडक.....Road

Attendance Sheet of FGD

Date: २०२०-०५-२४ Venue: हिमाली टोल बिकास संस्था Ward No. ११

Attendees

SN	Name	Organization/Title	Contact no.	Signature
१	विष्णु शर्मा	गाईघाट प्रमुख		
२	विष्णु शर्मा	गाईघाट प्रमुख		
३	विष्णु शर्मा	गाईघाट प्रमुख		
४	विष्णु शर्मा	गाईघाट प्रमुख		
५	विष्णु शर्मा	गाईघाट प्रमुख		
६	विष्णु शर्मा	गाईघाट प्रमुख		
७	विष्णु शर्मा	गाईघाट प्रमुख		
८	विष्णु शर्मा	गाईघाट प्रमुख		
९	विष्णु शर्मा	गाईघाट प्रमुख		
१०	विष्णु शर्मा	गाईघाट प्रमुख		
११	विष्णु शर्मा	गाईघाट प्रमुख		

Suggestions/Recommendations:

- सडकको मापदण्डको अभावमा नभएर केही अतिरिक्त मापदण्डको आवश्यकता भएको हुँदा कुनै औसतमा खर्चनामा रूपमा हेर्ने।
- कारणले प्रतिक्रिया दिनु भएको हुँदा consultant र सिडको बीचमा टाटो छिटो गरिनु पर्ने।

Stakeholder Consultation
 Nepal Urban Governance and Infrastructure Project (NUGIP)
 Triyuga Municipality
 Galghat, Udayapur

ESIA and ESMP of गौरीबाट, सिविलकोट, चोक, आदिवासी, चोक, आदिवासी Road
 प्रगतिशैल चोक (गौरीबाट) आदिवासीको बाइकोट, सिविलकोट
 Attendance Sheet

Date: २०७०/०२/१४ Venue: त्रियुगा न.पा.स. कार्यालय Ward No. १३
 Name: सविता कुमारी चौधरी
 Organization/Occupation: प.स. आदिवासी त्रियुगा न.पा.स. कार्यालय - १३
 Comments/Suggestions:

वडा नं. १३ धर्मशोक नु वडाभित्रा स्थिति उपकरण अन्तर्गतको
 माइलर टेम्पलाला नजारे वारी तिरुव रैडको इन्फ्रस्ट्रक्चर जमी
 विग्रीवा कार्य जयका की कमी गरिने जमी भुमि किरिमको समया
 सुभे ८ को कार्य गुणवत्ता रूपमा बन्नु पर्ने र को कार्य मा
 गभरी माको समयादेउ बाईकोट नु यविद्यो हुने र १ माको
 प्रगती बोलको वा इकायवसा नारीमा वापि भुमि किरिम
 अप्पारी मापदण्ड हुने र उक्त को वारी कोडे २०७०
 खदीको बासिहाइकादि विचार गभरी अनुमति हुने र

Signature: [Signature]
 सविता कुमारी चौधरी
 वडा न.पा.स. १३



Stakeholder Consultation
 Nepal Urban Governance and Infrastructure Project (NUGIP)
 Triyuga Municipality
 Galghat, Udayapur

ESIA and ESMP of गौरीबाट, सिविलकोट, चोक, आदिवासी, चोक, आदिवासी Road
 Attendance Sheet

Date: २०७०/२/१२ Venue: त्रियुगा न.पा.स. कार्यालय Ward No. १०
 Name: किरिता पाठे
 Organization/Occupation: वडा अध्यक्ष त्रियुगा न.पा.स. कार्यालय
 Comments/Suggestions:

१. सडक बनाउने कार्य विषय आएको हुँदा यथा सिधु चाँडो सडक
 बनाउनु पर्नेछ।
२. युवावर्ग र विद्यार्थीको आपतकाल अड्डाको स्थापना
 बनाउनु पर्नेछ।
३. यस गौरीबाट सिविलकोट चोक आदिवासीको बजारको
 मनमोहन कछुवा टेम्पलको कम्प्याउली गौरीबाट किरिमको बाइकोट
 बाइकोट (२०००) बाइकोट अक्का सेज १० मिटर आएको हुने
 पले भौतिको हानि हुने छैन। र कुनै पनि विवाद हुने
 छैन।
४. यो सडक चाँडो बनाईने यस क्षेत्रको ब्यापारिक कारिहाले
 चाँडै बाहान पत्रनेछन कपी, जापाहा, लडनेछ।

Signature: [Signature]

Stakeholder Consultation
Nepal Urban Governance and Infrastructure Project (NUGIP)
Triyuga Municipality
Gairaud Udayapur

ESIA and ESMP of गोर्खा रस्ता प्लाजिड (वडा नं: ११) अगाडी की गटौ Road

Attendance Sheet of FGD

Date: २०२०/०६/०२ Venue: महिल समुह Ward No. १०

SN	Name	Organization/Title	Contact no.	Signature
१	कल्पना खत्री		९८०५९९२९	[Signature]
२	विष्णु शर्मा			[Signature]
३	सुश्री मिश्र			[Signature]
४	सुशी शर्मा		९८९६१९०४५	[Signature]
५	विष्णु खत्री		९८९६०९६२	[Signature]
६	पञ्च देवि शर्मा			[Signature]
७	कविता शर्मा			[Signature]
८	विष्णु मिश्र			[Signature]
९	सुश्री खत्री		९८९६०६०४५	[Signature]
१०	सुश्री शर्मा		९८२१६९०६२	[Signature]

Suggestions/Recommendations:

पस रस्ताको अगाडी सिमरौको रस्ता अगाडी लम्वु हागे सवलय पुर्ण रूपमा समर्पण गर्दा दिवा हरिता मार्ग सुचारु गरी दिन र्नु हागे अनुपेय नर्दा। शर्दा। एकर एको दिवा मार्ग गरी हागेलाइ सवलय रूपमा हिटुवुल गन लम्वु अडिक्ने हुन पागे अनुपेय गर्दा। रस्ता अगाडी लम्वु गिन.पी.१० रस्ताको अगाडी

Stakeholder Consultation
Nepal Urban Governance and Infrastructure Project (NUGIP)
Triyuga Municipality
Gairaud Udayapur

ESIA and ESMP of गोर्खा रस्ता प्लाजिड (वडा नं: ११) अगाडी की गटौ Road

Attendance Sheet of FGD

Date: २०२०/०६/११ Venue: जी किंग टिम क्लब Ward No. ११

SN	Name	Organization/Title	Contact no.	Signature
१	[Signature]			[Signature]
२	Ramesh Dahal		९८५२३७६०११	[Signature]
३	सुश्री शर्मा		९८९६०९६२	[Signature]
४	विष्णु शर्मा		९८९६०९६२	[Signature]
५	सुश्री शर्मा		९८९६०९६२	[Signature]
६	विष्णु शर्मा		९८९६०९६२	[Signature]
७	विष्णु शर्मा		९८९६०९६२	[Signature]
८	विष्णु शर्मा		९८९६०९६२	[Signature]
९	विष्णु शर्मा		९८९६०९६२	[Signature]
१०	विष्णु शर्मा		९८९६०९६२	[Signature]

Suggestions/Recommendations:

- १) रस्ताको अगाडी सिमरौको रस्ता अगाडी लम्वु हागे सवलय पुर्ण रूपमा समर्पण गर्दा दिवा हरिता मार्ग सुचारु गरी दिन र्नु हागे अनुपेय नर्दा। शर्दा। एकर एको दिवा मार्ग गरी हागेलाइ सवलय रूपमा हिटुवुल गन लम्वु अडिक्ने हुन पागे अनुपेय गर्दा। रस्ता अगाडी लम्वु गिन.पी.१० रस्ताको अगाडी
- २) बाटो निर्माण गुणस्तर फुके हुनु परे।
- ३) बाटो निर्माणमा गुणस्तर सामानको प्रयोग हुनु परे।
- ४) बाटो निर्माणमा गुणस्तर सामानको प्रयोग हुनु परे।
- ५) गुणस्तर सामानको प्रयोग गरिनुको लागि।

Stakeholder Consultation
Nepal Urban Governance and Infrastructure Project (NUGIP)
Triyuga Municipality
Galsani, Udayapur

ESIA and ESMP of मुस्तापडा, देविचिपानी, चोक सडक, नौवैतोल, त्रियुगा नौवैतोल Road

Attendance Sheet of FGD

Date: 2020-08-06 Venue: नौवैतोल विषय संकाय Ward No: 99

SN	Name	Organization/Title	Contact no.	Signature
1	नेपाल कमिटी को	नौवैतोल	978222666	[Signature]
2	मोदी मोडि	नौवैतोल		[Signature]
3	जगत कुमार			[Signature]
4	जगत			[Signature]
5	जगत			[Signature]
6	नरमान को	नौवैतोल	978222666	[Signature]
7	मुस्तापडा नौवैतोल			[Signature]

Suggestions/Recommendations:

- 1- बाँधी भन्दा बाँधी सुगन्ता दिनेको काम गर्नु पर्ने।
- 2- वस्ती लक्ष्य नभएको काम भन्दा हुनुपर्ने।
- 3- लगापडा भन्दा हुनुपर्ने।
- 4- बाँधी बाँधी गर्ने बाँधी भन्दा बाँधी बाँधी पर्ने।
- 5- बाँधी बाँधी गर्ने बाँधी पर्ने।
- 6- पुरानो सडक निर्माण हुनुपर्ने।
- 7- बाँधी बाँधी गर्ने बाँधी पर्ने।
- 8- बाँधी बाँधी गर्ने बाँधी पर्ने।
- 9- बाँधी बाँधी गर्ने बाँधी पर्ने।
- 10- बाँधी बाँधी गर्ने बाँधी पर्ने।

Stakeholder Consultation
Nepal Urban Governance and Infrastructure Project (NUGIP)
Triyuga Municipality
Galsani, Udayapur

ESIA and ESMP of सिद्धिचोक, देविचिपानी, चोक सडक, नौवैतोल, त्रियुगा नौवैतोल Road

Attendance Sheet of FGD

Date: 2020-08-09 Venue: सिद्धिचोक विषय संकाय Ward No: 99

SN	Name	Organization/Title	Contact no.	Signature
1	नरमान को	नौवैतोल	978222666	[Signature]
2	जगत कुमार			[Signature]
3	जगत			[Signature]
4	जगत			[Signature]
5	जगत			[Signature]
6	जगत			[Signature]
7	जगत			[Signature]
8	जगत			[Signature]
9	जगत			[Signature]
10	जगत			[Signature]
11	जगत			[Signature]

Suggestions/Recommendations:

- 1- काम पारी सम्बन्धी काम हुनु पर्ने।
- 2- बाँधी बाँधी गर्ने बाँधी पर्ने।
- 3- बाँधी बाँधी गर्ने बाँधी पर्ने।
- 4- बाँधी बाँधी गर्ने बाँधी पर्ने।
- 5- बाँधी बाँधी गर्ने बाँधी पर्ने।
- 6- बाँधी बाँधी गर्ने बाँधी पर्ने।
- 7- बाँधी बाँधी गर्ने बाँधी पर्ने।
- 8- बाँधी बाँधी गर्ने बाँधी पर्ने।
- 9- बाँधी बाँधी गर्ने बाँधी पर्ने।
- 10- बाँधी बाँधी गर्ने बाँधी पर्ने।

Municipal documents related to GBV cases

१. हिंसाको प्रकृति विवरण : २०७७-७८




क.स	हिंसाको प्रकार	जम्मा संख्या	कैफियत
	➤ घरेलू हिंसा	३०	
१	शारीरिक	१३	
२	बहुविवाहा	३	
३	गालीगलौज	४	
४	मानसिक यातना	२	
५	चारीत्रिक आरोप	६	
६	स्रोत सुविधाबाट वन्चितीकरण	२	
	➤ सामाजिक हिंसा	१६	
१	कुटुपिट	१	
२	गालीगलौज		
३	बोक्सिको आरोप		
४	जबरजस्ती विवाहा		
५	वाल विवाहा	१५	
	➤ यौन हिंसा	३२	
१	बलात्कार	२१	
२	बलात्कारको प्रयास	२	
३	सामुहिक बलात्कार		
४	सामुहिक बलात्कारको प्रयास		
५	हाडनाता करणी		
६	यौन दुर्व्यवहार	४	
७	अलपत्र	२	
८	गर्भवती भई अलपत्र	३	
९	मानव बेचबिखन		
१०	आत्मा हत्या		
	जम्मा	७८	



१. हिंसाको प्रकृति विवरण : २०७८-७९

क्र.स	हिंसाको प्रकार	जम्मा सख्या	कैफीयत
➤	घरेलु हिंसा	२९	
१	शारीरिक	२९	
२	बहुविवाहा	६	
३	गालीगलौज	७	
४	मानाशिक यातना	८	
५	चारीत्रिक आरोप	२	
६	स्रोत सुविधाबाट बन्चितीकरण	४	
➤	सामाजिक हिंसा	८	
१	कूटपिट		
२	गालीगलौज	१	
३	बोक्सको आरोप	०	
४	जबरजस्ती विवाहा	०	
५	बाल विवाहा	७	
➤	यौन हिंसा	११	
१	बलात्कार	७	
२	बलात्कारको प्रयास	४	
३	सामुहिक बलात्कार		
४	सामुहिक बलात्कारको प्रयास		
५	हाडनाता करणी		
६	यौन दुर्व्यवहार		
७	हत्या		
८	हत्याका प्रयास		
९	मानव बेचबिखन		
१०	अन्य, अलपत्र	११	
	जम्मा	८६ जना	

F/Y  2079186

१. हिंसाको प्रकृति विवरण


क्र.स	हिंसाको प्रकार	जम्मा संख्या	कैफीयत
	➤ घरेलु हिंसा	६८	
१	शारिरिक	२५	
२	बहुविवाहा	५	
३	गालीगलौज	२०	
४	मानशिक यातना	१३	
५	चारीत्रिक आरोप	३	
६	स्रोत सुविधाबाट बन्चितीकरण	२	
	➤ सामाजिक हिंसा	५	
१	कुटपिट	०	
२	गालीगलौज	०	
३	वोक्सको आरोप	०	
४	जबरजस्ती विवाहा	०	
५	वाल विवाहा	५	
	➤ यौन हिंसा	१५	
१	बलात्कार	१३	
२	बलात्कारको प्रयास	२	
३	सामुहिक बलात्कार		
४	सामुहिक बलात्कारको प्रयास		
५	हाडनाता करणी		
६	यौन दुर्व्यवहार		
७	हत्या		
८	हत्याको प्रयास		
९	मानव बेचबिखनको प्रयास	१	
१०	अन्य,अलपत्र	५	
	जम्मा	९४ जना	

नोट सुरक्षावासभिन्न बसेर सेवा लिने संख्या ९४ र बाहिरबाट मनोविमर्श सेवा,कानुनी सहयोग,कानुनी परामर्श,पारिवारी परामर्श जस्ता लगाएको सेवालिनैहरुको संख्या गरी जम्मा ६५ जनाले सेवा पाएका छन भने सुरक्षाआवासमा सेवाग्राहि संग आएको आश्रीत वालवालिकाको संख्या ३० जना गरी यस बर्ष १२४ जनालाई सुरक्षावासभिन्न सेवा प्रदान गरीएको छ ।

Letter from Triyuga Small Town Water supply and Sanitation user Committee

द.नं.१/०५३/०५४

पत्र नं. : १०१५-४२०२४३

 **त्रियुगा साना शहरी खानेपानी तथा सरसफाई उपभोक्ता समिति (संस्था)**

वि.नं.पा.१०, बाबुसा उदयपुर

०८०/०८१

मिति : २०८०/०५/२५

पत्र संख्या : ४


चलानी नम्बर : ४

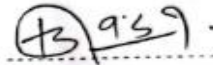
विषय : लागत स्टीमेट गरी पठाईएको सम्बन्धमा ।

श्रीमान् नगर प्रमुख ज्यू,

त्रियुगा नगरपालिकाको कार्यालय, गाईघाट उदयपुर

उपरोक्त सम्बन्धमा तहाँ कार्यालयको च.नं. २५७ मिति २०८०/०४/२९ को पत्रबाट व्यहोरा अवगत भई NUGIP कार्यक्रम अन्तर्गत तहाँ कार्यालयबाट छनीट भएका बडा नं. ११ र १० का विभिन्न सडकहरुमा हाल सञ्चालनमा रहेका खानेपानी पाईपलाईनहरु बन्न नागेका सडकका दायीं बायाँ रहेकामा उक्त पाईपलाईनहरुको लागत स्टीमेट गरी पठाईदिने भन्ने पत्र अनुसार लागत स्टीमेट गराई यसै पत्र साथ संलग्न राखि पठाईएको व्यहोरा पत्रसाथ सादर अनुरोध छ ।






श्रीम बहादुर खड्का
का.वा. अध्यक्ष
का.वा. अध्यक्ष

त्रि.न.पा. कार्यालय
द.नं. ३४६
मिति २०८०/५/२५
शाखा

Letter from NEA for pole relocation

 **नेपाल विद्युत प्राधिकरण**
(नेपाल सरकारको स्वामित्व)
वितरण तथा ग्राहक सेवा निर्देशनालय
कोशी प्रदेश प्रादेशिक कार्यालय धिराटनगर
उदयपुर वितरण केन्द्र
गाईघाट

कार्यालय प्रमुख ०३१-४२२६२९
प्रशासन ०३१-४२२६२९

प.सं.: २०८०/०८१ च.नं.: १६६ मिति : २०८०/०९/०२

श्री त्रियुगा नगरपालिका
त्रि.न.पा.११, गाईघाट

विषय: लागत ईस्टिमेट पठाइएको बारे ।

उपरोक्त सम्बन्धमा तहा कार्यालयबाट प.स. २०८०/०८१ को च.न. २५८ को पत्रअनुसार NUGIP कार्यक्रम अन्तर्गत तथा कार्यालय वाट छनोट भएका वडा न.१०,११ र १२ का विभिन्न सडकको किनारामा हाल संचालनमा रहेका विद्युतिय संरचनाहरूलाई वन्न लागेका नयाँ सडकको दाया बाया किनारामा रहने गरी नयाँ विद्युतिय संरचनाको लागि लागत ईस्टिमेट गरी यसै पत्र साथ संलग्न राखी पठाइएको व्यहोरा पत्रसाथ अनुरोध छ ।

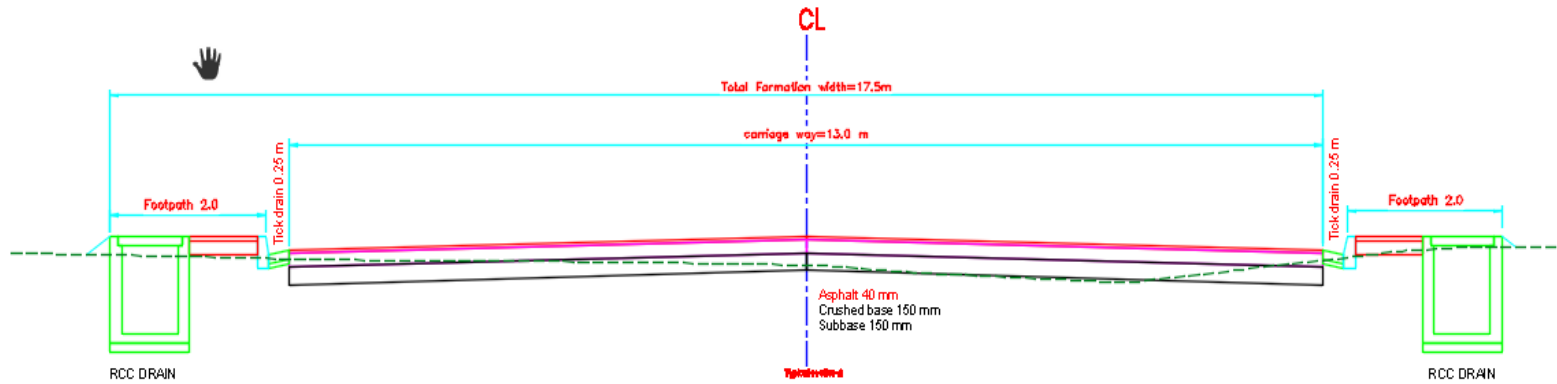
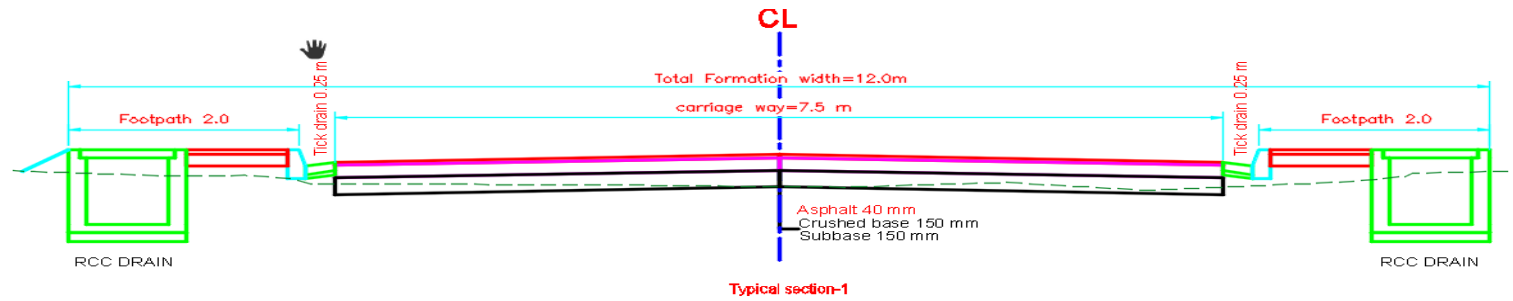
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११/२

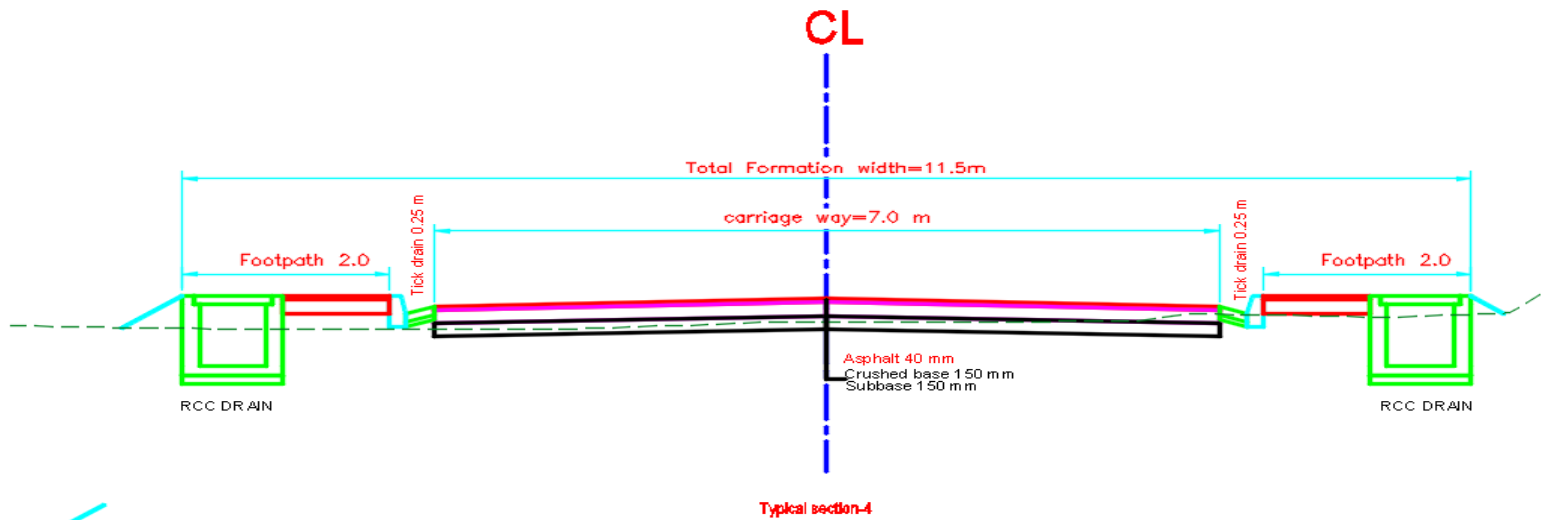
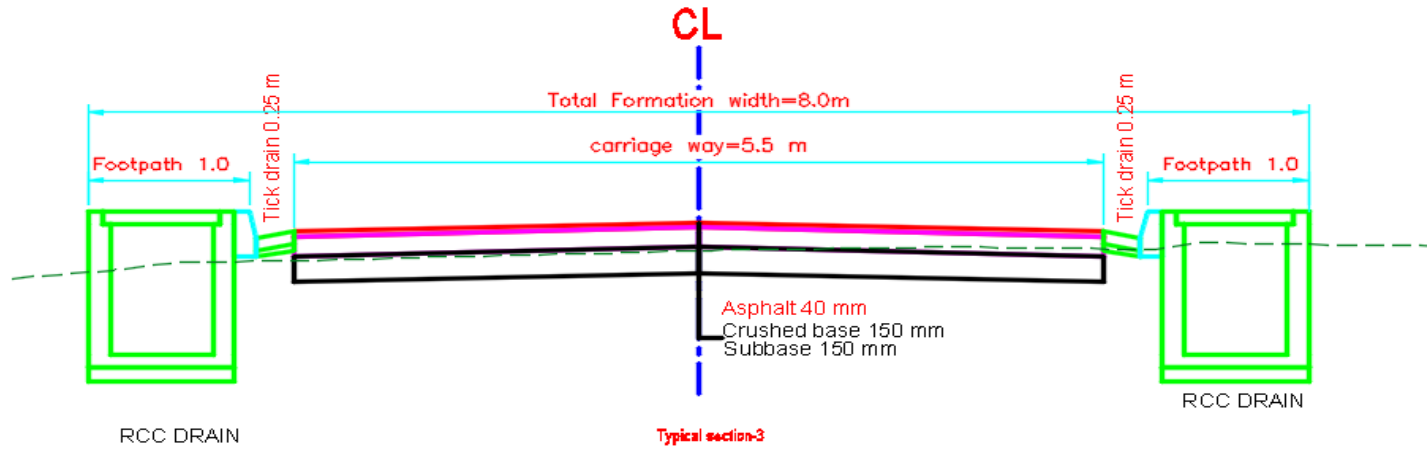
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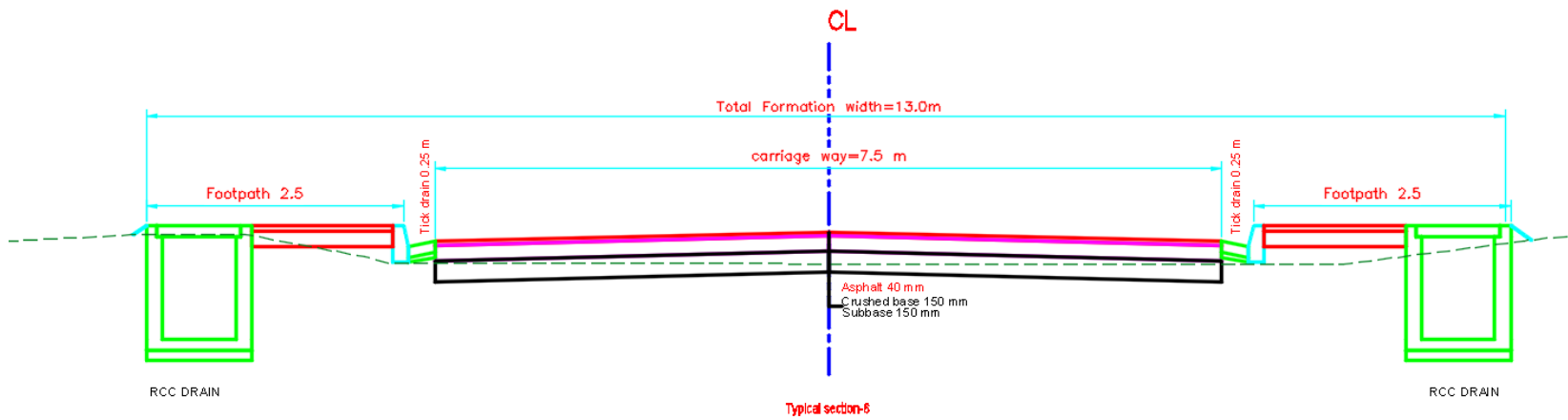
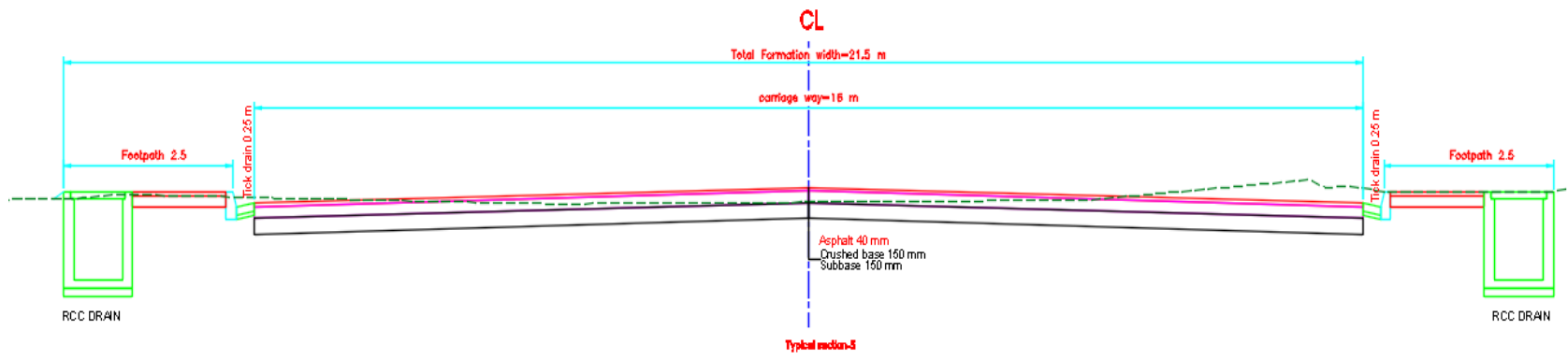
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केन्द्र प्रमुख
केन्द्र प्रमुख

Annex III: Proposed Typical Cross Sections







Annex IV: GoN Permissible Environmental limits/standards

(A) National Drinking Water Quality Standard, 2079 BS

A-1: Mandatory Parameters to be tested

SN	Parameters	Unit	Limits	Remarks
	Physical			
1	Turbidity	NTU	5	
2	pH		6.5 - 8.5	
3	Colour	TCU	5	
4	Taste & odour		Unobjectionable	
5	Electrical Conductivity	μS/cm	1500	
	Chemical			
6	Iron	mg/L	0.3 (3)	
7	Manganese	mg/L	0.20	
8	Arsenic	mg/L	0.05	
9	Fluoride	mg/L	0.50 - 1.50 (Min. - Max.)	
10	Ammonia	mg/L	1.50	
11	Chloride	mg/L	250	
12	Sulphate	mg/L	250	
13	Nitrate	mg/L	50	
14	Copper	mg/L	1	
15	Zinc	mg/L	3	
16	Aluminum	mg/L	0.20	
17	Total Hardness	mg/L	500	
18	Residual Chlorine	mg/L	0.10 - 0.50 (Min. - Max.)	
	Microbiological			
19	E-Coli	(CFU/10 ml)	0	

A-2: Additional Parameters to be tested based on Risk and Requirement

SN	Parameters	Unit	Limits	Remarks
	Physical			
1	Total Dissolved Solids	mg/L	1000	
	Chemical			
2	Calcium	mg/L	200	
3	Lead	mg/L	0.01	
4	Cadmium	mg/L	0.003	
5	Chromium	mg/L	0.05	
6	Cyanide	mg/L	0.07	
7	Mercury	mg/L	0.001	
8	Nitrites	mg/L	3	
	Microbiological			
1	Total Coliform	(CFU/10 ml)	0 (In 95% samples)	

(B) National Ambient Air Quality Standard, 2069 BS

Parameters	Units	Averaging Time	Concentration in Ambient Air, Maximum
TSP	µg/m ³	24 - hours	230
PM ₁₀	µg/m ³	24 - hours	120
PM _{2.5}	µg/m ³	24 - hours	40
Sulfur Dioxide	µg/m ³	Annual	50
		24-hours	70
Nitrogen Dioxide	µg/m ³	Annual	40
		24-hours	80
Carbon Monoxide	µg/m ³	8hours	10000
Lead	µg/m ³	Annual	0.5
Benzene	µg/m ³	Annual	5
Ozone	µg/m ³	8-hours	157

Ref.: Section 62, Number 19, Nepal Gazette, Part 5, 2069/04/29, Notice 2

(C) National Sound Pressure Level, 2069

Microenvironment	Sound Pressure Level, L _{eq} dB(A)	
	Daytime	Nighttime
Industrial Area	75	70
Commercial Area	65	55
Rural Settlement Area	45	40
Urban Settlement Area	55	50
Mixed Settlement Area	63	55
Pristine Area	50	40

Ref.: Section 62, Number 30, Nepal Gazette Part 5, 2069/7/13

(D) Diesel Powered Generator Emission Limits (g/kWh), 2069

Category, (kW)	CO	HC	NO _x	PM
kW < 8	8	1.3	9.2	1
8 = kW < 19	6.6	1.3	9.2	0.85
19 = kW < 37	6.5	1.3	9.2	0.85
37 = kW < 75	6.5	1.3	9.2	0.85
75 = kW < 130	5	1.3	9.2	0.7
130 = kW < 560	5	1.3	9.2	0.54

Ref.: Section 62, Number 30, Nepal Gazette Part 5, 2069/7/13

The minimum height of the chimney should be maintained not less than 11m for the industrial boiler utilizing solid or liquid fuel.

Annex V: Air, Noise, Water Quality Test Reports



**ENVIRONMENT MANAGEMENT
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Email: emas@emas.com.np, emasenv@gmail.com
Website: emas.com.np

ANALYSIS REPORT FOR AMBIENT AIR QUALITY

Report Number	26A/080-81		
Sample Number	17A/080/81		
Client	Bosphorous Technical Consulting Corp (BOTEK), GOEC Nepal P. Ltd		
Sampling location	Triyuga Municipality Ward No. 11, Gaighat townplanning area Road Section (1.5Km)	GPS point: 26°47'30.40"N 86°41'57.31"E	
Project	Upgrading of different inter connected Road Sections(10.59Km), Gaighat townplanning area Road Section (1.5Km), City hall*simaltar Gaighat Diktel road section (2.88 Km), Babarani DM Gate Road Section (0.65Km), Fulchowk PG Chowk Road Botetole Triyuga Corridor section (0.86Km), City Hall Ramchandra Basnet house Road Section (0.28Km), Gaighat Shivalay tole-Adarsha tole-ramailo danda manmohan school to karamgachhi diktel connecting, Road Section (2.25Km), Pragati tole kha (Quarter front) ko road to sakela road ward 11, 13 (2.17Km)		
Sampled by	Environment Management And Analysis Services P. Ltd., Dillibazar, Kathmandu		
Sampling Date	07 - 08 September 2023		
Report Date	20 September, 2023		
Instrument used	Respirable Dust Sampler (GTI 151) and Combined Sampler (GTI 241)		
Result			
Parameter	NAAQS	Observed Values	Method
TSP ($\mu\text{g}/\text{m}^3$)	230.0	164.5	IS 5182 (Part 14)-2000 (reaffirmed 2005)
PM ₁₀ ($\mu\text{g}/\text{m}^3$)	120.0	77.3	IS 5182 (Part -23):2006
PM _{2.5} ($\mu\text{g}/\text{m}^3$)	40.0	32.0	IS 5182: Part 24: 2019
SO ₂ ($\mu\text{g}/\text{m}^3$)	70.0	8.8	IS 5182 (Part 2)-2006
NO ₂ ($\mu\text{g}/\text{m}^3$)	80.0	8.3	IS 5182 (Part 6)-2006
CO ($\mu\text{g}/\text{m}^3$)	10000.0	<230.0	IS 5182 (Part - 10):2006

* - National Ambient Air Quality Standard, 2069, ** - National Ambient Air Quality Standard for TSP for Crusher Industry, IS Indian Standard

Remarks: The observed values are within the prescribed limit of NAAQS.

Sampled by

Checked by

Authorized by

**Environment Management
&
Analysis Services P. Ltd**



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ANALYSIS REPORT FOR AMBIENT AIR QUALITY

Report Number	27A/080-81		
Sample Number	17A/080/81		
Client	Bosphorous Technical Consulting Corp (BOTTEK), GOEC Nepal P. Ltd		
Sampling location	Triyuga Municipality Ward No. 11, City hall simaltar Gaighat Diktel road section (2.88 Km)		GPS Point: 26°48'11.59"N 86°42'20.16"E
Project	Upgrading of different inter connected Road Sections(10.59Km), Gaighat townplanning area Road Section (1.5Km), City hall simaltar Gaighat Diktel road section (2.88 Km), Babarani DM Gate Road Section (0.65Km), Fulchowk PG Chowk Road Botetole Triyuga Corridor section (0.86Km), City Hall Ramchandra Basnet house Road Section (0.28Km), Gaighat Shivalay tole-Adarsha tole-ramailo danda manmohan school to karamgachhi diktel connecting, Road Section (2.25Km), Pragati tole kha (Quarter front) ko road to sakela road ward 11, 13 (2.17Km)		
Sampled by	Environment Management And Analysis Services P. Ltd., Dillibazar, Kathmandu		
Sampling Date	07 - 08 September 2023		
Report Date	20 September, 2023		
Instrument used	Respirable Dust Sampler (GTI 151) and Combined Sampler (GTI 241)		
Result			
Parameter	NAAQS *	Observed Values	Method
TSP ($\mu\text{g}/\text{m}^3$)	230.0	131.4	IS 5182 (Part 14)-2000 (reaffirmed 2005)
PM ₁₀ ($\mu\text{g}/\text{m}^3$)	120.0	58.4	IS 5182 (Part -23):2006
PM _{2.5} ($\mu\text{g}/\text{m}^3$)	40.0	14.1	IS 5182: Part 24: 2019
SO ₂ ($\mu\text{g}/\text{m}^3$)	70.0	6.3	IS 5182 (Part 2)-2006
NO ₂ ($\mu\text{g}/\text{m}^3$)	80.0	8.9	IS 5182 (Part 6)-2006
CO ($\mu\text{g}/\text{m}^3$)	10000.0	<230.0	IS 5182 (Part - 10):2006

* - National Ambient Air Quality Standard, 2069, ** - National Ambient Air Quality Standard for TSP for Crusher Industry, IS Indian Standard

Remarks: The observed values are within the prescribed limit of NAAQS.

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Website: emas.com.np

ANALYSIS REPORT FOR AMBIENT NOISE LEVEL

Report Number	29N/080-81			Sample Number	18-N/080/81						
Sampling Date	07 - 08 September 2023										
Client	Bosphorous Technical Consulting Corp (BOTEK), GOEC Nepal P. Ltd										
Sampling location	Triyuga Municipality Ward No. 11, Gaighat townplanning area Road Section (1.5Km)			GPS point: 26°47'30.40"N 86°41'57.31"E							
Project	Upgrading of different inter connected Road Sections(10.59Km), Gaighat townplanning area Road Section (1.5Km), City hall simaltar Gaighat Diktel road section (2.88 Km), Babarani DM Gate Road Section (0.65Km), Fulchowk PG Chowk Road Botetole Triyuga Corridor section (0.86Km), City Hall Ramchandra Basnet house Road Section (0.28Km), Gaighat Shivalay tole-Adarsha tole-ramailo danda manmohan school to karamgachhi diktel connecting, Road Section (2.25Km), Pragati tole kha (Quarter front) ko road to sakela road ward 11, 13 (2.17Km)										
Sampled by	Environment Management And Analysis Services P. Ltd., Dillibazar, Kathmandu										
Report Date	20 September, 2023										
Instrument used	LUTRON SL - 4033SD										
Test method	Noise Measurement Protocol (Central Pollution Control Board, India)										
Hours	Time	Noise Level (dBA)			Hours	Time	Noise Level (dBA)				
		Lmax	Lmin	Leq			Lmax	Lmin	Leq		
06:00 - 07:00	Day	71.3	46.4	51.0	18:00 - 19:00	Night	69.1	44.1	49.1		
07:00 - 08:00		80.3	49.4	56.6	19:00 - 20:00		74.4	49.9	48.5		
08:00 - 09:00		77.1	55.6	54.6	20:00 - 21:00		67.2	43.1	48.4		
09:00 - 10:00		75.8	53.2	52.9	21:00 - 22:00		57.9	41.8	44.6		
10:00 - 11:00		83.0	51.4	61.9	22:00 - 23:00		55.5	43.0	47.6		
11:00 - 12:00		85.3	54.6	50.4	23:00 - 00:00		59.2	39.3	44.9		
12:00 - 13:00		78.3	49.5	52.9	00:00 - 01:00		60.7	38.9	44.2		
13:00 - 14:00		82.7	48.3	63.3	01:00 - 02:00		61.3	41.5	47.9		
14:00 - 15:00		87.4	46.6	51.8	02:00 - 03:00		62.9	42.5	47.9		
15:00 - 16:00		89.0	46.6	49.8	03:00 - 04:00		61.3	46.7	49.6		
16:00 - 17:00		81.9	47.1	54.2	04:00 - 05:00		64.3	44.5	50.3		
17:00 - 18:00		77.7	48.3	41.0	05:00 - 06:00		59.2	45.6	50.1		
Leq average (Day)				54.2	Leq average (Night)				47.8		
Permissible limit *				65.0	Permissible limit				55.0		

dBA - A-weighted decibels, Lmax : Maximum Sound Level, Lmin: Minimum Sound Level

Remarks: The equivalent noise level during the day and night hours comply with the permissible limit in reference to noise level standard 2069, under category of commercial area.

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ANALYSIS REPORT FOR AMBIENT NOISE LEVEL

Report Number	30/N/080-81			Sample Number	18-N/080/81						
Sampling Date	07 - 08 September 2023										
Client	Bosphorous Technical Consulting Corp (BOTTEK), GOEC Nepal P. Ltd										
Sampling location	Triyuga Municipality Ward No. 11, City hall simaltar Gaighat Diktel road section (2.88 Km)			GPS Point: 26°48'11.59"N 86°42'20.16"E							
Project	Upgrading of different inter connected Road Sections(10.59Km), Gaighat townplanning area Road Section (1.5Km), City hall simaltar Gaighat Diktel road section (2.88 Km), Babarani DM Gate Road Section (0.65Km), Fulchowk PG Chowk Road Botetole Triyuga Corridor section (0.86Km), City Hall Ramchandra Basnet house Road Section (0.28Km), Gaighat Shivalay tole-Adarsha tole-ramailo danda manmohan school to karamgachhi diktel connecting, Road Section (2.25Km), Pragati tole kha (Quarter front) ko road to sakela.road ward 11, 13 (2.17Km)										
Sampled by	Environment Management And Analysis Services P. Ltd., Dillibazar, Kathmandu										
Report Date	20 September, 2023										
Instrument used	LUTRON SL - 4033SD										
Test method	Noise Measurement Protocol (Central Pollution Control Board, India)										
Hours	Time	Noise Level (dBA)			Hours	Time	Noise Level (dBA)				
		Lmax	Lmin	Leq			Lmax	Lmin	Leq		
06:00 - 07:00	Day	65.2	48.7	52.2	18:00 - 19:00	Night	61.3	46.2	48.6		
07:00 - 08:00		60.8	49.1	51.5	19:00 - 20:00		66.3	47.3	49.4		
08:00 - 09:00		66.8	53.1	54.2	20:00 - 21:00		57.2	42.5	48.1		
09:00 - 10:00		71.9	52.8	53.7	21:00 - 22:00		55.0	39.1	51.5		
10:00 - 11:00		69.8	49.7	52.4	22:00 - 23:00		61.2	37.3	47.7		
11:00 - 12:00		65.2	50.7	54.3	23:00 - 00:00		63.2	36.1	47.1		
12:00 - 13:00		67.2	51.5	56.2	00:00 - 01:00		59.5	37.2	48.4		
13:00 - 14:00		65.0	51.7	55.4	01:00 - 02:00		64.6	42.4	46.6		
14:00 - 15:00		63.2	50.3	52.7	02:00 - 03:00		59.4	44.5	49.8		
15:00 - 16:00		66.2	49.1	53.2	03:00 - 04:00		62.4	47.3	49.4		
16:00 - 17:00		63.5	47.2	50.5	04:00 - 05:00		71.9	46.7	48.6		
17:00 - 18:00		71.9	48.7	51.4	05:00 - 06:00		63.2	49.7	51.8		
Leq average (Day)				53.1	Leq average (Night)				48.9		
Permissible limit *				65.0	Permissible limit				55.0		

dBA - A-weighted decibels, Lmax : Maximum Sound Level, Lmin: Minimum Sound Level

Remarks: The equivalent noise level during the day and night hours comply with the permissible limit in reference to noise level standard 2069, under category of commercial area.

Sampled by

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Website: emas.com.np

Water Analysis Report

Client: Bosphorous Technical Consulting Corp (BOTEK), GOEC Nepal P. Ltd
Sample Location: Triyuga Municipality Ward No. 11, City hall simaltar
Gaighat Diktel road section (2.88 Km)
GPS Point: 26°48'11.18"N 86°42'20.53"E
Report No: 32/W/080-81
Report Date: 20 September, 2023
Sampled by: EMAS P. Ltd.
Project: Upgrading of different inter connected Road Sections(10.59Km),
Gaighat townplanning area Road Section (1.5Km), City hall simaltar
Gaighat Diktel road section (2.88 Km), Babarani DM Gate Road Section
(0.65Km), Fulchowk PG Chowk Road Botetole Triyuga Corridor section
(0.86Km), City Hall Ramchandra Basnet house Road Section (0.28Km),
Gaighat Shivalay tole-Adarsha tole-ramailo danda manmohan school to
karamgachhi diktel connecting, Road Section (2.25Km), Pragati tole kha
(Quarter front) ko road to sakela road ward 11, 13 (2.17Km)

Sample Sources: Hand Pump
Owner: Bidur Katwal
Year of installation: 2006-
Depth of hand pump: 18 feet
Sampling Date: 08 September, 2023
Received Date: 09 September, 2023
Analysis Period: 09 - 12 Sept, 2023

Parameters	Unit	NDWQS	Observed Values	Test Methods
pH	-	6.5 - 8.5	7.2	4500-H ⁺ B, APHA, 22nd EDITION
Colour	-	5 (15)	<0.1	2120 B, APHA, 22 nd EDITION
Turbidity	NTU	5 (10)	2.5	2130 B, APHA, 22nd EDITION
Electrical Conductivity	µS/cm	1500	148.0	2510 B, APHA, 22nd EDITION
Temperature (Lab)	°C	-	24.0	2550 B., APHA, 22nd EDITION
Total Hardness	mg/l as CaCO ₃	500	68.0	2340 C, APHA, 22nd EDITION
Chloride	mg/l	250	3.2	4500-Cl ⁻ B, APHA, 22nd EDITION
Ammonia	mg/l	1.5	<0.02	4500-NH ₃ C., APHA, 17 th EDITION
Nitrate	mg/l as NO ₃	50	0.5	4500-NO ₃ - B., APHA, 22nd EDITION
Iron	mg/l	0.3 (3)	<0.01	3112 B., APHA, 22nd EDITION
Manganese	mg/l	0.2	<0.02	3112 B., APHA, 22nd EDITION
Arsenic	mg/l	0.05	<0.01	3114 C, APHA, 22nd EDITION
E.Coli	CFU/100 ml	Nil	Nil	9221 C., APHA, 22nd EDITION

NDWQS: National Drinking Water Quality Standard (2079), * - Values are upper and lower limit, () - Values are acceptable only when alternative is not available. **APHA:** American Public Health Association, N/A : Not Availalable

Remarks: Observed values of the parameters are within the NDWQS.

Analyzed by

Checked by

Authorized Signature

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Annex VI: List of Zebra Crossings

SN	Name of the Road	Chainage	Nos.	Remarks
1	TOWN Planning _1	0+015	1	
2		0+050.346	1	
3		0+085	1	
4		0+180.75	1	
5		0+264.5	1	
6		0+315.75	1	
7		0+543	1	
8		0+601.75	1	
9		0+820.25	1	
10		0+880	1	
11		1+000	1	
12		1+030.75	1	
13	TOWN Planning _2	0+867.373	1	
14		1+025.283	1	
15		0+471	1	
16		0+270	1	
17		0+221.25	1	
18	City hall Simaltar	0+025.25	1	
19		0+165	1	
20		0+236.25	1	
21		0+286.25	1	
22		0+260.399	1	
23		1+314.75	1	
24		1+270.25	1	
25		1+292.192	1	
26		0+925.25	1	
27		0+873.75	1	
28		0+481.25	1	
29		0+427.75	1	
30		1+784.5	1	
31		1+729.75	1	
32		2+109.75	1	
33		2+054.25	1	
34		2+649.75	1	
35		2+597.5	1	
36		2+854.25	1	
37		1+925	1	
38	Babarani DM Gate	0+617.75	1	
39		0+005.5	1	
40		0+157.75	1	
41		0+190.25	1	

SN	Name of the Road	Chainage	Nos.	Remarks
42		0+385.25	1	
43		0+421	1	
44	Fulchowk Triyuga Corridor	0+015.021	1	
45		0+384.782	1	
46		0+443.327	1	
47		0+850.951	1	
48	Pragati Tole_1	0+022.5	1	
49		0+141.5	1	
50		0+110.75	1	
51		0+279	1	
52		0+232	1	
53		0+766.5	1	
54	Pragati Tole_2	0+029.322	1	
55		0+131.917	1	
56		0+200	1	
57		0+593	1	
58		0+703.75	1	
59		0+994	1	
60		1+072.546	1	
61		1+280	1	
62		1+220	1	
63	Gaighat Shivalaya	0+040.331	1	
64		0+390.713	1	
65		0+702.502	1	
66		0+658.002	1	
67		0+890.752	1	
68		0+837.097	1	
69		1+206.752	1	
70		1+167.252	1	
71		1+587.502	1	
72		1+393.252	1	
Total			72	Nos.

Annex VII: Code of Conduct (CoC) on GBV

नेपाल शहरी शासकीय तथा पुर्वाधार आयोजना

कार्य स्थलमा हुने यौनजन्य तथा महिला हिंसा सम्बन्धी आचार संहिता

व्याक्तिगत आचार संहिता

म,यो आचार संहिता पालना गर्नु मेरो दाहित्व हो भनी स्वीकार गर्दछु।म कुनै पनि यौनजन्य तथा महिला हिंसा जस्ता कार्यमा संलग्न हुने छैन। परियोजना को काम को शिलसिलामा यो आचार संहिता पालना गर्न सहमत छु।

१. म जातजाति धर्म, भाषा, लिङ्ग, उमेर, राजनीतिक वा सामाजिक हैसियत, भौगोलिकता, पहुच, वैवाहिक स्थिती वा अन्य कुनै पनि आधारमा भेदभाव नगरी सबैलाई सम्मानजनक र समान रुपमा व्यवहार गर्नेछु।
२. सामाजिक सन्जालको प्रयोग गरी अश्लील शब्द, दुष्य सामग्री वा कार्यलय समय अर्धपछी वार्तालाप मार्फत सहकर्म/कामदार लाई यौन दुर्व्याहार गर्ने छैन।
३. कार्यस्थलमा सिट्टी बजाउने, चुम्बन गर्ने, व्याक्तिगत उपहार दिने आदि जस्ता कार्य गरी कर्मचारी, सहकर्म/कामदार लाई यौन दुर्व्याहार गर्ने छैन।
४. कुनै पनि प्रलोभन/ धम्की देखाई (जस्तै पदोन्नती लोभ देखाएर,जागीर नदिने धम्की दिएर शोषण गरेर आदि) यौन दुर्व्याहार पक्षमा संलग्न हुने छैन।
५. कार्य समयवाधि भित्र कुनैपनि मंदिरजन्य तथा लागुपदार्थको सेवन गर्ने छैन।
६. परियोजना सरोकारवाला वा वरपरका समुदायका सदस्यहरूलाई कुनैपनि म लैङ्गिक हिंसा तथा यौनजन्य दुर्व्याहार गर्ने छैन।
७. कुनै पनि कर्मचारी/श्रमिक विरुद्ध हिंसा गरिएको दोषी ठहरिएमा प्रचलित संधिय, प्रादेशिक, स्थानीय सरकार वर्ल्ड बैंक को कानून, निती नियम अनुसार सजाय/ दण्डित जरिवाना तिर्न तयार हुनेछु।
८. कार्य गर्ने शिलसिलामा सम्मानजनक निर्देशनहरूको पालना गर्दछु (वातावरणीय + सामाजिक)
९. मेरो जिम्मेवारी कुशलता र लगनशीलता पुर्वक पुरा गर्नेछु।

१०. सम्बंधित कार्यलय /कम्पनीले सन्चालन गरेको विभिन्न प्रशिक्षण कार्यक्रममा सक्रिय रुपमा भाग लिनेछु।
११. परियोजनाका प्रत्यक्ष लाभदायक सदस्य/समुदायमा यौन दुर्व्याहार/शोषण गर्ने छैन।
१२. विश्वासनीयता नैतिक उल्लघनको रिपोर्ट गरेमा कुनै कामदार विरुद्ध बदला लिने छैन।
१३. कार्य स्थलमा लैङ्गिक सम्बेदनशिल भाषाको प्रयोग गर्दछु
१४. कार्यस्थलमा महिला हिंसा तथा यौनजन्य क्रियाकलाप लाई प्रोत्साहन गर्ने खालका गर्तविधी गर्न दिने छैन।
१५. कार्यस्थलमा महिला तथा यौन हिंसा गर्तविधीहरूलाई प्रोत्साहन गर्ने छैन।
१६. १८ वर्षभन्दा मुनिका बालिकाहरूमा कुनै डिजिटल मिडीया मार्फत वा कुनै माध्यमबाट /स्वीकृती लिई वा नलिई यौनजन्य क्रियाकलापमा सहभागी हुनेछैन, यदि नाबालिका स्वीकृती लिई यौनजन्य क्रियाकलापमा गरेमा क्षमा हुदैन।
१७. परियोजना कार्यन्वयन को वेलाका यौनजन्य दुर्व्याहार /यौन शोषण भएमा वा आचार संहिता उल्लघन गरेमा बडा/ नगरपालिका स्तरमा रहेको गुनासो सुनवाई सयन्त्रमा तुरुन्त निवेदन/ जानकारी दिनेछु।
१८. कार्यस्थलमा कसैले यौनजन्य दुर्व्याहार सम्बन्धी शक्कापद व्यावहार गरेमा वा शक्कापद कार्य गरेमा तुरुन्त टोली प्रमुख /प्रबन्धकलाई जानकारी/निवेदन दिनेछु।

माथि उल्लेखित आचार संहिता राम्ररी पढे र बुझेको छु र कार्यस्थलमा कडाईका साथ पालना गर्दछु भनी हस्ताक्षर गर्दछु।

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व्यवस्थापक/टोली प्रमुख

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कर्मचारी/कामदार

Annex VIII: Stakeholder Consultation Details

1. Gaighat town planning area Road Section

Date	Consultation Type	Composition of Participants	Issues Raised	Response from Project
5/24/2080	KII	Shahi kumar Tuladhar Chairperson, Himali tole bikas sastha	<ul style="list-style-type: none"> • Must be Quality Road structures • Side drain must be made on both side of the road. • Must start the construction work as soon as possible. 	Project emphasis that there will be no compromise on the quality of the road structure. Proper monitoring will be done to the contractor.
2080/06/11	FGD	Ramesh Dahal and other Jyotinagar tole members (Participants: 7M/3F, including 6IPs)	<ul style="list-style-type: none"> • Mainly emphasized on the quality of road and the material that will be used during the construction. • Must be safe and clean side drain. 	We clarify that project will monitor for the quality and side drain will be made clear.
2080/05/24	FGD	Shahi kumar Tuladhar Chairperson and other members of Himali tole bikas sastha, (Participants: 9M/3F)	<ul style="list-style-type: none"> • Focused on the quality road structure. • Must be completed on time. • Must be drainage on both side of the road 	Project emphasis that there will be no compromise on the quality of the road structure. Proper monitoring will be done to the contractor.
2080/05/24	Meeting	Shashi Kumar Tuladhar and Himali tole members/Locals	<ul style="list-style-type: none"> • Must be drainage on both side of the road. • Local people must be used during the construction phase. • Training must be given. 	Project will address the drainage in the design. It will also take care of local labor as far as possible. There will be training program for the

Date	Consultation Type	Composition of Participants	Issues Raised	Response from Project
		(Participants: 10M/4F)	<ul style="list-style-type: none"> Environmental problem must be taken seriously. 	local labor. Environmental issues will be carefully addressed.
2080/06/11	Meeting	Bharat Prasad Chaudhary and members of Jyotinagar tole bikas (Participants: 17M/2F)	<ul style="list-style-type: none"> Very happy to help for the completion of the project. Willing to work in the project. Focused on the opportunity of local people to work in the project. Water must be sprayed during the construction phase. 	We clearly mention that local manpower will be used as far as possible and all other environmental and social issues will be seriously addressed.

City Hall simaltar Gaighat Diktel road section

Date	Consultation Type	Composition of Participants	Issues Raised	Response from Project
2080/05/11	FGD	Devi Prasad Khatiwoda and members of Shivalaya tole bikas Sastha, W-11 (Participants: 9M/2F)	<ul style="list-style-type: none"> No objection for the construction of the road. ROW is clear. Quality of must be maintained. Must be completed on time 	We thank them for their support and promised that the project will be completed on time.
2080/05/11	FGD	Nabin Karki and members of Indreshwor tole bikas Sastha, W-11 (Participants: 10M/1F)	<ul style="list-style-type: none"> Row is clear already in 2055 BS as 20m. No objection on the clearance of the ROW. Difficulty for the pedestrian on this existing road No any GBV. 	We assured them that project will be completed as soon as possible.

2080/05/11	Meeting	Devi Prasad Khatiwoda and other tole members of Shivalaya tole bikas Sastha, W-11 (Participants: 15M/3F)	<ul style="list-style-type: none"> • Must start work as soon as possible • Must be environmentally friendly • No any obstacles for the completion of the road project 	We made them clear about the completion date of the project and also mention that the environmental and social issues will be address properly.
2080/05/12	Meeting	Nabin Karki and members of Indreshwor tole bikas Sastha, W-11 (Participants: 14M/6F)	<ul style="list-style-type: none"> • Must complete the project with quality work. • Local people have to be involved in the construction work. • Training to the local people should be given. 	As already stated, that the project will be completed on time and issues related to the local empowerment and local labor force will be timely addressed. ESMP will be strictly followed.

Babarani DM Gate Road Section

Date	Consultation Type	Composition of Participants	Issues Raised	Response from Project
2080/05/12	FGD	Tek Bahadur Thapa and members of Babarani Tole bikas, Ward-11 (Participants: 10M/1F)	<ul style="list-style-type: none"> • Must start work and work must be of good quality. • Road must be completed on time. • Dust and other environmental must be settled during construction phase. • Local people must be given chance to work for their income and must provide training. 	The project completion date is made clear. We also focused that local people will be taken for the construction work as soon as possible. Environmental problems will be addressed timely and promptly.
2080/05/12	Meeting	Bishnu Shrestha, Ward member and Locals of	<ul style="list-style-type: none"> • Quite happy to know that construction will be started as soon as possible. • No any social and environmental issues 	Project is very serious about the local work force so that there will be limited effect to the natural

		Ward-11 (Participants: 18M/3F)	<ul style="list-style-type: none"> • Must be completed on time. • Local people must be used during construction work, 	resources. Environmental issues will be addressed on time. Water will be sprayed during the construction phase.
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Fulchowk PG Chowk Road Botetole Triyuga Corridor section

Date	Consultation Type	Composition of Participants	Issues Raised	Response from Project
2080/05/07	FGD	Tara Prasad Chaudhari and members of Fulchowk tole, Ward-12 (Participants: 11M/0F)	<ul style="list-style-type: none"> • Very delay for the start of the work. • Very poor condition of the existing road and very difficult for the pedestrians. • No any environmental and social issues. 	We clearly told them that the work will be completed on time. Environmental issues will be address properly during the construction phase.
2080/05/07	FGD	Lawa Dhakal and members of Anamnagar tole bikas, Ward-12 (Participants: 5M/6F)	<ul style="list-style-type: none"> • Very necessary to start this work as soon as possible. • Must be quality road structures. • Local people must be used for the labor work force. 	Project will be completed on time. Quality of the road will be monitor regularly by the concern authorities. Local manpower will be used as far as possible.
2080/05/07	Meeting	Narman Bot Ward 12 Chair and Locals (Participants: 15M/3F, All IPs)	<ul style="list-style-type: none"> • Very happy to know that the work will be started soon. • ROW is clear and people are very willing to help the project. • Must be good quality of road. • Training for the local people must be provided. 	Completion time was stated and quality of the road is assured. Environmental and social problems will be addressed promptly.

Date	Consultation Type	Composition of Participants	Issues Raised	Response from Project
			<ul style="list-style-type: none"> Dust and other environmental problems must be settled by the project during the construction of the road 	
2080/05/07	Meeting	Shankar Bahadur Kathayat and Locals of Ward 12 (Participants: 12M/7F)	<ul style="list-style-type: none"> Road ROW is clear municipality in 2055 BS. No issues on the environmental and social part. Women empowerment must be taken into consideration. Road safety must be taken seriously during construction phase. 	Different training program, awareness campaign will be done by the concern authorities. Road safety and safety signboard will be installed on every alignment on the regular interval and as per need.

City Hall Ramchandra Basnet house Road Section

Date	Consultation Type	Composition of Participants	Issues Raised	Response from Project
2080/05/09	FGD	Sabina Phuyal and other members of Namuna Marga tole bikas sastha (Participants: 5M/7F)	<ul style="list-style-type: none"> Must be completed the project on time. 	Project will be completed on time. There is no permanent structure and big trees in the alignment so there are no any environmental and social issues.
2080/05/12	Meeting	Bishnu Shrestha, ward member and Locals of Ward-11 (Participants: 14M/12F)	<ul style="list-style-type: none"> Very willing to help for the project. Must start the work as soon as possible. Quality must be assured. Dust must be controlled during the constructing work. 	Road safety and children safety measure will be taken very seriously. Quality of work will be monitored very seriously. Work will be completed on time.

Gaighat Shivalay tole-Adarsha tole-Ramailo danda Manmohan school to Karamgachhi Diktel connecting Road Section

Date	Consultation Type	Composition of Participants	Issues Raised	Response from Project
2080/06/08	FGD	Bhabindra Khatri and other members of Ramailodanda tole bikas Sastha (Participants: 7M/4F)	<ul style="list-style-type: none"> • Requested for the fast work to be completed. • Willing to help for the project as far as possible. • No GBV and other social related issues. 	Project will address all the environmental and social issues if any arise during the construction stage as well as operational stage. Awareness program will be provided to the local people.
2080/06/08	FGD	Kalpana Khatri and other member of Yakata women group of Ramailodanda tole (Female: 10 nos.)	<ul style="list-style-type: none"> • Work must be done effectively and efficiently. • Very willing to help for the project 	We highly appreciate the people of this area and promise them to start the work as soon as possible.
2080/06/08	Meeting	Subindra BK, ward chair and members of Ramailodanda tole bikas Sastha (Participants: 6M/7F)	<ul style="list-style-type: none"> • Very willing to help the project. • Must be careful during construction phase. • Quality of road must not be compromised 	Project will closely monitor the quality of the road, Environmental and social issues will be addressed timely and promptly.
2080/06/08	Meeting	Kalpana Khatri and other member of Yakata women group of Ramailodanda tole (Female: 10 nos.)	<ul style="list-style-type: none"> • Women must be given priority for the work during the construction phase. • Must be completed the work on time. • Very difficult to walk at this existing condition. • Proper training must be provided to the local people. 	Project will emphasis for the women empowerment and training on the different aspect will be provided to the local people. Construction of the road will be completed on time and with quality road structures.

Date	Consultation Type	Composition of Participants	Issues Raised	Response from Project

Pragati tole kha (Quarter front) ko road to sakela road

Date	Consultation type	Composition of participants	Issues raised	Response from project
2080/05/23	FGD	Gita Rai, Chairperson of Sakela tole bikas Sastha and sakela tole members, Ward-11 (Participants: 0M/11F)	<ul style="list-style-type: none"> • Quite positive and enthusiastic to support the project to construct the road. • Road must be completed on time with quality road structures. • Women should encourage to work and must provide training for the women groups for their empowerment • Road construction should begin soon 	The project will carefully design the road. Women group training and empowerment will be seriously taken.
2080/05/25	FGD	Aasha Rai and members of Sakela Women Group, Ward-11 (Participants: 12 female)	<ul style="list-style-type: none"> • Difficulty using the road due to poor existing condition. • IPs women should be given more priority. • No sexual offense. • Some gender related violence. 	Project made clear that project will be completed on time. Local women will be given priority to work and for training as well. Campaign and awareness programs will be conducted for GBV and SH.
2080/05/25	FDG	Tilak Bahadur Katuwal and members of Janajyoti tole bikas Sastha, Ward-13 (Participants: 6M/4F)	<ul style="list-style-type: none"> • All ready to clear ROW and no trees and houses are within the ROW. • No any conflict issue for the construction of the road. • Must start to construction as soon as possible. • Water must be sprayed during construction phase. • Plantation must be done on both side by the project 	There will be no impact on structures except for some trees sapling. Proper ESMP will be followed. Also Project assured that plantation will be done and the cost will be incorporated.

Date	Consultation type	Composition of participants	Issues raised	Response from project
2080/05/23	Meeting	Gita Rai and locals of Sakela tole bikas Sastha, Ward-11 (Participants: 6M/24F)	<ul style="list-style-type: none"> • Public are positive for construction. • Construction of road needs to tender fast. • Women empowerment should be focused • Public are okay and will help for the project to complete smoothly. • Environmental issues should be addressed on timely and properly 	Made clear about the completion date of the project and made sure that all other issues will be address accordingly.
2080/06/12	Meeting	Kala Rai and members of Sakela tole, Ward-11 (Participants: 17M/22F)	<ul style="list-style-type: none"> • Quit concern about labors that are needed during construction phase. • Training for the local people should be given. • Quality of road should not be compromised. • Willing to help for the project. 	Project made them sure that the labors will be taken from the local level as far as possible. The training will be given for the workers prior to the start of the project. Quality of the road will not be compromised.

Annex IX: Existing electric pole details

Table 7-15: Existing electric pole details

SN	Chainage	Direction	Name of the Road	SN	Chainage	Direction	Name of the Road
1	0+330	Left	Babarani to DM gate	83	1+310	Left	City Hall to Simaltar
2	0+380	Left	Babarani to DM gate	84	1+375	Left	City Hall to Simaltar
3	0+410	Left	Babarani to DM gate	85	1+430	Left	City Hall to Simaltar
4	0+445	Left	Babarani to DM gate	86	1+690	Right	City Hall to Simaltar
5	0+005	Left	Town Planning 1	87	1+870	Left	City Hall to Simaltar
6	0+030	Right	Town Planning 1	88	1+920	Left	City Hall to Simaltar
7	1+125	Left	Town Planning 1	89	1+935	Left	City Hall to Simaltar
8	1+135	Left	Town Planning 1	90	1+985	Left	City Hall to Simaltar
9	1+150	Left	Town Planning 1	91	2+120	Left	City Hall to Simaltar
10	1+170	Left	Town Planning 1	92	2+225	Right	City Hall to Simaltar
11	1+195	Right	Town Planning 1	93	2+275	Right	City Hall to Simaltar
12	0+210	Right	Town Planning 1	94	2+330	Right	City Hall to Simaltar
13	0+225	Right	Town Planning 1	95	2+375	Right	City Hall to Simaltar
14	0+250	Right	Town Planning 1	96	2+510	Right	City Hall to Simaltar
15	0+310	Left	Town Planning 1	97	0+020	Left	Ramchandra Basnet
16	0+330	Right	Town Planning 1	98	0+065	Left	Ramchandra Basnet
17	0+380	Right	Town Planning 1	99	0+145	Left	Ramchandra Basnet
18	0+435	Right	Town Planning 1	100	0+170	Left	Ramchandra Basnet
19	0+470	Right	Town Planning 1	101	0+225	Right	Ramchandra Basnet
20	0+510	Left	Town Planning 1	102	0+005	Left	Shivalaya Tole Ramailo Dada
21	0+580	Left	Town Planning 1	103	0+040	Right	Shivalaya Tole Ramailo Dada
22	0+650	Left	Town Planning 1	104	0+085	Left	Shivalaya Tole Ramailo Dada
23	0+675	Left	Town Planning 1	105	0+150	Left	Shivalaya Tole Ramailo Dada
24	0+725	Left	Town Planning 1	106	0+200	Left	Shivalaya Tole Ramailo Dada
25	0+740	Left & Right	Town Planning 1	107	0+245	Left	Shivalaya Tole Ramailo Dada
26	0+755	Left	Town Planning 1	108	0+290	Right	Shivalaya Tole Ramailo Dada
27	0+775	Right	Town Planning 1	109	0+310	Left	Shivalaya Tole Ramailo Dada

SN	Chainage	Direction	Name of the Road	SN	Chainage	Direction	Name of the Road
28	0+790	Left	Town Planning 1	110	0+330	Left	Shivalaya Tole Ramailo Dada
29	0+815	Right	Town Planning 1	111	0+355	Left	Shivalaya Tole Ramailo Dada
30	0+840	Right	Town Planning 1	112	0+405	Left	Shivalaya Tole Ramailo Dada
31	0+865	Right	Town Planning 1	113	0+465	Left	Shivalaya Tole Ramailo Dada
32	0+915	Right	Town Planning 1	114	0+485	Left	Shivalaya Tole Ramailo Dada
33	0+925	Left	Town Planning 1	115	0+510	Left	Shivalaya Tole Ramailo Dada
34	0+935	Right	Town Planning 1	116	0+630	Left	Shivalaya Tole Ramailo Dada
35	0+945	Right	Town Planning 1	117	0+685	Left	Shivalaya Tole Ramailo Dada
36	0+955	Left	Town Planning 1	118	0+720	Left	Shivalaya Tole Ramailo Dada
37	0+975	Right	Town Planning 1	119	0+925	Left	Shivalaya Tole Ramailo Dada
38	0+985	Left	Town Planning 1	120	0+975	Right	Shivalaya Tole Ramailo Dada
39	1+010	Left	Town Planning 1	121	1+030	Right	Shivalaya Tole Ramailo Dada
40	0+030	Left	Town Planning 2	122	1+070	Left	Shivalaya Tole Ramailo Dada
41	0+055	Left	Town Planning 2	123	1+130	Left	Shivalaya Tole Ramailo Dada
42	0+075	Left	Town Planning 2	124	1+185	Left	Shivalaya Tole Ramailo Dada
43	0+095	Right	Town Planning 2	125	1+235	Left	Shivalaya Tole Ramailo Dada
44	0+125	Right	Town Planning 2	126	1+365	Right	Shivalaya Tole Ramailo Dada
45	0+185	Right	Town Planning 2	127	1+495	Right	Shivalaya Tole Ramailo Dada
46	0+230	Right	Town Planning 2	128	1+550	Left	Shivalaya Tole Ramailo Dada
47	0+365	Right	Town Planning 2	129	1+570	Left	Shivalaya Tole Ramailo Dada
48	0+400	Right	Town Planning 2	130	1+595	Left	Shivalaya Tole Ramailo Dada
49	0+430	Left	Town Planning 2	131	0+075	Right	Pragati Tole, Section 1
50	0+489	Left	Town Planning 2	132	0+0110	Left	Pragati Tole, Section 1

SN	Chainage	Direction	Name of the Road	SN	Chainage	Direction	Name of the Road
51	0+005	Left	City Hall to Simaltar	133	0+160	Left	Pragati Tole, Section 1
52	0+045	Right	City Hall to Simaltar	134	0+230	Left	Pragati Tole, Section 1
53	0+050	Right	City Hall to Simaltar	135	0+265	Left	Pragati Tole, Section 1
54	0+110	Right	City Hall to Simaltar	136	0+295	Left	Pragati Tole, Section 1
55	0+130	Right	City Hall to Simaltar	137	0+325	Left	Pragati Tole, Section 1
56	0+155	Right	City Hall to Simaltar	138	0+360	Left	Pragati Tole, Section 1
57	0+180	Left	City Hall to Simaltar	139	0+440	Left	Pragati Tole, Section 1
58	0+210	Right	City Hall to Simaltar	140	0+465	Left	Pragati Tole, Section 1
59	0+255	Right	City Hall to Simaltar	141	0+495	Left	Pragati Tole, Section 1
60	0+290	Right	City Hall to Simaltar	142	0+530	Left	Pragati Tole, Section 1
61	0+345	Left	City Hall to Simaltar	143	0+880	Right	Pragati Tole, Section 1
62	0+355	Right	City Hall to Simaltar	144	0+925	Left	Pragati Tole, Section 1
63	0+375	Right	City Hall to Simaltar	145	0+975	Left	Pragati Tole, Section 1
64	0+395	Left	City Hall to Simaltar	146	1+130	Right	Pragati Tole, Section 1
65	0+435	Left	City Hall to Simaltar	147	1+250	Right	Pragati Tole, Section 1
66	0+470	Left	City Hall to Simaltar	148	0+020	Left	Pragati Tole, Section 2
67	0+505	Right	City Hall to Simaltar	149	0+120	Left	Pragati Tole, Section 2
68	0+525	Left	City Hall to Simaltar	150	0+155	Left	Pragati Tole, Section 2
69	0+570	Left	City Hall to Simaltar	151	0+180	Left	Pragati Tole, Section 2
70	0+610	Left	City Hall to Simaltar	152	0+335	Left	Pragati Tole, Section 2
71	0+655	Left	City Hall to Simaltar	153	0+400	Right	Pragati Tole, Section 2
72	0+710	Left	City Hall to Simaltar	154	0+530	Left	Pragati Tole, Section 2
73	0+740	Left	City Hall to Simaltar	155	0+550	Right	Pragati Tole, Section 2
74	0+765	Left,Right	City Hall to Simaltar	156	0+570	Left	Pragati Tole, Section 2
75	0+780	Right	City Hall to Simaltar	157	0+605	Left	Pragati Tole, Section 2
76	0+835	Left	City Hall to Simaltar	158	0+035	Left	Fulchowk Road
77	0+910	Left	City Hall to Simaltar	159	0+075	Left	Fulchowk Road
78	0+960	Right	City Hall to Simaltar	160	0+125	Left	Fulchowk Road
79	1+010	Right	City Hall to Simaltar	161	0+165	Left	Fulchowk Road
80	1+065	Right	City Hall to Simaltar	162	0+685	Left,Right	Fulchowk Road
81	1+085	Right	City Hall to Simaltar	163	0+710	Left	Fulchowk Road
82	1+235	Left	City Hall to Simaltar	164	0+850	Left	Fulchowk Road
Total						167	Nos.

Annex X: Photographs

Pictorial highlights of the field work



Consultation meeting with community people of Jyoti nagar tole , Town planning Road



Consultation meeting with community people of Himali tole, Town planning Road



Community consultation at Indreshworr tole of at City Hall Simaltar Road section



Community consultation with Indreshworr tole at City Hall Simaltar Road section



Consultation meeting with community people of Shivalayai tole, City Hall Simaltar Road section



Consultation meeting with Teacher of Jana Premi Basic School



Community consultation meeting at Babarani tole



Community consultation meeting at Babarani tole



Community consultation with Locals at Fulchowk



Community consultation with Locals at Amarnagr tole



Consultation with Bote community at Bote tole



Community consultations meeting at Namuna Marga of Cityhall Ramchandra Road section



Community consultation meeting at Ramailodanda tole, Gaighat Shivalaya Road section

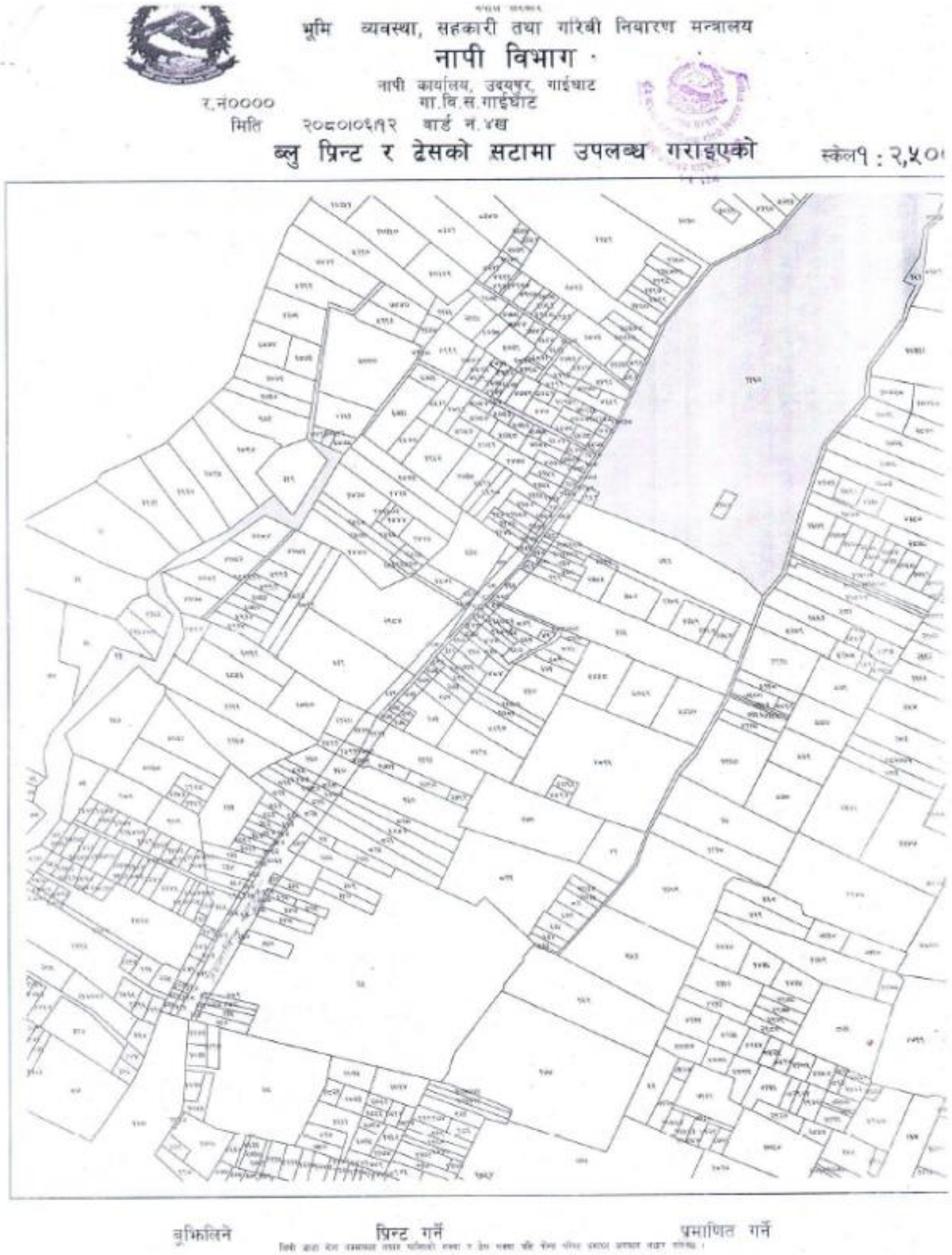
Community consultation meetings at Ramailodanda tole, Gaighat Shivalaya Road section

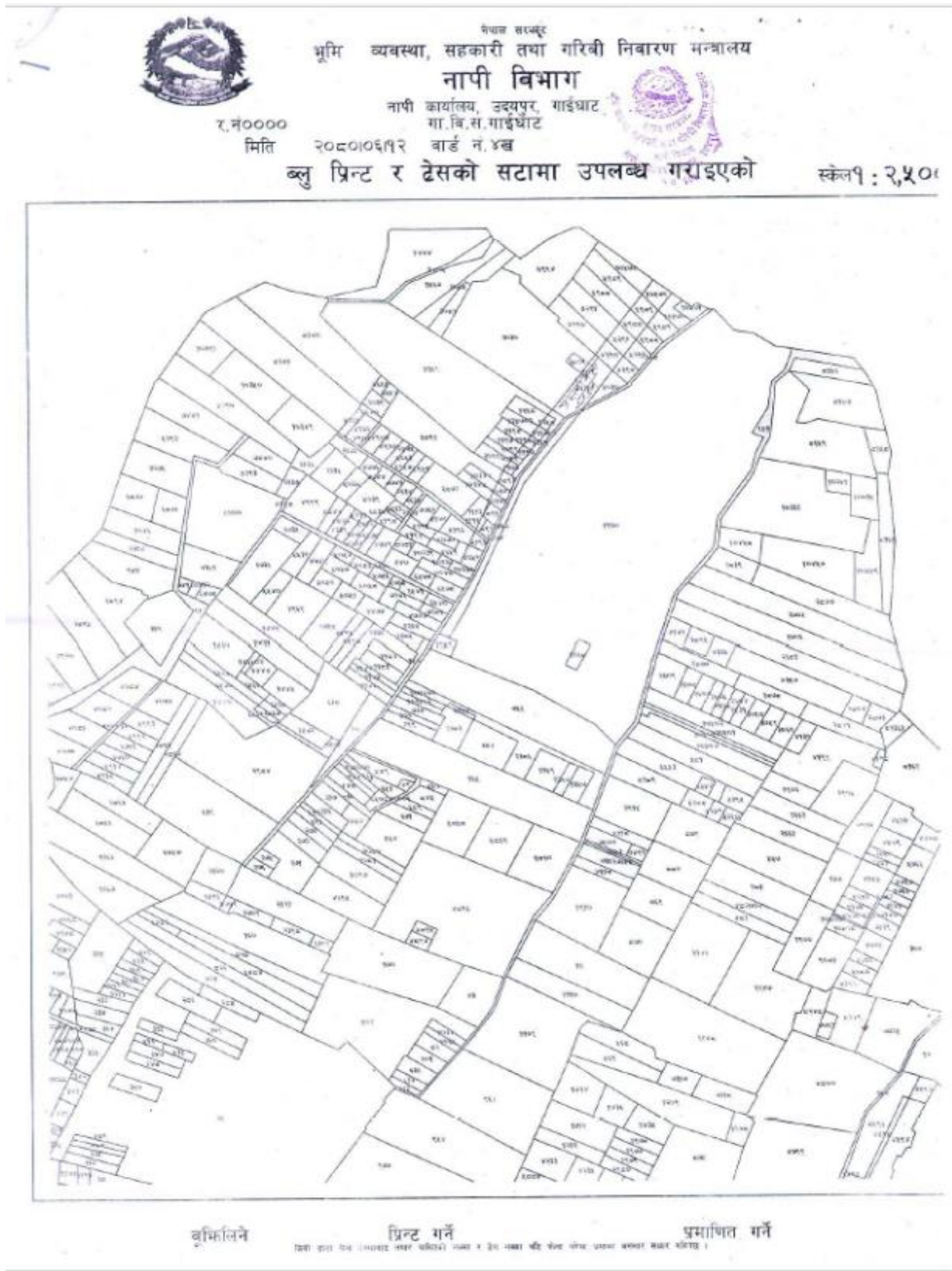


Consultation meetings at Janjyoti tole, Pragati tole road section

Consultation meeting at Sakela tole, Pragati tole road section

ii) Cityhall Simaltar Road







नेपाल सरकार
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नापी विभाग

र.नं००००

मिति २०८०।०६।०८

नापी कार्यालय, उदयपुर, गाईघाट
गा.वि.स.गाईघाट

वार्ड नं.४क



ब्लु प्रिन्ट र टेसको सटामा उपलब्ध गराइएको

स्केल १ : २,५००



बुझिलिने

प्रिन्ट गर्ने

प्रमाणित गर्ने

यो नक्सा केस सम्बन्धमा त्रुटि भएमा यसको जम्मा नगर्नु।



नेपाल सरकार
भूमि व्यवस्था, सहकारी तथा गरिबी निवारण मन्त्रालय
नापी विभाग

र.नं००००

मिति २०८०।०६।०८

वार्ड नं. ४क
ब्लु प्रिन्ट र टेसको सटामा उपलब्ध गराइएको



स्केल १ : २,५००



बुझिलिने

प्रिन्ट गर्ने

प्रमाणित गर्ने

यो सटामा केवल सम्बन्धित दस्तावेजहरूको आधारमा तैयार गरिएको छ। यो सटामा कुनै पनि सम्बन्धित दस्तावेजको अभावमा त्रुटि हुन सक्छ।



नेपाल सरकार
भूमि व्यवस्था, सहकारी तथा गरिबी निवारण मन्त्रालय
नापी विभाग

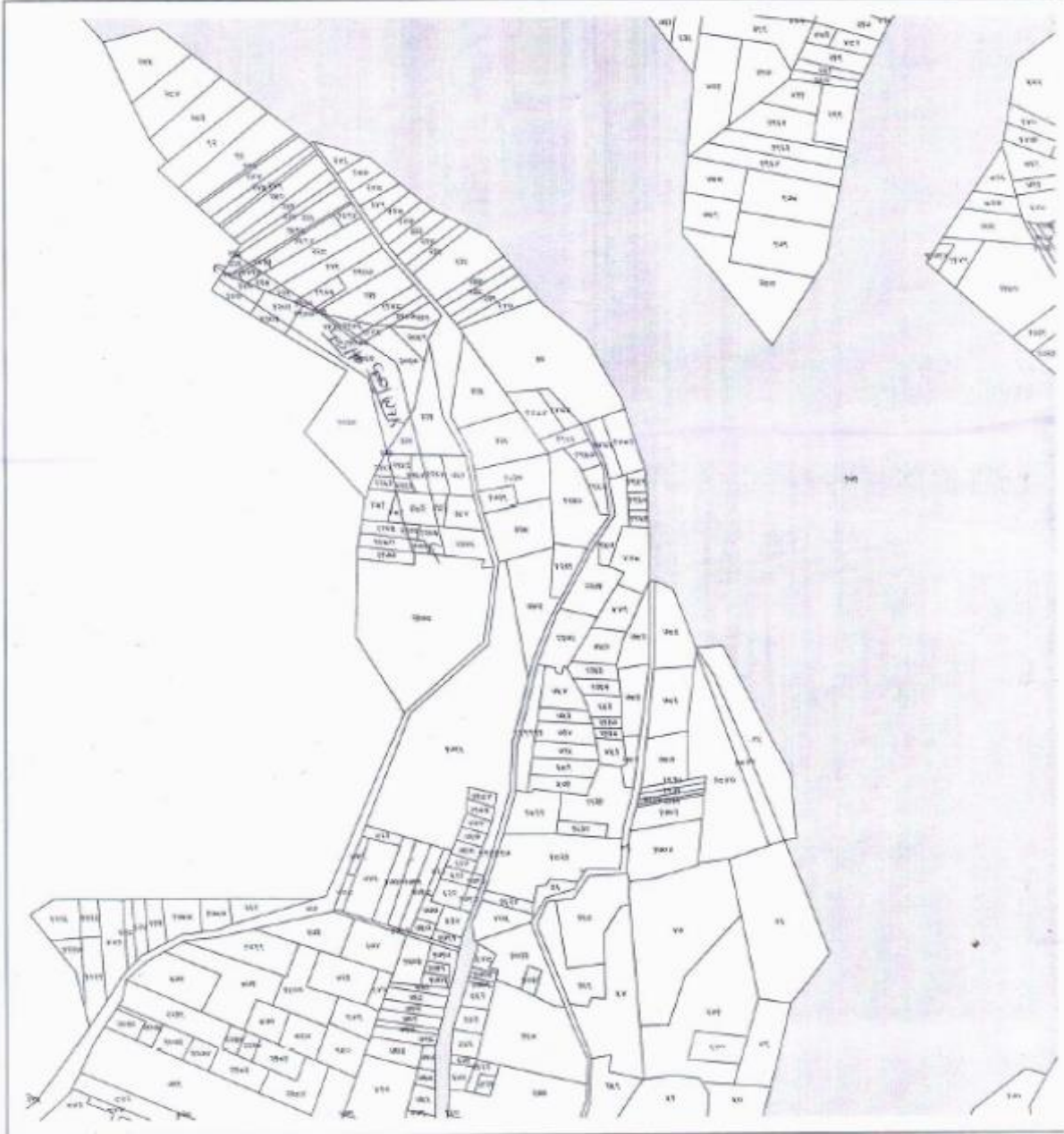
र.नं००००

मिति २०८०।०६।२४ बाई नं.४क

नापी कार्यालय, उदयपुर, गाईघाट
गा.वि.स.गाईघाट

ब्लु प्रिन्ट र टेसको सटामा उपलब्ध गराइएको

स्केल १ : २,५००



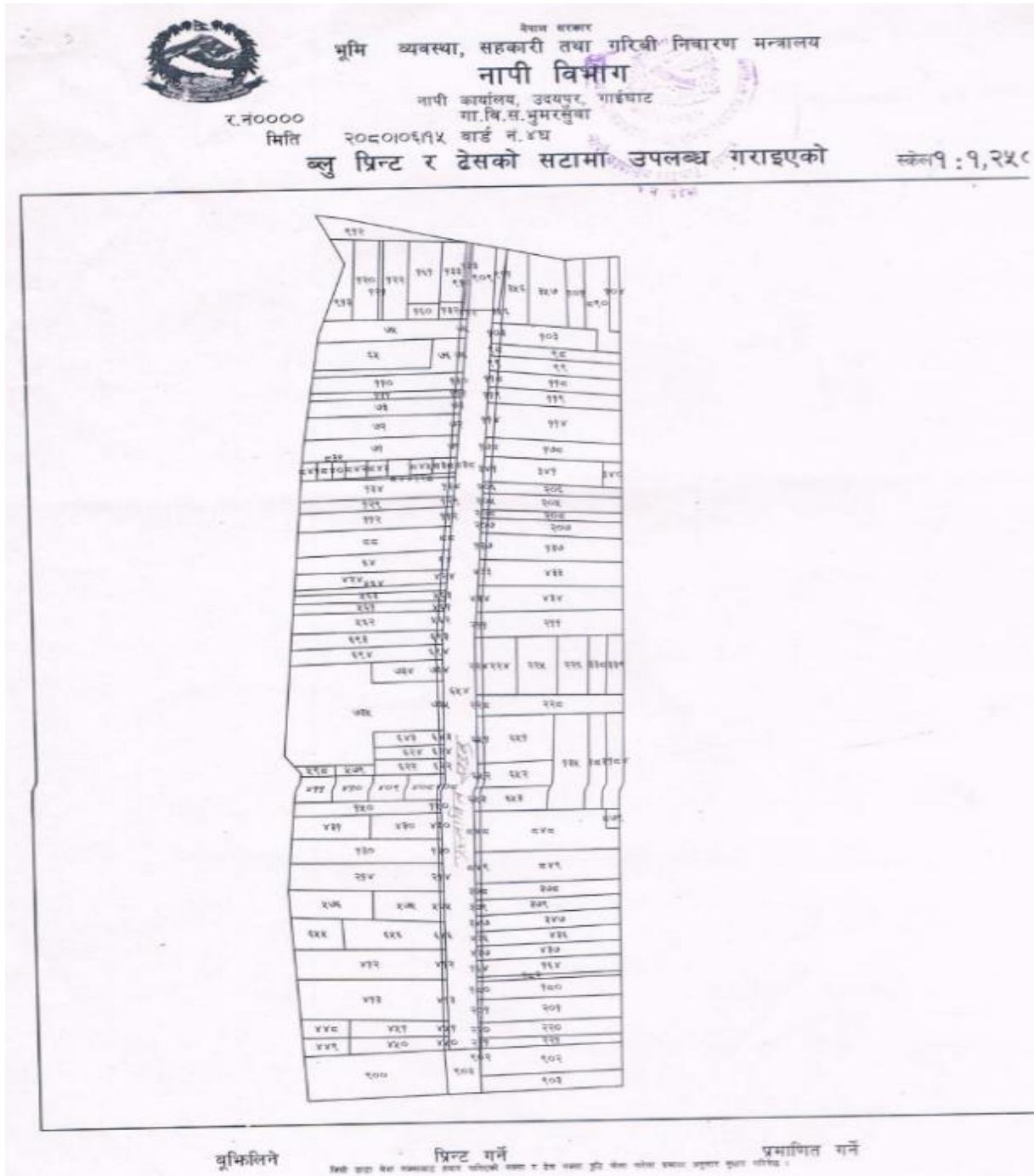
बुझिलिने

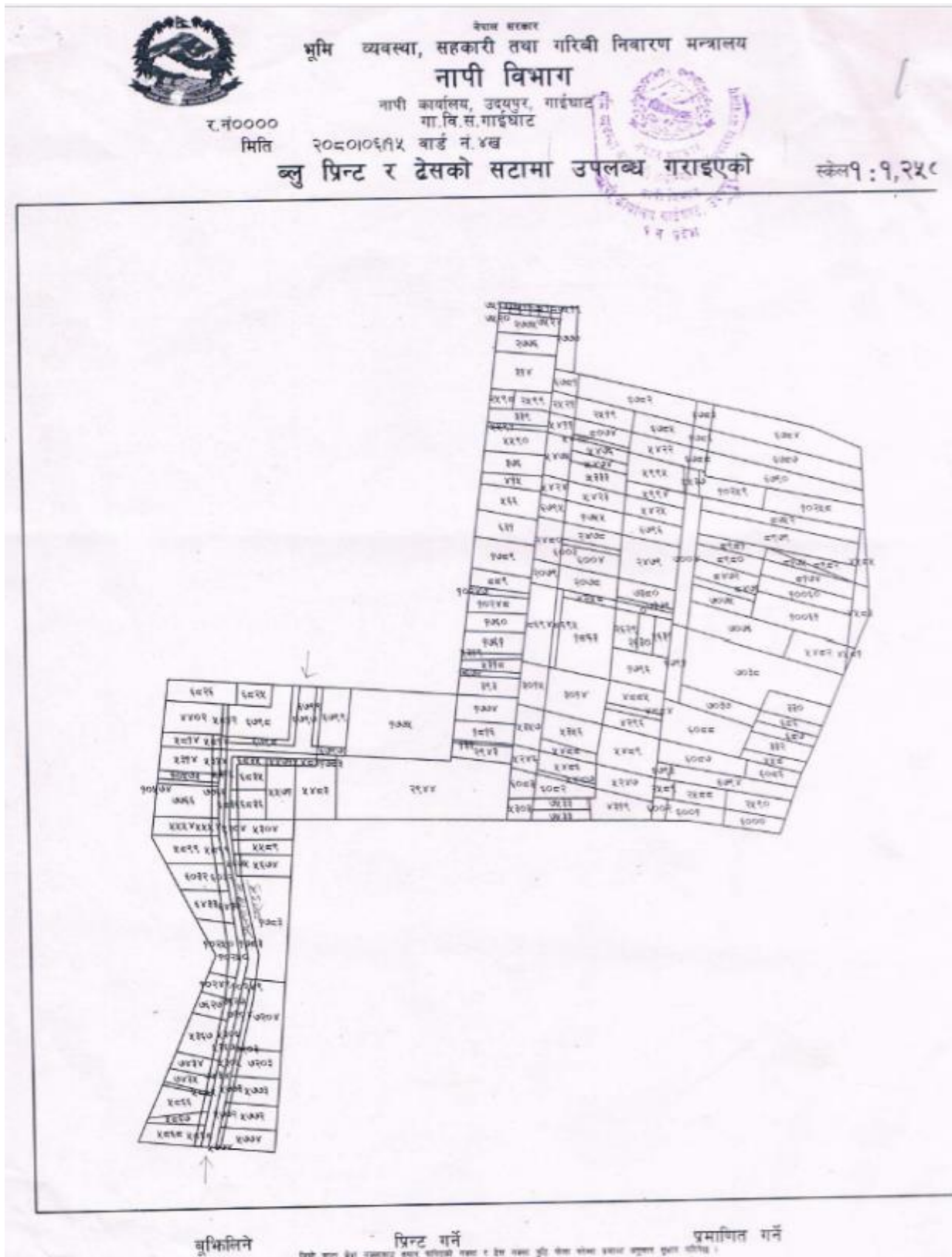
प्रिन्ट गर्ने

प्रमाणित गर्ने

जिहा डाटा रमा नक्साबाट तयार गरिलिने नक्सा र डेटा तयार हुने कला श्रेणी समान अनुमत हुन सक्छ ।

iii) Babarani DM gate Road







नेपाल सरकार
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नापी विभाग

र. नं ००००

मिति २०८०/०८/१५ वार्ड नं. ३क

ब्लु प्रिन्ट र टेसको सटामा उपलब्ध गराइएको

स्कैन १ : १, २५



भूमिलिने

प्रिन्ट गर्ने

प्रमाणित गर्ने

निको झट्टो रंग प्रयोग गरेर प्रिन्ट गर्ने र त्यसको कृपया कसैलाई देखाउनु हुदैन।

iv) Fulchowk Road





नेपाल सरकार
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नापी विभाग

र.नं००००

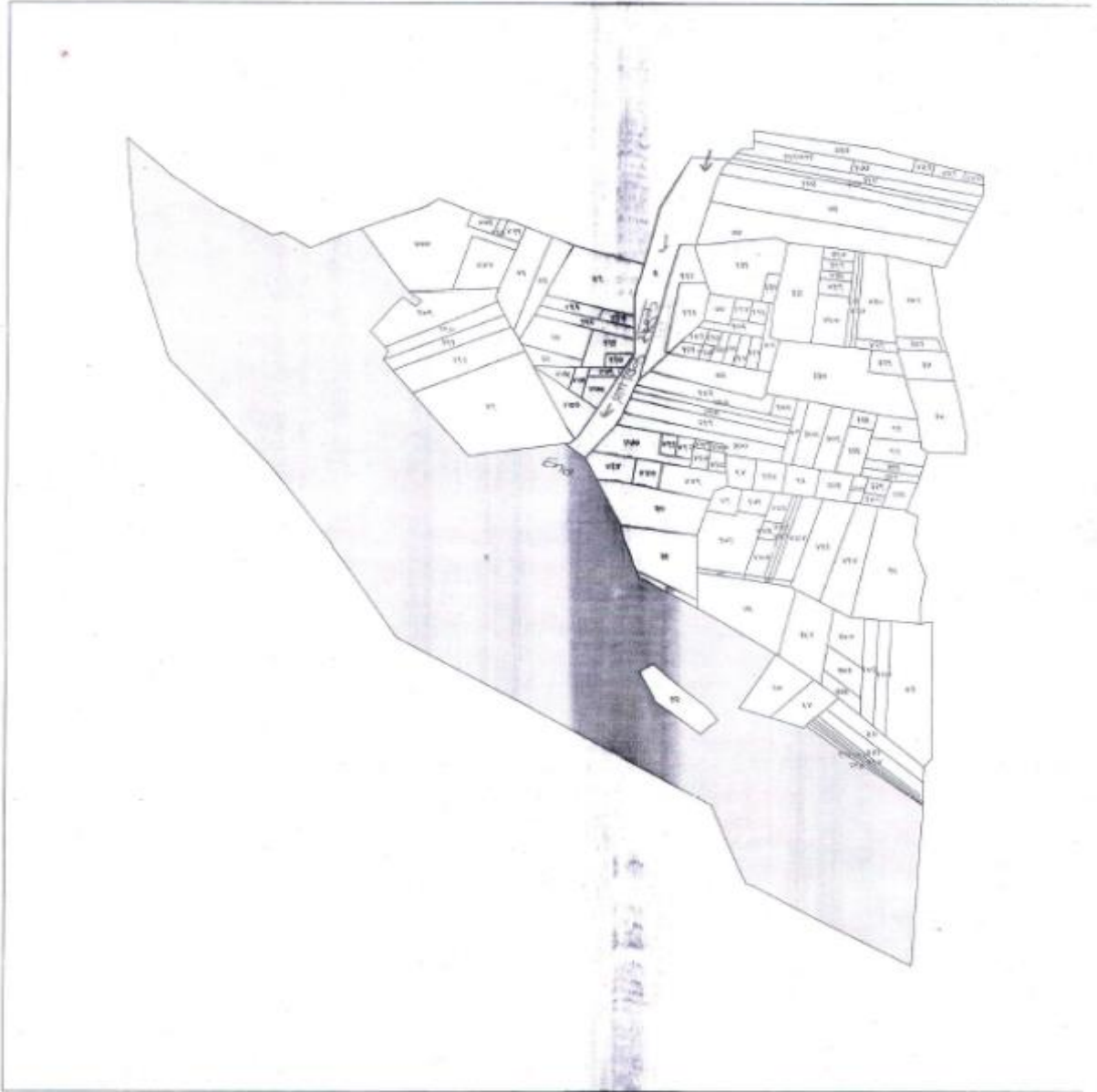
मिति २०८०/०६/२५ वार्ड नं. ३ख

नापी कार्यालय, उदयपुर, गाईघाट
गा.वि.सं. गाईघाट



ब्लु प्रिन्ट र टेसको सटामा उपलब्ध गराइएको

स्केल १ : २,५००



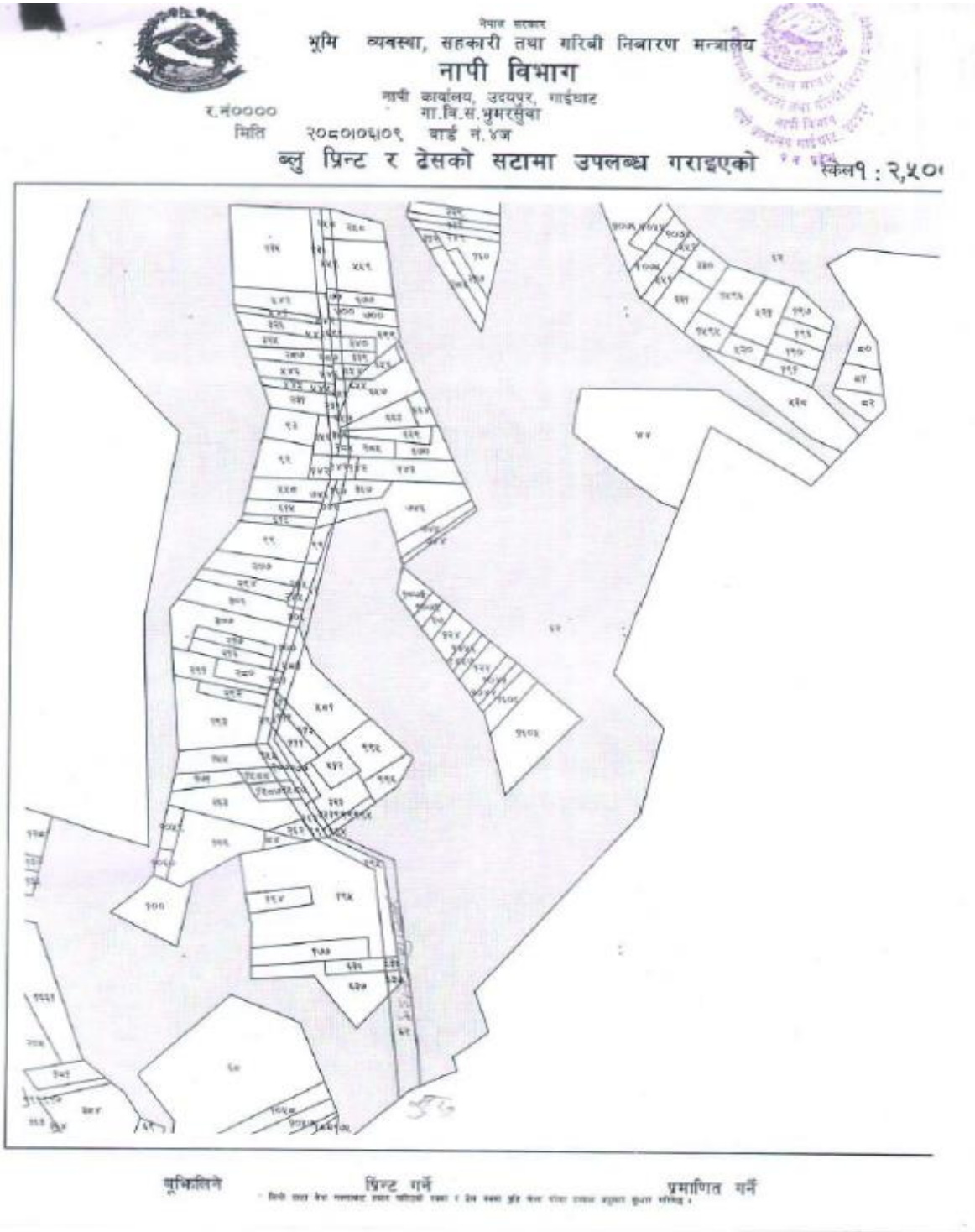
भूमिलिने

प्रिन्ट गर्ने

प्रमाणित गर्ने

यो नक्सा नेपाल सरकारको तर्फबाट तैयार गरिएको नक्सा हो जसको प्रयोग र प्रसारण गर्न पाइने छैन ।

v) Gaighat Shivalaya Road





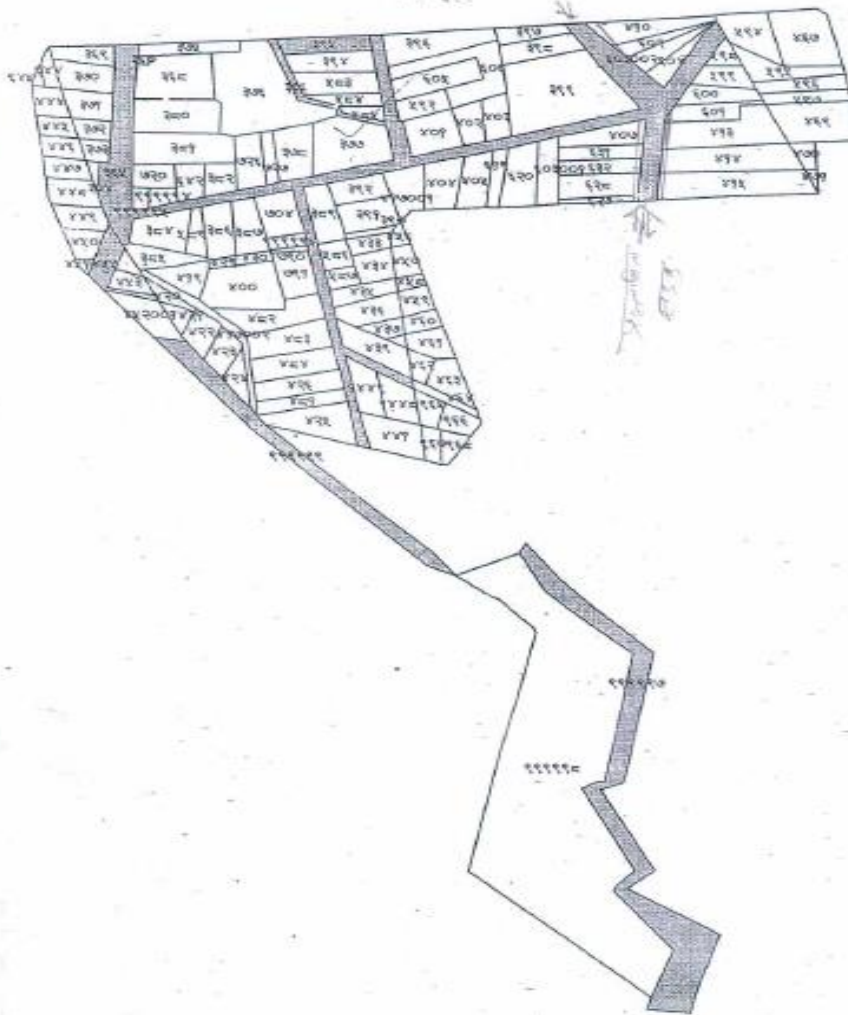
नेपाल सरकार
भूमि व्यवस्था, सहकारी तथा गरिबी निवारण मन्त्रालय
नापी विभाग
नापी कार्यालय, जुवयपुर, गाईघाट
गा.वि.सं. नं.मा. मुमरसुवा

विभिन्न भूखण्डको नापमा मात्र

मिति २०७८/०९/१२ बार्ड नं. ४३

ब्लु प्रिन्ट र ट्रेसको सटामा उपलब्ध गराइएको

स्केल १:२,५००



प्रिन्ट गर्ने
बैकको प.न.

प्रमाणित गर्ने

मिति २०७८/९/१२ को पत्रानुसार

२०७८/१२/१२



नेपाल सरकार

भूमि व्यवस्था, सहकारी तथा गरिबी निवारण मन्त्रालय

नापी विभाग

नापी कार्यालय, उदयपुर, गाईघाट

गा.वि.स.भुमरसुवा

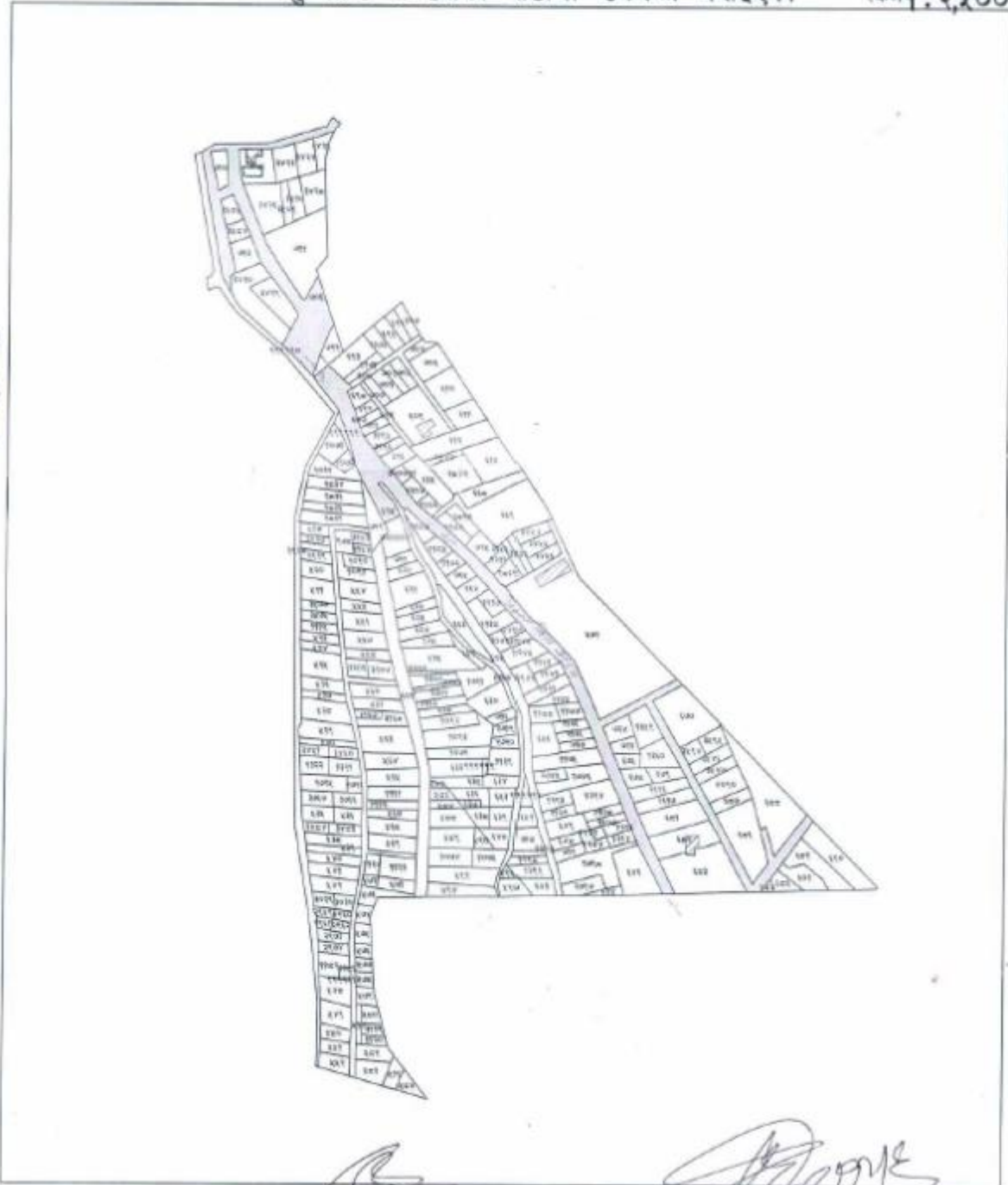
बार्ड नं. ४ख

र.नं००००
मिति

१०४९
२०७९/१२/०९

ब्लु प्रिन्ट र ट्रेसको सटामा उपलब्ध गराइएको

स्केल १:२,५००



भूमिलिने

प्रिन्ट गर्ने

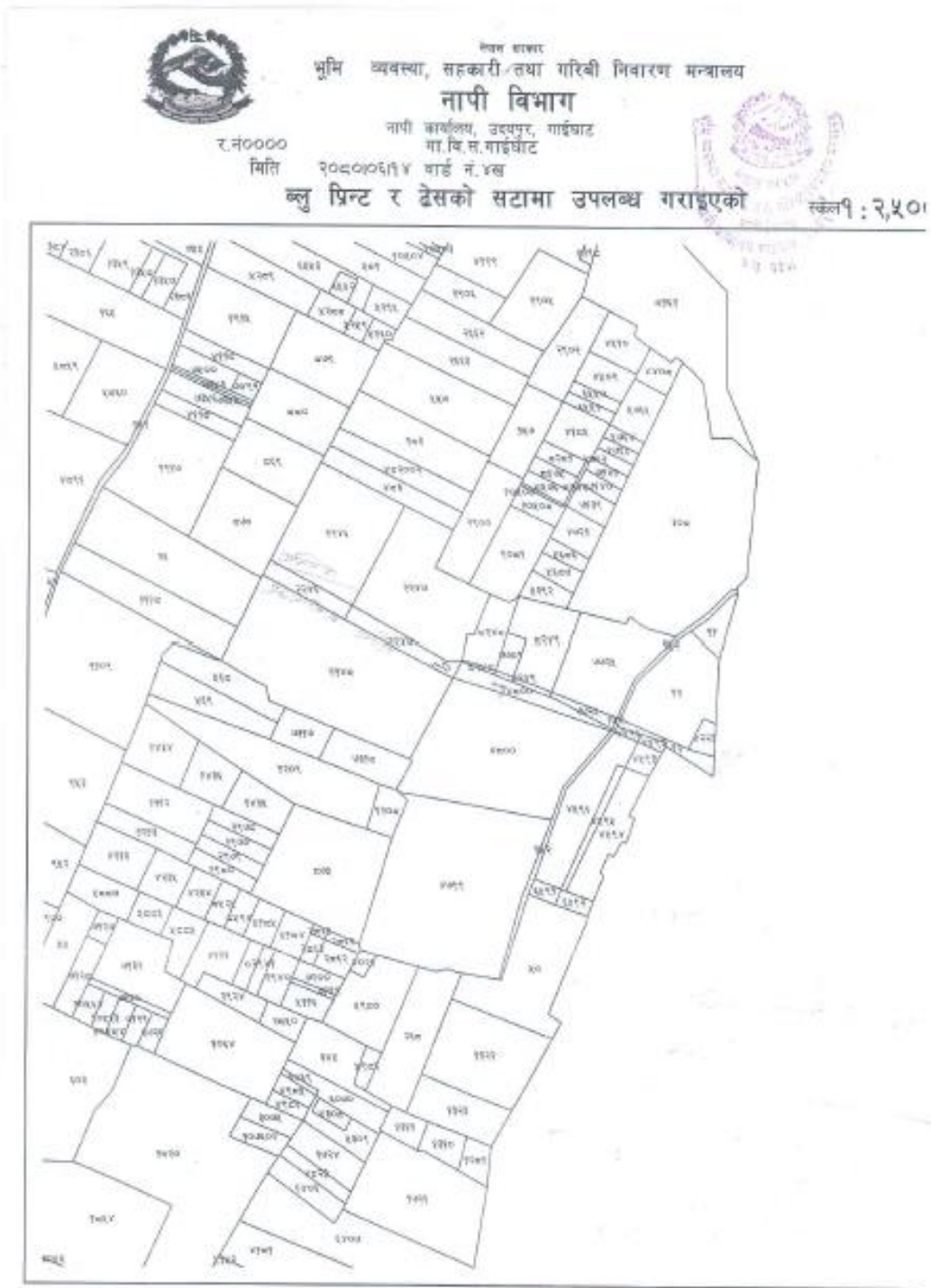
प्रमाणित गर्ने

सर्वेक्षक

सिने बोट रंग चलाएपछि मात्र फोटोको प्रयोग गर्न सकिने हुनेछ।

vi) Pragati tole Road







नेपाल सरकार
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नापी विभाग

र.नं००००
मिति २०८०।०६।०

नापी कार्यालय, उदयपुर, गार्डघाट
गा.वि.स.गार्डघाट
वार्ड नं. ४ ग

ब्लु प्रिन्ट र टेसको सटामा उपलब्ध गराइएको

स्केल १ : २,५००



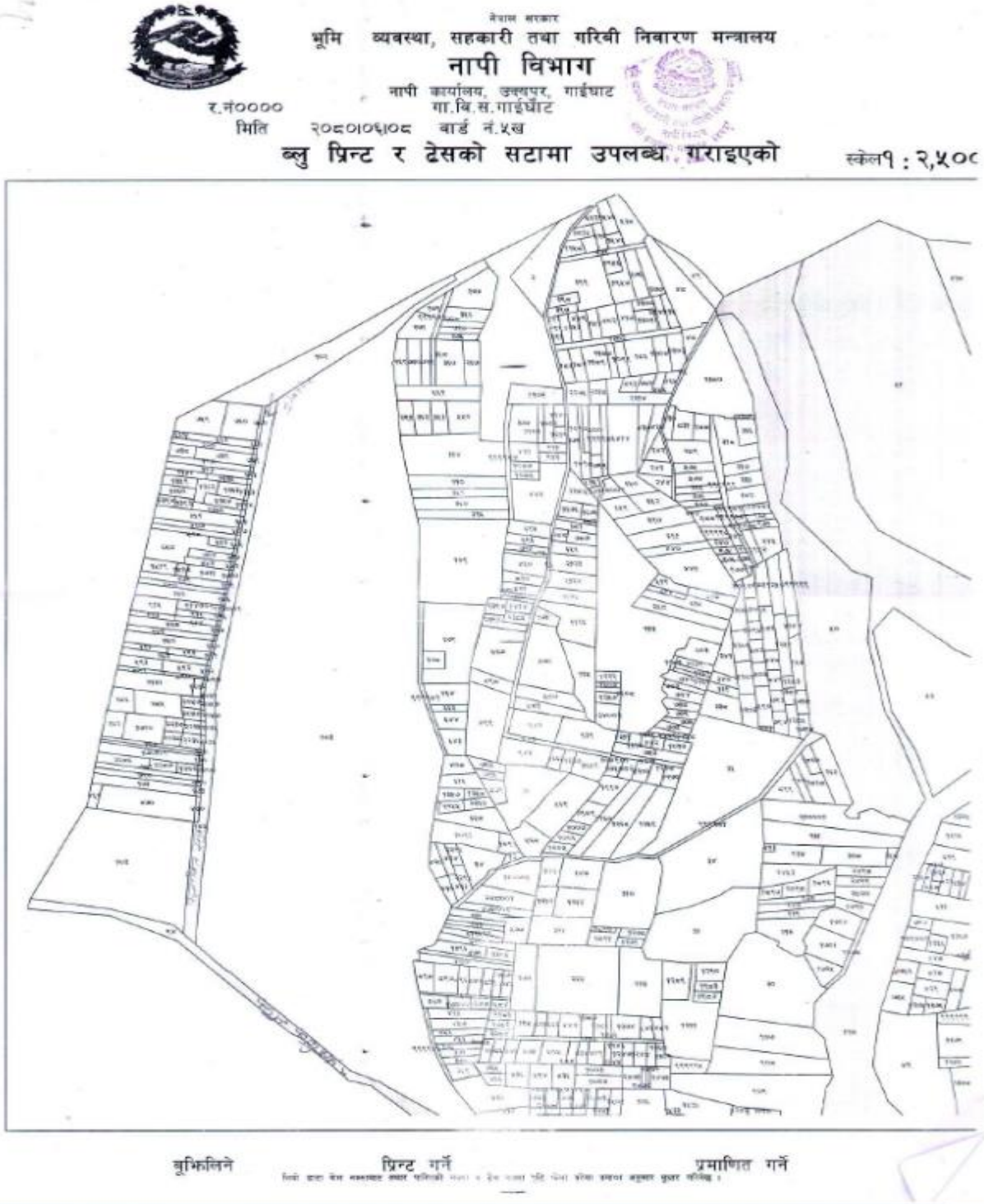
भूमिलिने

प्रिन्ट गर्ने

प्रमाणित गर्ने

योको कडा रूपमा जमानत गरिनु पर्नेछ। योको र ३३ नंका कुनै अन्य किसिमको सुचना कुनै पनि स्थितिमा

vii) Sagarmatha Road





नेपाल सरकार
भूमि व्यवस्था, सहकारी तथा गरिबी निवारण मन्त्रालय
नापी विभाग

र. नं ००००

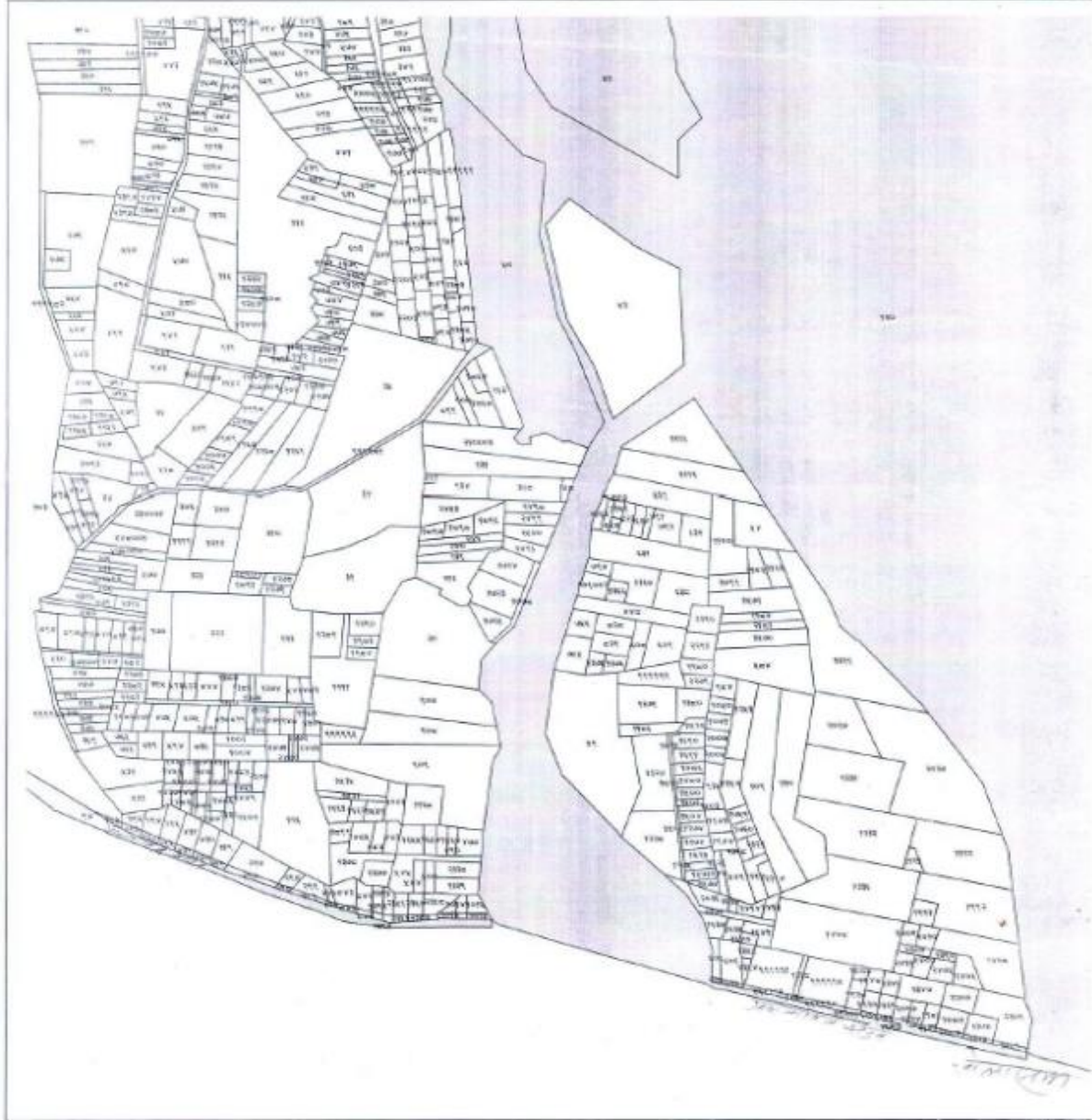
मिति २०८०।०६।१२ वार्ड नं. ५ख

नापी कार्यालय, उदयपुर, गाईघाट
गा. वि. सं. गाईघाट



ब्लु प्रिन्ट र टेसको सटामा उपलब्ध गराइएको

स्केल १ : २,५००



बुझिलिने

प्रिन्ट गर्ने

प्रमाणित गर्ने

अन्य डाटा रोज नसक्योत अन्य शर्तिका नक्सा र टोल नक्सा बटै पठाए जसमा अन्याय नभएत नसक्यो ।



भूमि व्यवस्था, सहकारी तथा गरिबी निवारण मन्त्रालय
नापी विभाग

र. नं ०००००

मिति २०८०।०६।२५, बाई नं. ६क

नापी कार्यालय, उदयपुर, गाईघाट
गा. वि. स. गाईघाट

ब्लु प्रिन्ट र टेसको सटामा उपलब्ध गराइएको

स्केल १ : २,५००



भूमिलिने

प्रिन्ट गर्ने

प्रमाणित गर्ने

योको प्रयोग केवल नक्साकारको नक्सा निर्माणको लागि मात्रै गर्न सकिनेछ। योको प्रयोग अन्य कुनै उद्देश्यका लागि ग्राह्य नभइनेछ।



र. नं००००

मिति २०८०/०६/१०

भूमि व्यवस्था, सहकारी तथा गरिबी निवारण मन्त्रालय

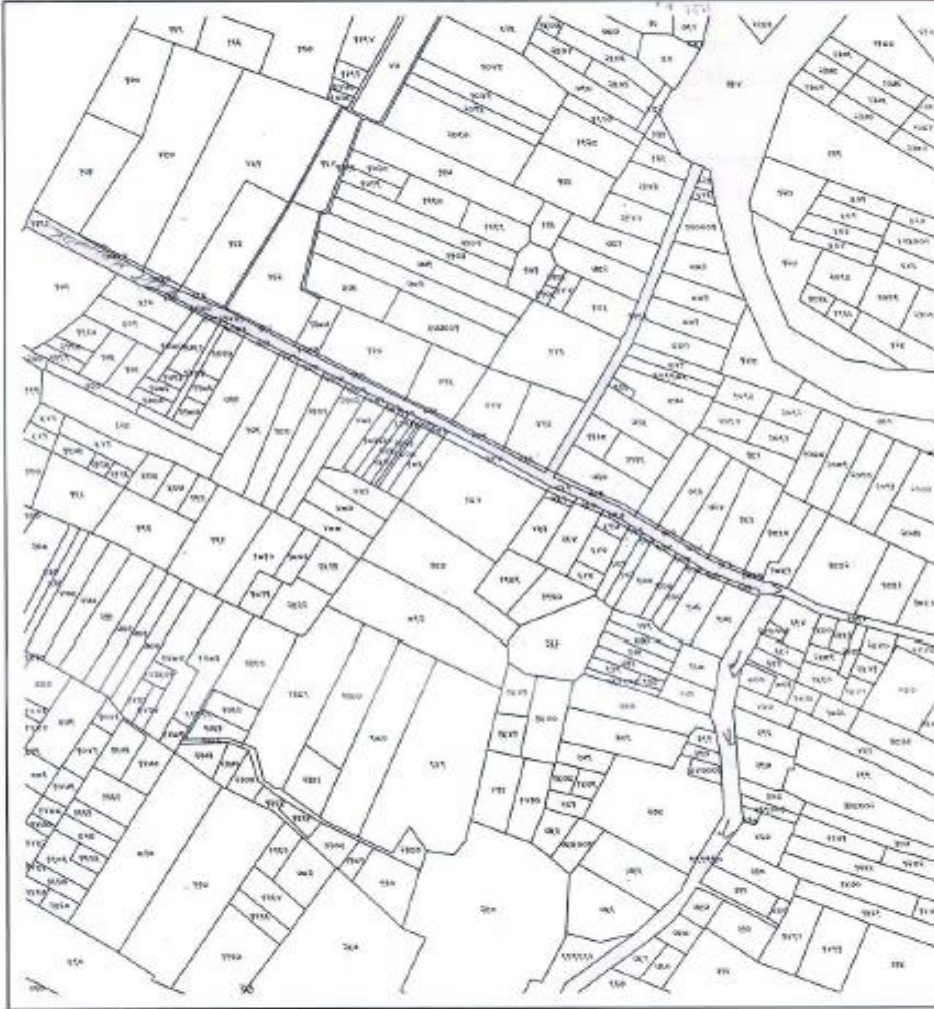
नापी विभाग

गा.पि.स. माईघाट

वार्ड नं.६स

जु प्रिन्ट र टेसको सटामा उपलब्ध गराइएको

स्केल १ : २,५०१

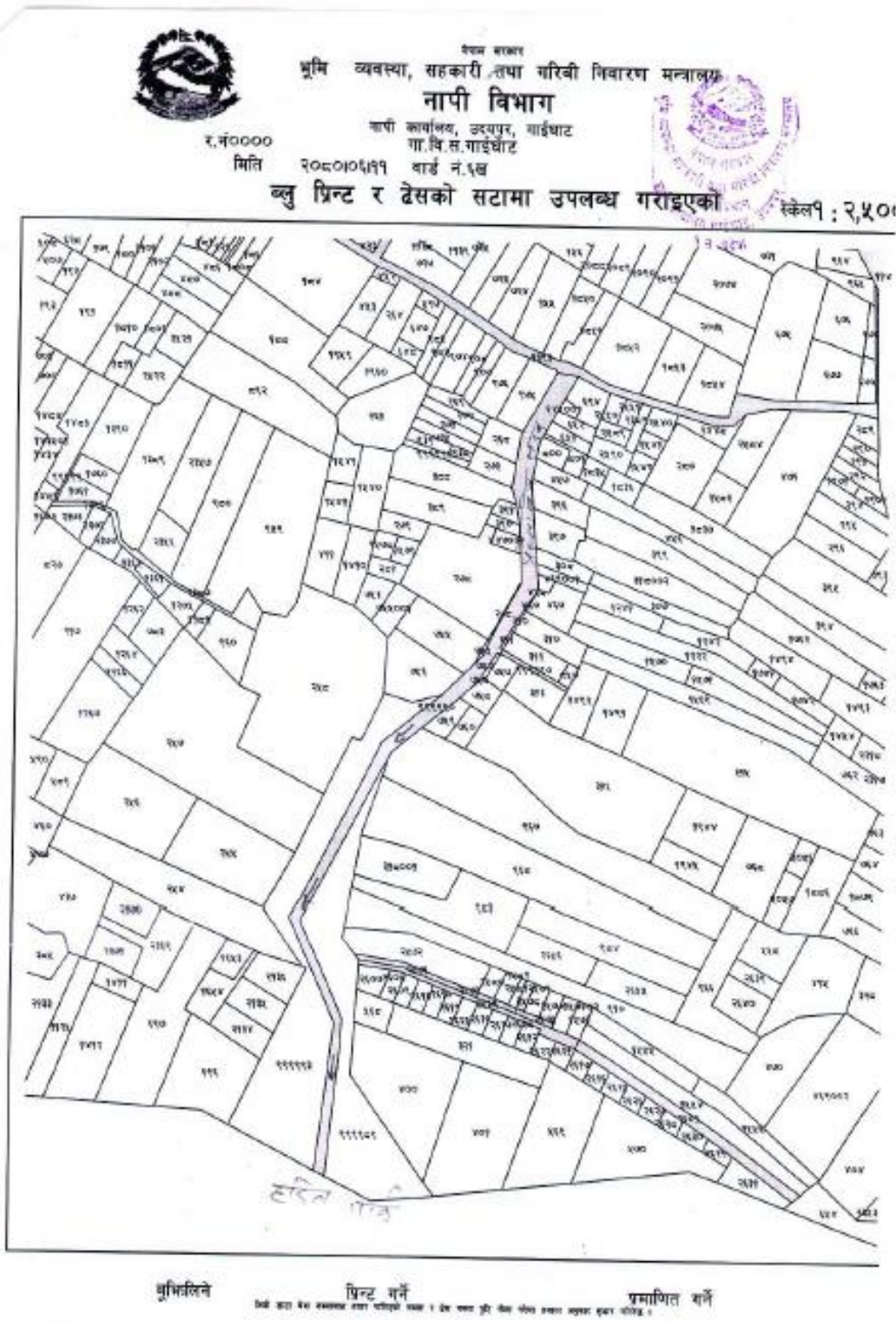


इभिलिने

प्रिन्ट गर्ने

पुनर्मापित गर्ने

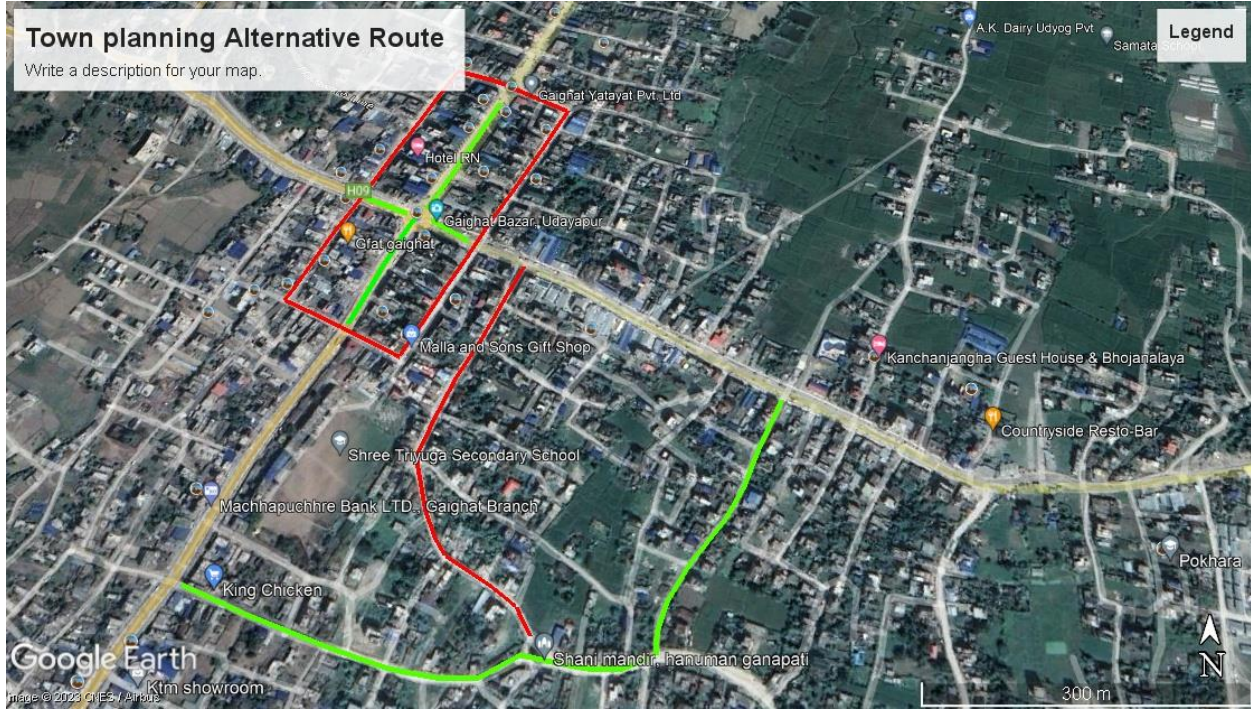
नक्सा तयार पार्ने कर्मचारीको नाम र उक्त नक्सा तयार गर्ने मिति उल्लेख गर्नुपर्नेछ।



Annex XII: Alternative route

In below images: **Red lines** are proposed road and **Green lines** are Alternative routes

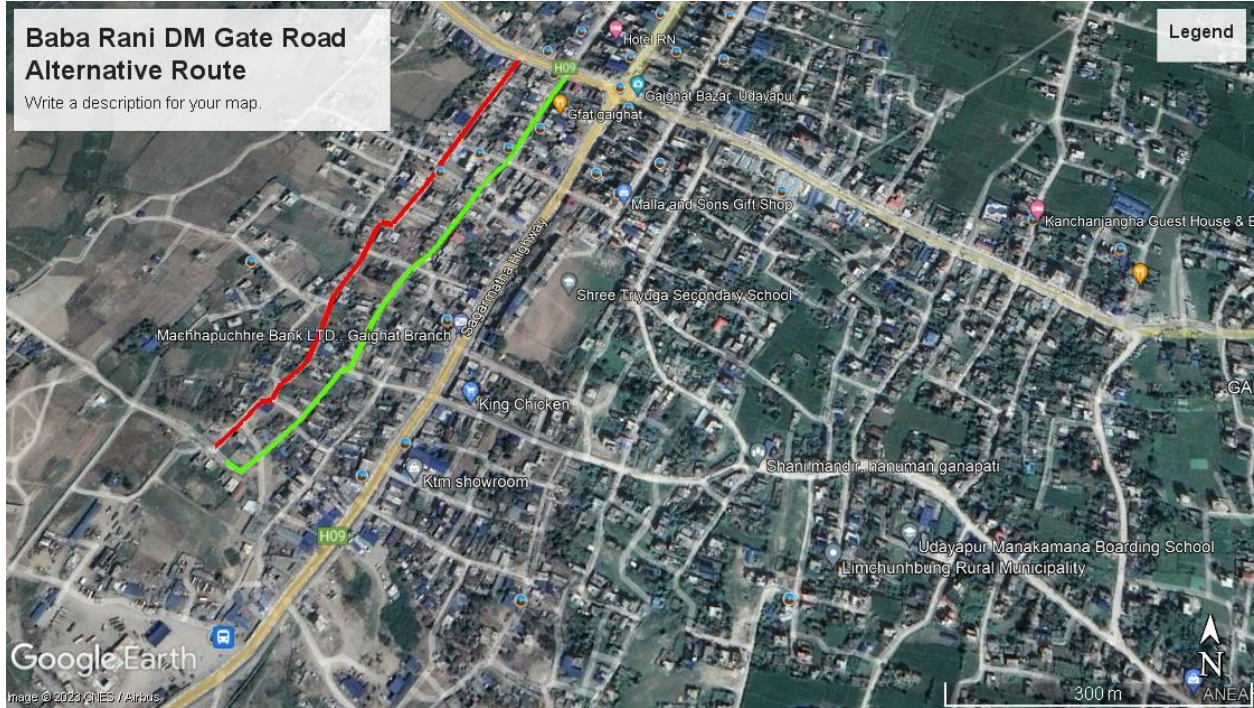
A) Gaighat town planning area Road Section



B) City hall- Simaltar-Gaighat- Diktel Road Section



C) Babarani DM Gate Road Section



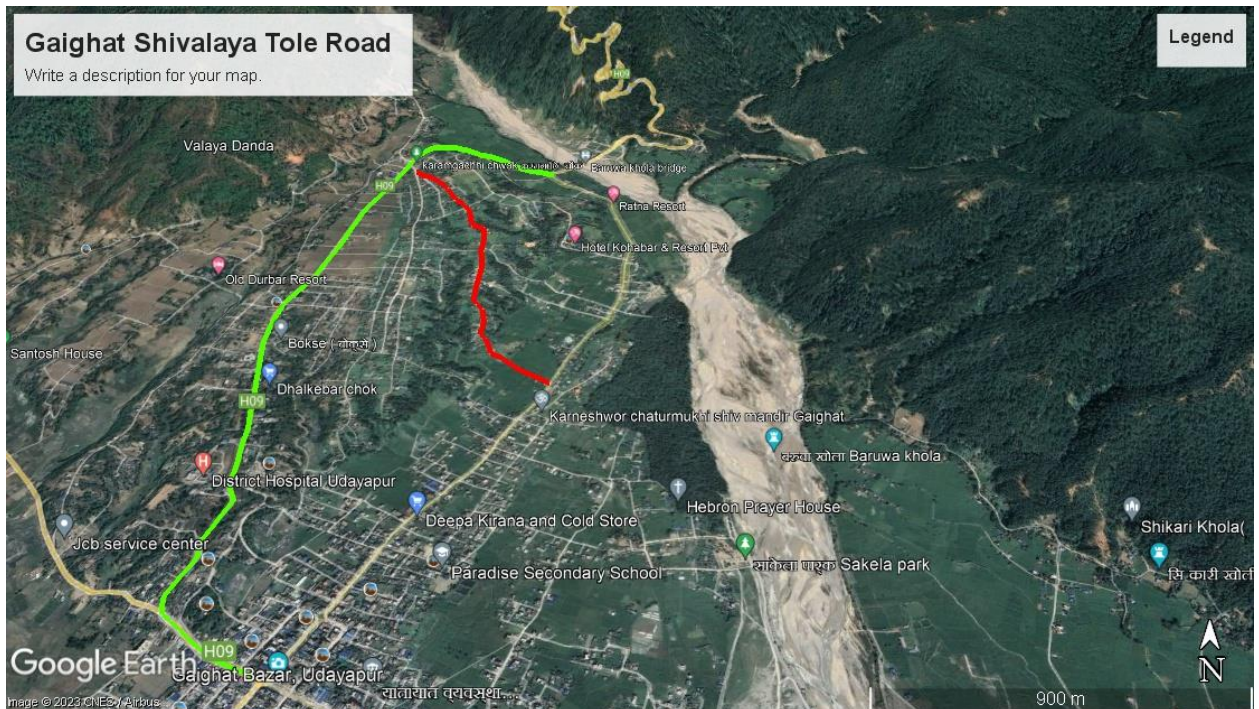
D) Fulchowk PG chowk Road Bote tole Triyuga Corridor section



E) City hall- Ramchandra Basnet house section



F) Gaighat Shivalaya tole-Adarsha tole-Ramailo danda Manmohan school to Karamgachhi Diktel connecting Road Section



G) Pragati tole kha (In front of Quarter) ko road to sakela road ward 12 Road section

